



VERIFY SCALES
 BAR IS ONE INCH ON ORIGINAL DRAWING
 0 ——— 1"
 IF NOT ONE INCH ON THIS SHEET, ADJUST SCALES ACCORDINGLY.

UNOFFICIAL GRAPHIC SCALE
 0 1/2 1 MILE 2 MILES



**MUNICIPALITY OF ANCHORAGE
 PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT**

**QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE**

**PM&E PROJECT NUMBER 21-13
 DECEMBER 2024
 95% DESIGN**

**PROJECT AREA
 THIS CONTRACT**

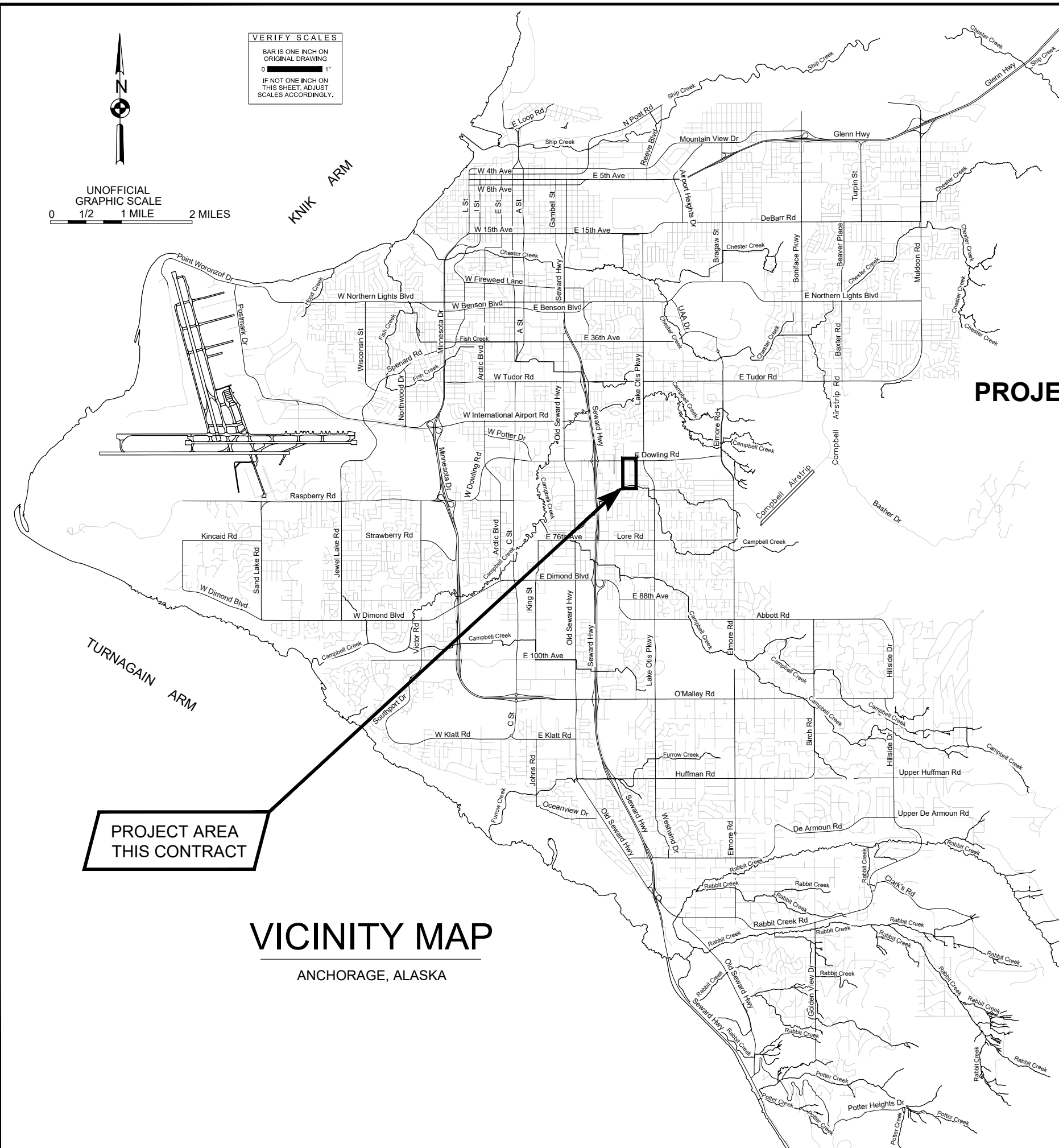
VICINITY MAP
 ANCHORAGE, ALASKA

PREPARED BY:



APPROVED BY:

MELINDA KOHLHAAS, P.E.
 MUNICIPAL ENGINEER



SHEET INDEX		
SHEET NO	DESCRIPTION	SCHEDULE
GENERAL		
G1	COVER SHEET	ALL
G2	SHEET INDEX	ALL
G3	GENERAL NOTES	ALL
G4	LEGEND AND ABBREVIATIONS	ALL
G5	KEY MAP	ALL
SURVEY		
V1	SURVEY CONTROL	ALL
V2	SURVEY CONTROL	ALL
V3	TEMPORARY EASEMENT & PERMIT MAP	ALL
V4	TEMPORARY EASEMENT & PERMIT MAP	ALL
V5	TEMPORARY EASEMENT & PERMIT MAP	ALL
DEMOLITION		
B1	DEMOLITION PLAN	ALL
B2	DEMOLITION PLAN	ALL
B3	DEMOLITION PLAN	ALL
B4	DEMOLITION SUMMARY TABLES	ALL
B5	DEMOLITION SUMMARY TABLES	ALL
B6	DEMOLITION SUMMARY TABLES	ALL
TYPICAL SECTIONS		
C1	TYPICAL SECTIONS	SCHED A
C2	TYPICAL SECTIONS	SCHED A
C3	TYPICAL SECTIONS	SCHED A
ROADWAY		
R1	ROADWAY PLAN & PROFILE	SCHED A
R2	ROADWAY PLAN & PROFILE	SCHED A
R3	ROADWAY PLAN & PROFILE	SCHED A
R4	ROADWAY PLAN & PROFILE	SCHED A
R5	ROADWAY PLAN & PROFILE	SCHED A
R6	INTERSECTION LAYOUT PLAN	SCHED A
R7	INTERSECTION LAYOUT PLAN	SCHED A
R8	INTERSECTION LAYOUT PLAN	SCHED A
R9	INTERSECTION LAYOUT PLAN	SCHED A
R10	DRIVEWAY RECONSTRUCTION PLAN	SCHED A
R11	DRIVEWAY RECONSTRUCTION PLAN	SCHED A

SHEET INDEX		
SHEET NO	DESCRIPTION	SCHEDULE
ROADWAY SUMMARY TABLES		
T1	ROADWAY SUMMARY TABLES	SCHED A
T2	ROADWAY SUMMARY TABLES	SCHED A
T3	ROADWAY SUMMARY TABLES	SCHED A
ROADWAY DETAILS		
D1	ROADWAY DETAILS	SCHED A
D2	ROADWAY DETAILS	SCHED A
D3	ROADWAY DETAILS	SCHED A
D4	ROADWAY DETAILS	SCHED A
D5	ROADWAY DETAILS	SCHED A
D6	ROADWAY DETAILS	SCHED A
D7	ROADWAY DETAILS	SCHED A
D8	ROADWAY DETAILS	SCHED A
SIGNING & STRIPING		
S1	SIGNING & STRIPING	SCHED A
S2	SIGNING & STRIPING	SCHED A
STORM DRAIN		
SD1	STORM DRAIN PLAN & PROFILE	SCHED B
SD2	STORM DRAIN PLAN & PROFILE	SCHED B
SD3	STORM DRAIN PLAN & PROFILE	SCHED B
SD4	STORM DRAIN PLAN & PROFILE	SCHED B
SD5	STORM DRAIN PLAN & PROFILE	SCHED B
SD6	STORM DRAIN DETAILS	SCHED B
SD7	STORM DRAIN DETAILS	SCHED B
SD8	STORM DRAIN DETAILS	SCHED B
SD9	STORM DRAIN DETAILS	SCHED B
SD10	STORM DRAIN SUMMARY TABLES	SCHED B
ILLUMINATION		
I1	ILLUMINATION PLAN	SCHED C
I2	ILLUMINATION PLAN	SCHED C
I3	ILLUMINATION SCHEDULES	SCHED C
I4	LOAD CENTER SCHEDULE AND SCHEMATICS	SCHED C
I5	ILLUMINATION DETAILS	SCHED C
I6	SELECT MASS STANDARD DETAILS (REFERENCE ONLY)	SCHED C

WORK SCHEDULES	
A	ROADWAY IMPROVEMENTS
B	DRAINAGE IMPROVEMENTS
C	ILLUMINATION IMPROVEMENTS

File: I:\jobdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Sheet Index.dwg

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
 THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
 CONTRACTOR: _____ TITLE: _____ DATE: _____
 BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR—PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

CONTRACTOR

INSPECTOR

PLAN CHECK

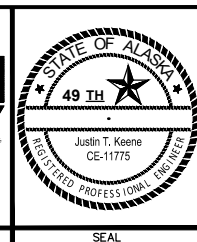
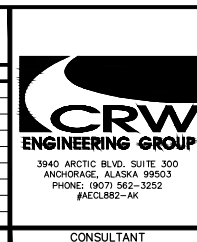
CONSTRUCTION RECORD

VERTICAL DATUM

REVISIONS

CONSULTANT

SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE ALL

SHEET INDEX

SCALE HOR. N/A VER. N/A

GRID SW2033

DATE DEC 2024 STATUS 95%

SHEET **G2** of **G5**

GENERAL NOTES

1. ALL WORK SHALL BE IN ACCORDANCE WITH THE MUNICIPALITY OF ANCHORAGE (MOA) STANDARD SPECIFICATIONS, DATED 2024, (HEREINAFTER REFERRED TO AS MASS), THE LATEST EDITION OF THE ANCHORAGE WATER AND WASTEWATER UTILITY (AWWU) DESIGN AND CONSTRUCTION PRACTICES MANUAL (DCPM) AND THE SPECIAL PROVISIONS.
2. CAUTION!!! THE LOCATION OF THE EXISTING FEATURES AND UTILITIES SHOWN IN THESE DRAWINGS (PLAN & PROFILES) ARE APPROXIMATE. WHERE SINGLE CABLE, ELECTRIC, TELEPHONE, TRAFFIC, AND FIBER OPTIC LINES ARE SHOWN IN THE PLANS, MULTIPLE CONDUITS EXIST IN THESE LOCATIONS AND SHALL BE PROTECTED IN PLACE BY CONTRACTOR UNLESS OTHERWISE NOTED. THE CONTRACTOR SHALL VERIFY THE HORIZONTAL AND VERTICAL LOCATION OF ALL FEATURES AND UTILITIES ENCOUNTERED AND RECORD THEIR LOCATION ON THE CONTRACT RECORD DRAWINGS. DISCREPANCIES SHALL BE BROUGHT TO THE IMMEDIATE ATTENTION OF THE ENGINEER. CONTRACTOR SHALL PROTECT EXISTING UTILITIES IN PLACE. CONTRACTOR SHALL SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED ON THE DRAWINGS. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
3. PRIOR TO BEGINNING CONSTRUCTION, THE CONTRACTOR SHALL OBTAIN ALL NECESSARY PERMITS WHICH ARE NOT SPECIFICALLY INDICATED AS BEING PROVIDED BY THE OWNER IN THE SPECIAL PROVISIONS. CONTRACTOR SHALL ADHERE TO ALL PERMIT REQUIREMENTS. THE PERMITS SHALL BE MAINTAINED ON THE PROJECT SITE. COPIES SHALL BE GIVEN TO THE ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
4. ALL WORK IN CLOSE PROXIMITY TO EXISTING OVERHEAD/UNDERGROUND TELEPHONE, CABLE, FIBER OPTIC, GAS, AND ELECTRIC UTILITIES SHALL COMPLY WITH APPLICABLE FEDERAL, STATE AND LOCAL STATUTES, CODES AND GUIDELINES AND THE SHORING AND CLEARANCE REQUIREMENTS OF THE SERVING UTILITY. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
5. LIMITS OF ROADWAY EXCAVATION SHOWN ON THE DRAWINGS ARE APPROXIMATE. ACTUAL LIMITS SHALL BE DETERMINED IN THE FIELD BY THE ENGINEER DURING CONSTRUCTION OPERATIONS.
6. GEOTECHNICAL (SOILS) INFORMATION IS INCLUDED IN THE CONTRACT DOCUMENTS.
7. ALL WORK SHALL BE PERFORMED WITHIN PUBLIC RIGHT-OF-WAY, PUBLIC USE EASEMENT, SLOPE EASEMENT, TEMPORARY CONSTRUCTION EASEMENT, DRAINAGE EASEMENT, ELECTRIC EASEMENT, INTRAGOVERNMENTAL USE PERMIT OR, TEMPORARY CONSTRUCTION PERMIT AREAS. THE EASEMENTS AND TEMPORARY CONSTRUCTION PERMITS ACQUIRED FOR THIS PROJECT MAY HAVE RESTRICTIONS. SEE CONTRACT DOCUMENTS FOR RESTRICTIONS.
8. CONTRACTOR SHALL RESTORE DISTURBED PROPERTY, INCLUDING DRAINAGE SWALES, TO PRE-CONSTRUCTION CONDITIONS, UNLESS OTHERWISE DIRECTED BY ENGINEER. PAYMENT FOR RESTORING DISTURBED PROPERTY OUTSIDE OF IDENTIFIED CONSTRUCTION LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE. DISTURBED AREAS NOT BEING PAVED OR FINISHED WITH GRAVEL/CONCRETE SHALL BE TOPSOILED AND SEEDED WITH SCHEDULE A SEEDING MIX UNLESS OTHERWISE NOTED.
9. PROJECT CLEARING AND GRUBBING LIMITS SHALL COINCIDE WITH THE LIMITS OF DISTURBANCE AS SHOWN ON THE DEMOLITION (B) SHEETS. CONTRACTOR SHALL OBTAIN APPROVAL OF THE CLEARING AND GRUBBING LIMITS BY THE ENGINEER PRIOR TO CLEARING AND GRUBBING, SEE SPECIFICATIONS FOR MORE INFORMATION. CONTRACTOR SHALL CLEAR TREE BRANCHES/LIMBS PER TREE CLEARING DETAILS SHOWN ON SHEET D7.
10. SLOPE LIMITS SHOWN ON THE DRAWINGS ARE APPROXIMATE. THE CONTRACTOR SHALL DETERMINE THE ACTUAL SLOPE LIMITS BASED ON PRECONSTRUCTION SURVEY DATA.
11. IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, THE CONTRACTOR SHALL SAW CUT AND REMOVE ADDITIONAL PAVEMENT BEYOND THE INITIAL SAW CUT, A MINIMUM OF 1-FOOT ONTO UNDISTURBED ASPHALT. AT TRANSVERSE JOINTS FINAL SAW CUT LINE SHALL BE SKEWED 15' - 25' PER DETAIL 2, SHEET D5. ASPHALT TACK COAT SHALL BE APPLIED BY CONTRACTOR TO THE SAWN FACE OF ASPHALT PRIOR TO BEGINNING PAVING.
12. PAVEMENT CROSS SLOPE ON SIDE STREETS SHALL VARY AT INTERSECTIONS TO PROVIDE POSITIVE DRAINAGE. SEE ROADWAY (R) SHEETS FOR INTERSECTION LAYOUTS.
13. ALL WORK AND MATERIALS REQUIRED FOR REMOVING ANY LITTER OR DEBRIS CREATED BY CONSTRUCTION OPERATIONS WITHIN THE PROJECT LIMITS SHALL BE CONSIDERED INCIDENTAL TO THE PROJECT AND NO SEPARATE PAYMENT SHALL BE MADE.
14. ALL ORGANIC MATERIAL SHALL BE REMOVED FROM THE SUBGRADE TO A DEPTH TO BE DETERMINED BY THE ENGINEER. NO ORGANIC MATERIAL OR OTHER DELETERIOUS MATERIAL SHALL BE UTILIZED FOR BACKFILL.
15. THE CONTRACTOR SHALL SUBMIT RECORD SURVEY NOTES WITH THE RECORD DRAWINGS.
16. ROADWAY/DRIVEWAY EXCAVATION SHALL BE MEASURED BY EXCAVATED CROSS-SECTION AND SHALL BE LIMITED TO THE PAY LIMITS IDENTIFIED IN THE TYPICAL CROSS SECTIONS SHOWN ON THE C SHEETS, UNLESS ADDITIONAL EXCAVATION IS DIRECTED BY THE ENGINEER IN WRITING.
17. THE PROJECT ROADWAY CENTERLINE STATIONING IS NOT RIGHT-OF-WAY CENTERLINE PER SURVEY CONTROL DRAWINGS UNLESS OTHERWISE NOTED. SEE SURVEY CONTROL DRAWINGS FOR HORIZONTAL AND VERTICAL CONTROL.
18. ALL CURB LOCATIONS, RADIUS MEASUREMENTS AND ELEVATIONS ARE TO THE TOP BACK OF CURB (TBC) UNLESS OTHERWISE NOTED.
19. MAINTAIN A MINIMUM OF TEN FEET (10') HORIZONTAL AND EIGHTEEN INCHES (18") SEPARATION BETWEEN THE OUTSIDE OF PIPES FOR WATER MAINS AND SERVICES TO SANITARY SEWER OR STORM DRAIN. INSTALL INSULATION BOARD (R-18) BETWEEN THE PIPES WHEN THE VERTICAL SEPARATION IS BETWEEN EIGHTEEN INCHES (18") AND THIRTY-SIX INCHES (36"). INSULATION MAY BE OMITTED WHEN THE VERTICAL SEPARATION IS GREATER THAN THIRTY-SIX INCHES (36"). WHERE STORM OR SEWER CROSS A WATER LINE, THE JOINTS OF ALL PIPES ARE TO HAVE A MINIMUM SEPARATION OF NINE FEET (9') FROM THE CROSSING.
20. EXISTING WATER AND SEWER SERVICE LINES ARE NOT SHOWN IN THE PROFILES UNLESS SPECIFICALLY CALLED OUT.
21. ALL CURB AND GUTTER SHALL BE PAID AS "P.C.C. CURB AND GUTTER (ALL TYPES)".
22. EXISTING SHALLOW (CABLE, ELECTRIC, TELEPHONE, GAS, FIBER OPTIC, ETC) UTILITIES AND RELOCATED PROPOSED SHALLOW UTILITIES ARE NOT SHOWN IN THE TYPICAL CROSS SECTIONS. EXISTING SHALLOW UTILITY CROSSINGS ARE SHOWN AT AN ASSUMED ELEVATION IN THE PROFILES UNLESS OTHERWISE NOTED. RELOCATED PROPOSED SHALLOW UTILITIES ARE NOT SHOWN IN THE PLANS OR PROFILES. RELOCATED PROPOSED SHALLOW UTILITIES ARE TO BE RELOCATED BY OTHERS AS SHOWN IN THE UTILITY RELOCATION PLANS, SEE CONTRACT DOCUMENTS FOR MORE INFORMATION.
23. THE MATCH EXISTING ELEVATIONS AS SHOWN IN THE PLANS ARE APPROXIMATE. CONTRACTOR SHALL ADJUST PROPOSED GRADES AS REQUIRED TO MATCH INTO EXISTING ELEVATIONS PER THE DIRECTION OF THE ENGINEER.
24. ALL FILL, USABLE EXCAVATION, AND TRENCH BACKFILL SHALL BE COMPACTED TO NINETY-FIVE PERCENT (95%) OF MAXIMUM DENSITY AT OPTIMUM MOISTURE CONTENT, PER MASS DIVISION 20 EARTHWORK, BASED ON MODIFIED PROCTOR TEST VALUES. ALL FILLS SHALL BE PLACED IN LIFTS NOT EXCEEDING 12-INCHES.
25. FIRE HYDRANTS SHALL BE ADJUSTED TO FINAL GRADE BY AWWU O&M DIVISION ON A REIMBURSABLE BASIS. THE CONTRACTOR IS TO PROVIDE WRITTEN NOTICE TO THE ENGINEER A MINIMUM OF SEVEN (7) DAYS PRIOR TO THE NEED FOR FINAL FIRE HYDRANT ADJUSTMENT. THE WRITTEN NOTICE IS TO CONTAIN, AT A MINIMUM, THE MANUFACTURER AND MODEL NUMBER OF THE HYDRANT AND VERTICAL ADJUSTMENT NEEDED IN SIX (6") INCREMENTS.
26. THE HORIZONTAL AND VERTICAL LOCATION OF THE EXISTING STORM DRAIN SYSTEM TO BE REPLACED/EXTENDED IS IN A DIFFERENT HORIZONTAL AND VERTICAL LOCATION OF THE PROPOSED STORM DRAIN SYSTEM TO BE INSTALLED IN LOCATIONS AS SHOWN ON THE STORM DRAIN (SD) SHEETS.
27. UNLESS OTHERWISE NOTED ALL VALVE BOXES, KEYBOXES, CLEANOUTS, CATCH BASINS, AND MANHOLES WITHIN THE CONSTRUCTION DISTURBANCE LIMITS SHALL BE ADJUSTED RELATIVE TO FINISH GRADE PER MASS, THESE DRAWINGS OR THE SPECIAL PROVISIONS.
28. IN CASE OF CONFLICT BETWEEN STATIONING AND DIMENSIONED LOCATION OF PIPE OR FITTINGS, USE DIMENSIONED LOCATIONS.
29. THE CONTRACTOR SHALL BE RESPONSIBLE FOR EROSION AND SEDIMENT CONTROLS AS NECESSARY TO COMPLY WITH FEDERAL, STATE, AND MUNICIPAL LAWS THAT PROHIBIT UNPERMITTED DISCHARGE OF POLLUTANTS, INCLUDING SEDIMENTS, THAT ARE A RESULT OF EROSION AND OTHER CONSTRUCTION ACTIVITIES. THE CONTRACTOR SHALL CONDUCT ALL WORK SO SEDIMENT IS NOT TRANSPORTED ONTO THE ROADWAY OR ADJACENT PROPERTY. AT A MINIMUM, THE CONTRACTOR SHALL SWEEP UP ANY SEDIMENT TRACKED ONTO PAVED SURFACES IN PUBLIC RIGHT-OF-WAY WITHIN 24 HOURS OF THE TRACKING TO MINIMIZE THE WASH-OFF OF SEDIMENT INTO THE STORM DRAINS OR WATERWAYS.
30. WATER RESULTING FROM CONTRACTOR'S DEWATERING EFFORT MAY NOT BE PUMPED OR OTHERWISE DIVERTED INTO EXISTING STORM DRAINS OR CREEKS UNLESS PERMITS ARE OBTAINED BY THE CONTRACTOR, INCLUDING BUT NOT LIMITED TO, THOSE REQUIRED BY THE MOA STORM WATER PLAN REVIEW OFFICE. UNDER NO CIRCUMSTANCES WILL THE CONTRACTOR BE ALLOWED TO DIVERT WATER FROM AN EXCAVATION ONTO ROADWAYS. CONTRACTOR SHALL PROVIDE A DISPOSAL SITE FOR EXCESS WATER AND SHALL BE RESPONSIBLE FOR SECURING ALL NECESSARY PERMITS AND APPROVALS. CONTRACTOR SHALL PROVIDE COPIES OF NECESSARY PERMITS AND APPROVALS TO THE MOA RIGHT-OF-WAY PERMIT OFFICE.

CALL BEFORE YOU DIG!!!	
Alaska Digline, Inc. Statewide	811
Alaska Railroad	265-2520
Military Fuel Lines	552-3760
State Storm Drains	333-2411

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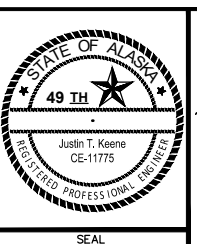
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 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY							
BASE	CB	BW							
TOPOGRAPHY	CB	BW							
PROFILE	RB	JK							
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'			
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'			
GAS	CB	BW	STAKING						
TELEPHONE	CB	BW							
ELECTRIC	JH	TK							
DESIGN	RB	JK	ASBUILT						
QUANTITIES	RB	JK	CONTRACTOR						
PRELIMINARY/FINAL	RB	JK	INSPECTOR						
MUNICIPAL/STATE	RB	JK							
	PLAN CHECK		CONSTRUCTION RECORD						
			VERTICAL DATUM						
			REVISIONS						
			CONSULTANT						
			SEAL						



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE	ALL	
GENERAL NOTES			
SCALE	HOR. N/A VER. N/A	GRID SW2033 DATE DEC 2024	STATUS 95% SHEET 63 of 65

File: E:\Jobs\10155.00_Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\01_G4\10155.00_Quinhagak Legend And Abbreviations.dwg

PLAN LEGEND

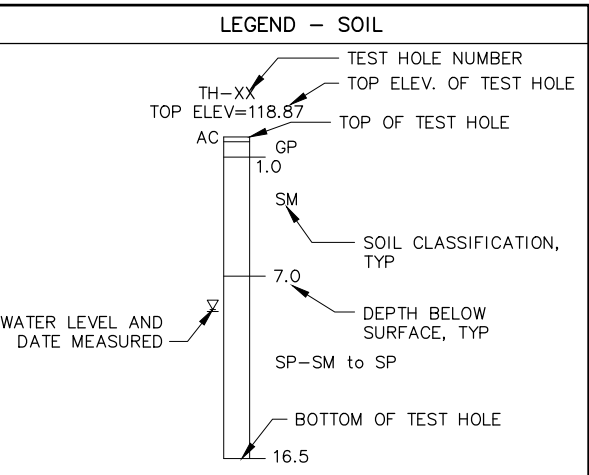
PROPERTY		
EXISTING	PROPOSED	
---	---	CENTERLINE
---	---	EASEMENT LINE
---	---	PROPERTY LINE
---	---	ROW LINE
---	---	SECTION LINE
---	---	TEMPORARY CONSTRUCTION EASEMENT/PERMIT
UTILITY		
EXISTING	PROPOSED	
-----	-----	ABANDONED UTILITY
---	---	CABLE TV LINE (UNDERGROUND)
---	---	CABLE TV LINE (OVERHEAD)
---	---	CABLE TV LINE & FIBER OPTIC (OVERHEAD)
---	---	CABLE TV PEDESTAL (UNDERGROUND)
---	---	CONTROLLER OR ATR CABINET
---	---	CULVERT
---	---	ELECTRIC LINE (UNDERGROUND)
---	---	ELECTRIC LINE (OVERHEAD)
---	---	ELECTRIC & CABLE TV LINE (OVERHEAD)
---	---	ELECTRIC & TELEPHONE LINE (OVERHEAD)
---	---	ELECTRIC, CABLE TV & FIBER OPTIC (OVERHEAD)
---	---	ELECTRIC JB TYPE IA
---	---	ELECTRIC JB TYPE II
---	---	ELECTRIC JB TYPE III
---	---	ELECTRIC LOAD CENTER
---	---	ELECTRIC MANHOLE/JB
---	---	ELECTRIC METER
---	---	ELECTRIC PEDESTAL UNDERGROUND
---	---	ELECTRIC SIGN
---	---	ELECTRIC SWITCH CABINET
---	---	ELECTRIC TRANSFORMER
---	---	ELECTRIC VAULT
---	---	FIBER OPTIC LINE (UNDERGROUND)
---	---	FIBER OPTIC VAULT
---	---	FLOOR DRAIN
---	---	FOOTING DRAIN SERVICE LINE
---	---	FOOTING DRAIN SERVICE CONNECTION
---	---	GAS LINE
---	---	GAS METER
---	---	GAS VALVE
---	---	GUY POLE
---	---	GUY ANCHOR
---	---	JOINT USE ELECTRIC & TELEPHONE POLE
---	---	LIGHTED BOLLARD
---	---	LIGHTING LINE
---	---	LUMINAIRE
---	---	LUMINAIRE (PEDESTRIAN)
---	---	REMOVE PIPE
---	---	SANITARY SEWER LINE
---	---	SANITARY SEWER MANHOLE
---	---	SANITARY SEWER SERVICE CONNECTION
---	---	SANITARY SEWER CLEANOUT
---	---	STORM DRAIN LINE
---	---	SUBDRAIN LINE
---	---	STORM DRAIN CATCH BASIN
---	---	STORM DRAIN CATCH BASIN MANHOLE OR MH
---	---	STORM DRAIN MANHOLE (TYPE VARIES)
---	---	STUBOUT CAPPED OR PLUGGED END

UTILITY		
EXISTING	PROPOSED	
---	---	TELEPHONE & CABLE TV LINE (OVERHEAD)
---	---	TELEPHONE LINE (OVERHEAD)
---	---	TELEPHONE LINE (UNDERGROUND)
---	---	TELEPHONE MANHOLE
---	---	TELEPHONE PEDESTAL
---	---	TRAFFIC DETECTOR LOOPS
---	---	TRAFFIC LINE (UNDERGROUND)
---	---	TRAFFIC SIGNAL POLE
---	---	TRAFFIC SIGNAL POLE/LUMINAIRE
---	---	UTILITY POLE
---	---	WATER LINE
---	---	WATER FIRE HYDRANT
---	---	WATER KEY BOX
---	---	WATERTIGHT SANITARY SEWER MANHOLE
---	---	WATER VALVE
---	---	WATER WELL
ROADWAY		
EXISTING	PROPOSED	
---	---	APPROX SLOPE LIMITS (CUT)
---	---	APPROX SLOPE LIMITS (FILL)
---	---	COLORED CONCRETE (RED, THICKNESS VARIES, IMPRINTED)
---	---	CURB & GUTTER
---	---	EDGE OF PAVEMENT
---	---	EDGE OF SIDEWALK/CONCRETE
---	---	GUARDRAIL, BARRIER RAIL
---	---	POROUS PAVEMENT SYSTEM
---	---	RETAINING WALL (TYPE VARIES)
---	---	STREET SIGN
---	---	UNPAVED (GRAVEL) EDGE OF ROAD/DWY

MISCELLANEOUS		
EXISTING	PROPOSED	
---	---	BLUFF AREA/ EARTHWORK SLOPE
---	---	BOLLARD/POST (TYPE VARIES)
---	---	BOULDER
---	---	CONTOUR
---	---	DECK
---	---	DRAINAGE ARROW (DIRECTION OF FLOW)
---	---	DRAINAGE SWALE
---	---	FENCE (TYPE VARIES)
---	---	FENCE (DECORATIVE)
---	---	HOUSE OR STRUCTURE
---	---	LANDSCAPING ROCK
---	---	MAILBOX (INDIVIDUAL)
---	---	MAILBOX (CLUSTER)
---	---	NEWS BOX
---	---	PARKING METER
---	---	PARCEL NUMBER WITH PARCEL ADDRESS BELOW
---	---	STREAMBANK RECONSTRUCTION (FULL)
---	---	STREAMBANK RECONSTRUCTION (LIMITED)
---	---	STREAM/EDGE OF WATERWAY
---	---	TREE/SHRUB (CONIFEROUS)
---	---	TREE/SHRUB (DECIDUOUS)
---	---	TEST BORING OR TEST HOLE
---	---	VEGETATION & BRUSH/TREE LINE

PROFILE LEGEND

SYMBOL		
EXISTING	PROPOSED	
---	---	APPROXIMATE EXCAVATION LIMITS
---	---	GROUND OVER PIPE
---	---	GRADE AT CENTER LINE
---	---	GRADE AT LEFT ROW
---	---	GRADE AT RIGHT ROW
---	---	PIPE (PROFILE)
---	---	PIPE (SECTION)
---	---	STORM DRAIN CATCH BASIN/OGS
---	---	STORM DRAIN/SANITARY SEWER MANHOLE & PIPE
---	---	UTILITY CROSSING
---	---	UTILITY CROSSING (WATER/SEWER/STORM DRAIN)
---	---	UTILITY CROSSING (CABLE)
---	---	UTILITY CROSSING (ELECTRIC)
---	---	UTILITY CROSSING (FIBER OPTIC)
---	---	UTILITY CROSSING (GAS)
---	---	UTILITY CROSSING (TELEPHONE)
---	---	INSULATION
---	---	RIPRAP



- NOTES:
- STANDARD LEGEND AND ABBREVIATIONS SHOWN. NOT ALL LEGEND ITEMS AND ABBREVIATIONS ARE PART OF THIS CONTRACT.
 - SOIL CLASSIFICATION IS BASED UPON UNIFIED SOIL CLASSIFICATION (ASTM D 2487-00), SEE GEOTECHNICAL SOIL BORING LOGS FOR MORE INFORMATION.
 - SEE LEGEND ON SHEET V1 FOR SURVEY CONTROL SYMBOLS. ADDITIONAL LEGEND AND ABBREVIATION ITEMS NOT SHOWN HERE ARE PROVIDED ON SPECIFIC SHEETS THROUGHOUT THE DRAWINGS.

COMMON ABBREVIATIONS (ABBR.)

ABBR.	DESCRIPTION	ABBR.	DESCRIPTION
AC	ASPHALT CONCRETE	MSL	MEAN SEA LEVEL
	ASBESTOS CEMENT	N	NORTH
ACP	ASPHALT CONCRETE PAVEMENT	N/A	NOT APPLICABLE
AD	ALGEBRAIC DIFFERENCE	N.I.C.	NOT IN CONTRACT
ADEC	ALASKA DEPARTMENT OF ENVIRONMENTAL CONSERVATION	NTS	NOT TO SCALE
		NWT	NO WATER TABLE
AWG	AMERICAN WIRE GAUGE	OC	ON CENTER
AWWA	AMERICAN WATER WORKS ASSOCIATION	OCEW	ON CENTER EACH WAY
APPROX/ APPX	APPROXIMATE	OD	OUTSIDE DIAMETER
BM	BENCH MARK	OGS	OIL AND GRIT SEPARATOR
	BEGINNING OF PROJECT	OH	OVERHEAD
BOP	BOTTOM OF PIPE (OUTSIDE)	PC	POINT OF CURVATURE
	BOTTOM OF STEEL	PCC	PORTLAND CONCRETE CEMENT
BOS	BOTTOM OF STEEL		POINT OF CONTINUOUS CURVATURE
C&G	CURB AND GUTTER	PCMP	PRECOATED CORRUGATED METAL PIPE
CB	CATCH BASIN	PCPEP	PERFORATED CORRUGATED POLYETHYLENE PIPE
CBMH	CATCH BASIN MANHOLE	PI	POINT OF INTERSECTION
CI	CAST IRON	PL, P/L	PROPERTY LINE
C/L, CL	CENTERLINE	POB	POINT OF BEARING
CMP	CORRUGATED METAL PIPE	PSL	POSTED SPEED LIMIT
CO	CLEANOUT	PT	POINT OF TANGENCY
CONST	CONSTRUCTION	PUE	PUBLIC USE EASEMENT
CPEP	CORRUGATED POLYETHYLENE PIPE	PVC	POINT OF VERTICAL CURVATURE
CY	CUBIC YARD	PVC	POLYVINYL CHLORIDE
DIA	DIAMETER	PVI	POINT OF VERTICAL INTERSECTION
DI	DUCTILE IRON	PVT	POINT OF VERTICAL TANGENT
DW	DETECTABLE WARNING	REINF	REINFORCEMENT
DWY	DRIVEWAY	ROW, R/W	RIGHT OF WAY
E	EAST	RJB	RESTRAINED JOINT INTEGRAL BELL
ELEC	ELECTRIC / ELECTRICAL	RT, R	RIGHT
ELEV, EL	ELEVATION	S	SOUTH
EOP	END OF PROJECT / EDGE OF PAVEMENT	S/W	SIDEWALK
F&I	FURNISH AND INSTALL	SS	STAINLESS STEEL
FF	FINISHED FLOOR	SF	SQUARE FOOT
FG	FINISHED GRADE	SI	STREET INTERSECTION
FH	FIRE HYDRANT	ST	STREET
GA	GAUGE	STA	STATION / STATIONING
GALV	GALVANIZED	STD	STANDARD
GB	GRADE BREAK	STRUCT	STRUCTURE
GV	GATE VALVE	TBC	TOP BACK OF CURB
H/HORIZ	HORIZONTAL	TBM	TEMPORARY BENCH MARK
HMWPE	HIGH MOLECULAR WEIGHT POLYETHYLENE	TCP	TEMPORARY CONSTRUCTION PERMIT
JB	JUNCTION BOX	TELE	TELEPHONE
LC	LOAD CENTER	TH	TEST HOLE
IAW	IN ACCORDANCE WITH	TOP	TOP OF PIPE
ID	INSIDE DIAMETER	TOS	TOP OF STEEL
IE/INV	INVERT ELEVATION (INSIDE BTM OF PIPE)	TW	TOP OF WALL
INTX	INTERSECTION	TYP	TYPICAL
INV	INVERT	UG	UNDERGROUND
KB	KEYBOX	UON	UNLESS OTHERWISE NOTED
LF	LINEAR FOOT	UTIL	UTILITY
LT, L	LEFT	VERT	VERTICAL
LUM	LUMINAIRE	VB	VALVE BOX
MAX	MAXIMUM	VC	VERTICAL CURVE
ME	MATCH EXISTING	W	WEST
MH	MANHOLE	W/	WITH
MIN	MINIMUM		
MON	MONUMENT		

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ TITLE: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW	STAKING							
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
PRELIMINARY/FINAL	RB	JK	INSPECTOR							
MUNICIPAL/STATE	RB	JK								

CRW ENGINEERING GROUP

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC0882-AK

STATE OF ALASKA
49 TH
JUSTIN T. KEENE
CE-11775
REGISTERED PROFESSIONAL ENGINEER

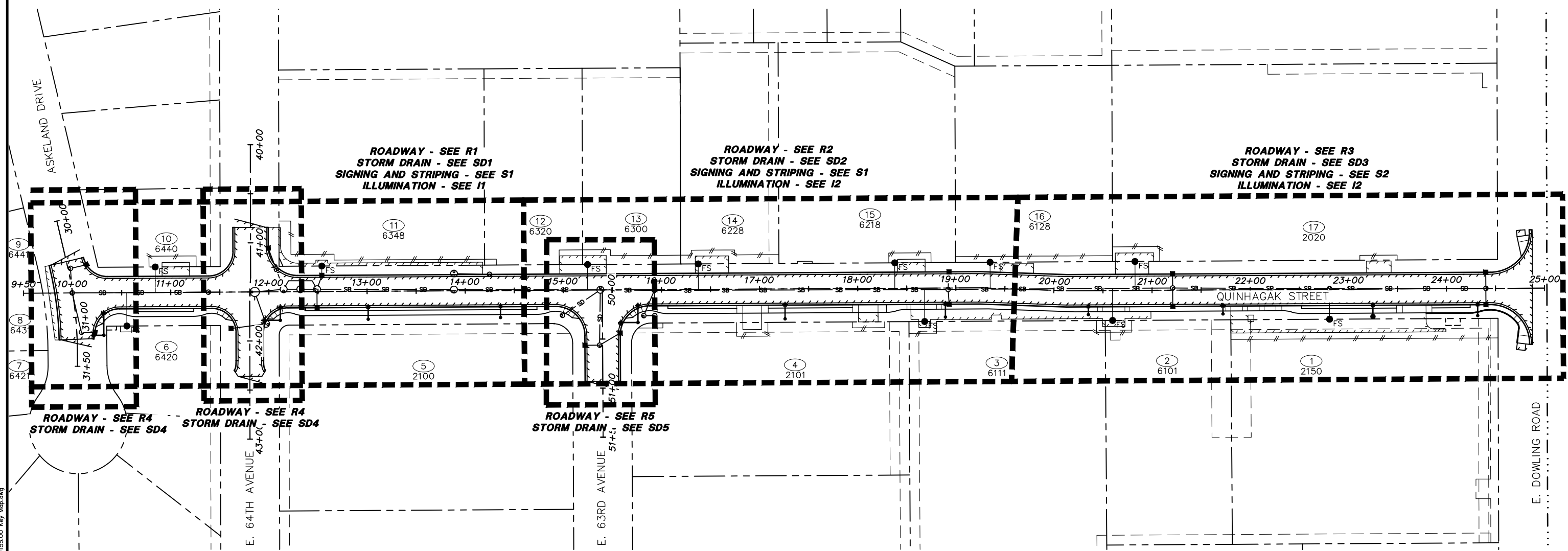
UNIVERSITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION
E. DOWLING ROAD TO ASKELAND DRIVE ALL

LEGEND AND ABBREVIATIONS

SCALE HOR. N/A VER. N/A GRD SW2033 DATE DEC 2024 STATUS 95% SHEET 64 of 65



ROADWAY - SEE R1
STORM DRAIN - SEE SD1
SIGNING AND STRIPING - SEE S1
ILLUMINATION - SEE I1

ROADWAY - SEE R2
STORM DRAIN - SEE SD2
SIGNING AND STRIPING - SEE S1
ILLUMINATION - SEE I2

ROADWAY - SEE R3
STORM DRAIN - SEE SD3
SIGNING AND STRIPING - SEE S2
ILLUMINATION - SEE I2

ROADWAY - SEE R4
STORM DRAIN - SEE SD4

ROADWAY - SEE R5
STORM DRAIN - SEE SD5

NOTES:

- EXISTING FEATURES ARE NOT SHOWN FOR CLARITY.
- NOT ALL SHEETS ARE CALLED OUT FOR CLARITY.

RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____
BY: _____ TITLE: _____ DATE: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
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BY: _____

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TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST



CRW ENGINEERING GROUP
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECLE82-AK

STATE OF ALASKA
49 TH
Justin T. Keene
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REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION
E. DOWLING ROAD TO ASKELAND DRIVE ALL

KEY MAP

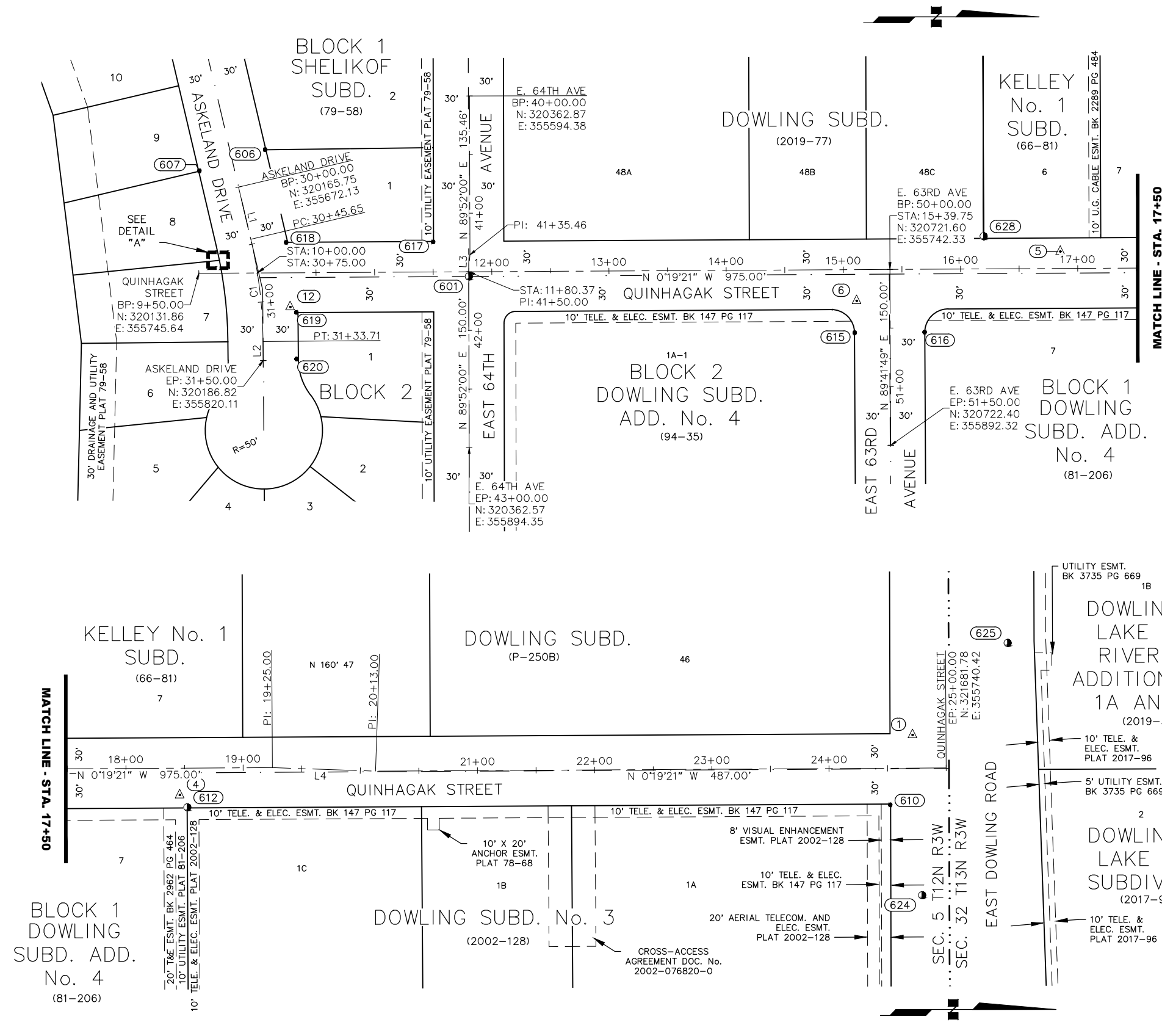
SCALE HOR. 1"=50'
VER. N/A

GRID SW2033
DATE DEC 2024 STATUS 95%

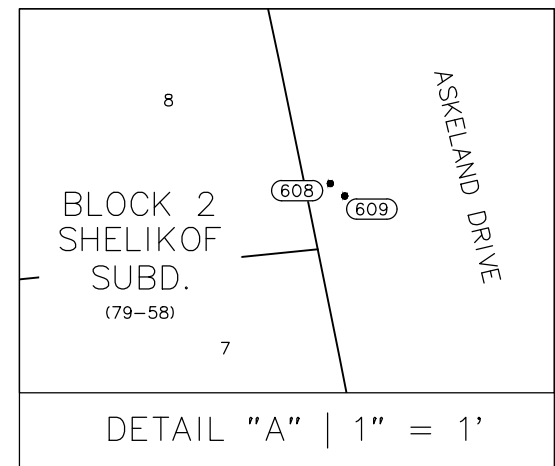
SHEET G5 of G5

File: I:\webdata\10152.00 Camrose Drive Storm Drainage\00 CADD 2019\01 Working Set\01 Civil\10155.00 Key Map.dwg

File: E:\webdata\10155.00_Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\02_Survey\03_Survey Control\SURVEY CONTROL.dwg



- LEGEND**
- EXISTING ALUMINUM CAP
 - EXISTING REBAR OR IRON PIPE
 - ▲ CONTROL SET BY CRW
 - (500) CONTROL POINT NUMBER



HORIZONTAL CONTROL

COORDINATE SYSTEM:
THIS PROJECT IS LOCATED ENTIRELY WITHIN THE ANCHORAGE BOWL 2000 ADJUSTMENT, A LOCAL SURFACE GRID COORDINATE SYSTEM EXPRESSED IN U.S. SURVEY FEET UNITS DEVELOPED BY THE ALASKA DEPARTMENT OF TRANSPORTATION.

BASIS OF COORDINATES:
THE BASIS OF COORDINATES IS NGS STATION O'MALLEY, LOCATED NEAR THE INTERSECTION OF THE NEW SEWARD HIGHWAY AND O'MALLEY ROAD. SAID STATION HAS ANCHORAGE BOWL 2000 COORDINATES OF 303939.2310 N, 353362.5446 E. U.S. SURVEY FEET.

BASIS OF BEARINGS:
THE BASIS OF BEARINGS IS A LOCAL PLANE BEARING BETWEEN NGS STATION O'MALLEY AND NGS STATION LOOP 2 USE RM 3 1964. NGS STATION LOOP 2 USE RM 3 1964 BEARS N 01°43'26.4" E A DISTANCE OF 49488.4476 FEET FROM NGS STATION O'MALLEY. NGS STATION LOOP 2 USE RM 3 1964 HAS ANCHORAGE BOWL 2000 COORDINATES OF 353405.2778 N, 354851.3982 E. U.S. SURVEY FEET.

TRANSLATION PARAMETERS:
TO CONVERT THE LOCAL COORDINATES TO NAD83 (92) STATE PLANE COORDINATES EXPRESSED IN U.S. SURVEY FEET, TRANSLATE USING +2,296,868.6878 N U.S. SURVEY FEET, +1,312,517.4904 E U.S. SURVEY FEET, AND SCALE USING 0.9998910192.

VERTICAL CONTROL

PROJECT VERTICAL DATUM IS GAAB 1972 ADJUSTMENT HOLDING MOA BENCHMARK GAAB 22 WITH PUBLISHED ELEVATION OF 162.82', AS DESCRIBED ON PAGE D29 OF THE MOA BENCHMARK BOOK, AND MOA BENCHMARK GAAB 20 WITH PUBLISHED ELEVATION OF 183.44', AS DESCRIBED ON PAGE D35 OF THE MOA BENCHMARK BOOK.

SURVEY NOTES

1. FIELD SURVEY WAS CONDUCTED MAY 9 THROUGH JUNE 22, 2022.
2. ALL POINTS SHOWN HEREON WERE ESTABLISHED BY NETWORK STATIC GNSS, REDUNDANT RTK GNSS, OR CONVENTIONALLY VIA REPEATED ANGLES FROM MULTIPLE BACK SIGHTS.
3. ALIGNMENTS SHOWN ARE PROJECT ALIGNMENTS AND DO NOT NECESSARILY REPRESENT RIGHT-OF-WAY CENTERLINE.
4. SEE SHEET V2 FOR POINT TABLES.

CURVE TAG TABLE

TAG No.	RADIUS	ARC LENGTH	CHORD BEARING	CHORD LENGTH
C1	395.00'	88.06'	N83°09'04"E	87.87'

LINE TAG TABLE

TAG No.	LENGTH	BEARING
L1	45.65'	N76°45'53"E
L2	16.29'	N89°32'15"E
L3	14.54'	S86°11'21"E
L4	88.00'	N1°57'25"E

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.

CONTRACTOR: _____ TITLE: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

DATA TRANSFER CHECKED BY: _____ TITLE: _____

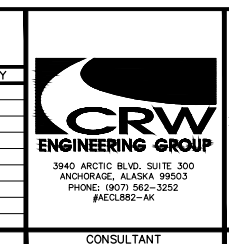
COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE ALL

SURVEY CONTROL

ASKELAND DRIVE, E 63RD AVE, E 64TH AVE, & QUINHAGAK STREET

SCALE HOR. 1"=50'	GRID 992033	DATE DEC 2024	STATUS 95%	SHEET 1 of 5
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POINT TABLE – ASKELAND DRIVE					
POINT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
618	30+54.54	28.87 LT	320206.39	355719.26	FOUND 1-1/4" Y.P.C. F.W.G.
608	30+57.28	29.74 RT	320149.56	355733.88	FOUND 5/8" REBAR IN GRAVEL F.W.G.
609	30+57.45	29.61 RT	320149.72	355734.01	FOUND 5/8" REBAR W/BROKEN PLASTIC CAP F.W.G.
12	31+06.07	24.51 LT	320209.93	355774.30	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
619	31+10.76	29.25 LT	320215.02	355778.96	FOUND 5/8" REBAR F.W.G.
620	31+48.85	28.39 LT	320215.19	355818.73	FOUND 5/8" REBAR 0.1' A.G.
607	N/A	N/A	320132.31	355658.14	FOUND LEANING 5/8" REBAR 0.5' A.G.
606	N/A	N/A	320188.43	355640.19	FOUND 5/8" REBAR 0.5' B.G.

POINT TABLE – EAST 64th AVE.					
POINT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
617	41+24.42	31.365 RT	320331.80	355718.87	FOUND 5/8" REBAR 0.1' B.G.
601	41+54.00	0.00 RT	320362.23	355748.35	FOUND 2" ALUMINUM CAP IN PAVEMENT F.W.G.
* 627	N/A	N/A	320391.73	355518.28	FOUND 1-1/4" Y.P.C. 0.2' A.G.
* 621	N/A	N/A	320363.72	356203.20	FOUND 2" ALUMINUM CAP F.W.G.
* 605	N/A	N/A	320361.19	355288.37	FOUND 2" ALUMINUM CAP IN MONUMENT CASE 0.5' B.G.
* 604	N/A	N/A	320362.93	356038.26	FOUND 2" ALUMINUM CAP IN PAVEMENT F.W.G.

* NOT SHOWN, OUTSIDE OF VIEWPORT

POINT TABLE – EAST 63rd AVE.					
POINT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
6	50+26.61	28.43 RT	320693.32	355769.08	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
616	50+53.64	29.30 LT	320751.18	355795.81	FOUND 5/8" REBAR F.W.G.
615	50+53.80	30.30 RT	320691.59	355796.29	FOUND 5/8" REBAR 0.7' B.G.
* 603	N/A	N/A	320723.54	356203.15	FOUND BENT 5/8" REBAR IN PAVEMENT F.W.G.
* 614	N/A	N/A	320752.21	355933.17	FOUND BENT 5/8" REBAR F.W.G.

* NOT SHOWN, OUTSIDE OF VIEWPORT

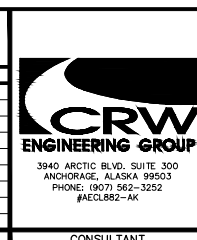
POINT TABLE – QUINHAGAK STREET					
POINT	STATION	OFFSET	NORTHING	EASTING	DESCRIPTION
608	9+67.77	11.67 LT	320149.56	355733.88	FOUND 5/8" REBAR IN GRAVEL F.W.G.
609	9+67.92	11.54 LT	320149.72	355734.01	FOUND 5/8" REBAR W/BROKEN PLASTIC CAP F.W.G.
618	10+24.68	25.96 LT	320206.39	355719.26	FOUND 1-1/4" Y.P.C. F.W.G.
12	10+27.91	29.09 RT	320209.93	355774.30	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
619	10+32.97	33.78 RT	320215.02	355778.96	FOUND 5/8" REBAR F.W.G.
617	11+50.09	25.65 LT	320331.80	355718.87	FOUND 5/8" REBAR 0.1' B.G.
601	11+80.35	4.00 RT	320362.23	355748.35	FOUND 2" ALUMINUM CAP IN PAVEMENT F.W.G.
6	15+11.32	26.60 RT	320693.32	355769.08	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
628	16+19.82	27.83 LT	320801.51	355714.04	FOUND 3" MONUMENT TUBE W/CAP REMOVED
5	16+85.40	14.47 LT	320867.17	355727.04	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
4	18+45.91	22.92 RT	321027.88	355763.52	SET 2" ALUMINUM CAP ON 5/8" 0.1' B.G.
612	18+52.11	33.50 RT	321034.14	355774.06	FOUND 3-1/4" ALUMINUM CAP 0.4' B.G.
* 611	24+51.73	259.74 RT	321634.97	356000.43	FOUND 2" ALUMINUM CAP 0.6' B.G.
610	24+52.11	31.17 RT	321634.07	355771.86	FOUND 1-1/4" Y.P.C. 0.2' B.G.
* 623	24+54.86	948.60 LT	321631.31	354792.09	FOUND 1-1/4" Y.P.C. 0.1' B.G.
1	24+71.63	28.39 LT	321653.25	355712.20	SET 2" ALUMINUM CAP ON 5/8" 0.2' B.G.
* 624	24+78.84	108.46 RT	321661.23	355849.00	FOUND 2" ALUMINUM CAP F.W.G.
* 626	N/A	N/A	321731.10	354625.62	FOUND 2" ALUMINUM CAP F.W.G.
625	N/A	N/A	321734.17	355633.79	FOUND 2" ALUMINUM CAP F.W.G.
* 622	N/A	N/A	321687.01	356662.53	FOUND 2" BRASS CAP IN MONUMENT CASE 0.7' B.G.

* NOT SHOWN, OUTSIDE OF VIEWPORT

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\02 Survey\03 Survey Control\SURVEY CONTROL.dwg

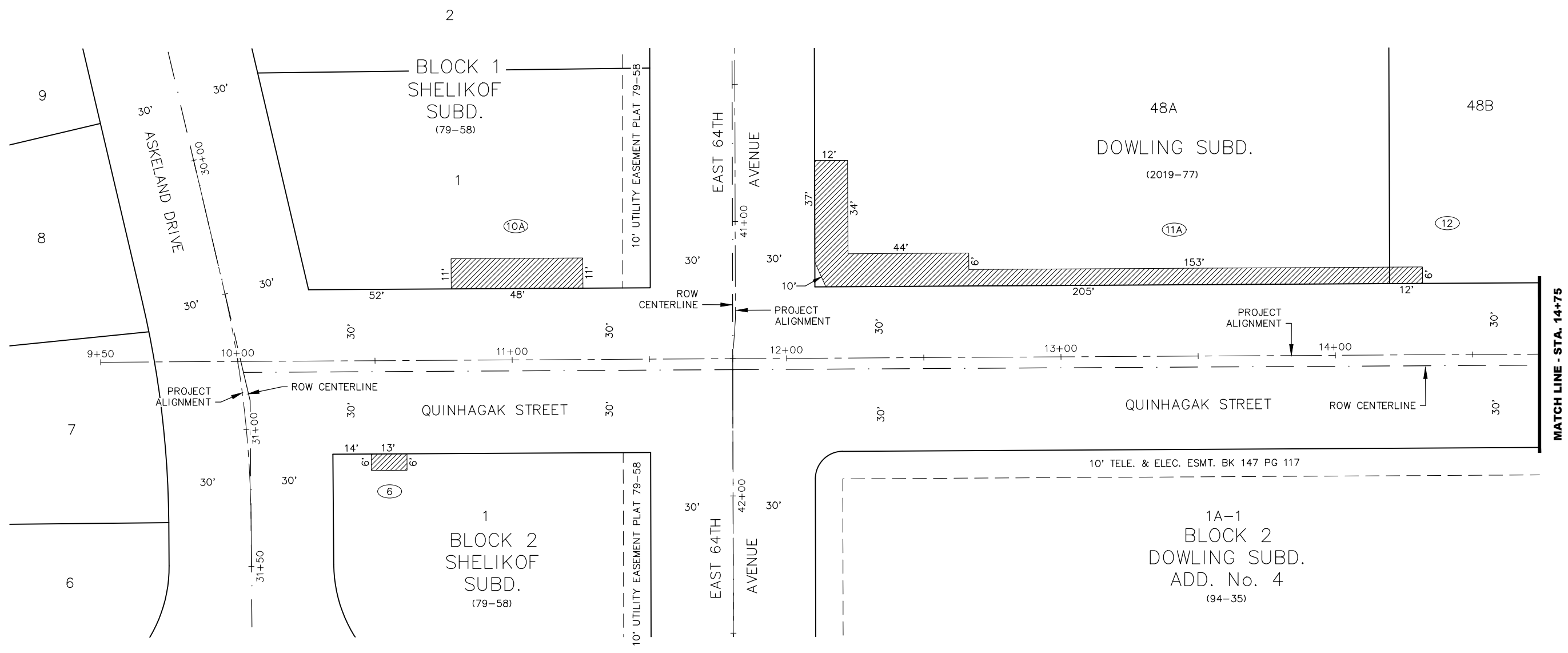
RECORD DRAWING
 1. DATA PROVIDED BY: _____ TITLE: _____
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 CONTRACTOR: _____
 BY: _____ TITLE: _____ DATE: _____
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DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES/FINAL	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								
PLAN CHECK										
CONSTRUCTION RECORD										
VERTICAL DATUM										
REVISIONS										
CONSULTANT										
SEAL										



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT		
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE	ALL
SURVEY CONTROL		
POINT TABLES		
SCALE	HOR. N/A VER. N/A	GRID 5W2033 DATE DEC 2024 STATUS 95% SHEET
		V2 of V5

File: I:\jobdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\02 Survey\04 Easements\21-13 Quinhagak Temp_Esmt_and_Permi_Map.dwg



MATCH LINE - STA. 14+75

TEMPORARY EASEMENT AND PERMIT TABLE		
PARCEL	LEGAL DESCRIPTION	TYPE
6	SHELIKOF BLK 2 LT 1	TCP
10A	SHELIKOF BLK 1 LT 1	TCP
11A	DOWLING LT 48A	TCP
12	DOWLING LT 48B	TCP



- LEGEND**
- 1 PARCEL NUMBER
 - TEMPORARY CONSTRUCTION PERMIT (TCP)
 - TEMPORARY CONSTRUCTION EASEMENT (TCE)

TEMPORARY CONSTRUCTION PERMITS (TCP) ARE DIMENSIONED ON THIS SHEET. TEMPORARY CONSTRUCTION EASEMENTS (TCE) ARE DIMENSIONED ON A SEPARATE PARCEL MAP EXHIBIT.

RECORD DRAWING
 1. DATA PROVIDED BY: _____ TITLE: _____
 THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
 CONTRACTOR: _____ TITLE: _____ DATE: _____
 BY: _____
 2. DATA TRANSFERRED BY: _____ TITLE: _____
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DESIGN	RB	JK								
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MUNICIPAL/STATE	RB	JK								



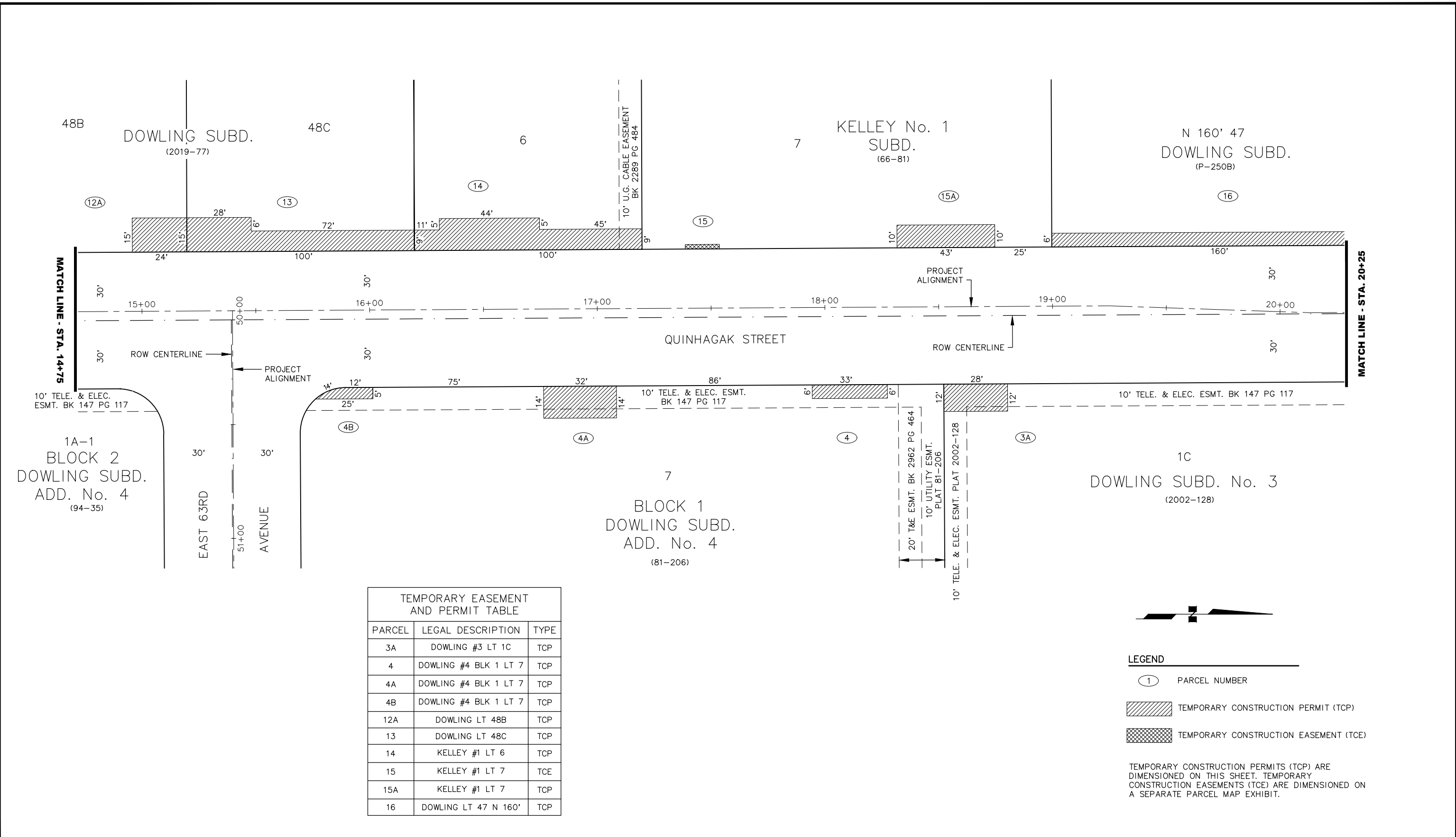
CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECLE82-AK

STATE OF ALASKA
 49 TH
 ANTHONY J. ROBINSON
 LS-12316
 REGISTERED PROFESSIONAL LAND SURVEYOR

UNIVERSITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE ALL
TEMPORARY EASEMENT & PERMIT MAP
 QUINHAGAK STREET STA 9+50 TO 14+75
 SCALE HOR. 1"=20'
 VER. N/A
 GRID SW2033
 DATE DEC 2024 STATUS 95% SHEET V3 of V5

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\02 Survey\04 Easements\21-13 Quinhagak Temp_Esmt_and_Permi_Map.dwg



TEMPORARY EASEMENT AND PERMIT TABLE		
PARCEL	LEGAL DESCRIPTION	TYPE
3A	DOWLING #3 LT 1C	TCP
4	DOWLING #4 BLK 1 LT 7	TCP
4A	DOWLING #4 BLK 1 LT 7	TCP
4B	DOWLING #4 BLK 1 LT 7	TCP
12A	DOWLING LT 48B	TCP
13	DOWLING LT 48C	TCP
14	KELLEY #1 LT 6	TCP
15	KELLEY #1 LT 7	TCE
15A	KELLEY #1 LT 7	TCP
16	DOWLING LT 47 N 160'	TCP



- LEGEND**
- (1) PARCEL NUMBER
 - TEMPORARY CONSTRUCTION PERMIT (TCP)
 - TEMPORARY CONSTRUCTION EASEMENT (TCE)

TEMPORARY CONSTRUCTION PERMITS (TCP) ARE DIMENSIONED ON THIS SHEET. TEMPORARY CONSTRUCTION EASEMENTS (TCE) ARE DIMENSIONED ON A SEPARATE PARCEL MAP EXHIBIT.

RECORD DRAWING
 1. DATA PROVIDED BY: _____ TITLE: _____
 THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
 CONTRACTOR: _____ TITLE: _____ DATE: _____
 BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								



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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE ALL
TEMPORARY EASEMENT & PERMIT MAP
 QUINHAGAK STREET STA 14+75 TO 20+25

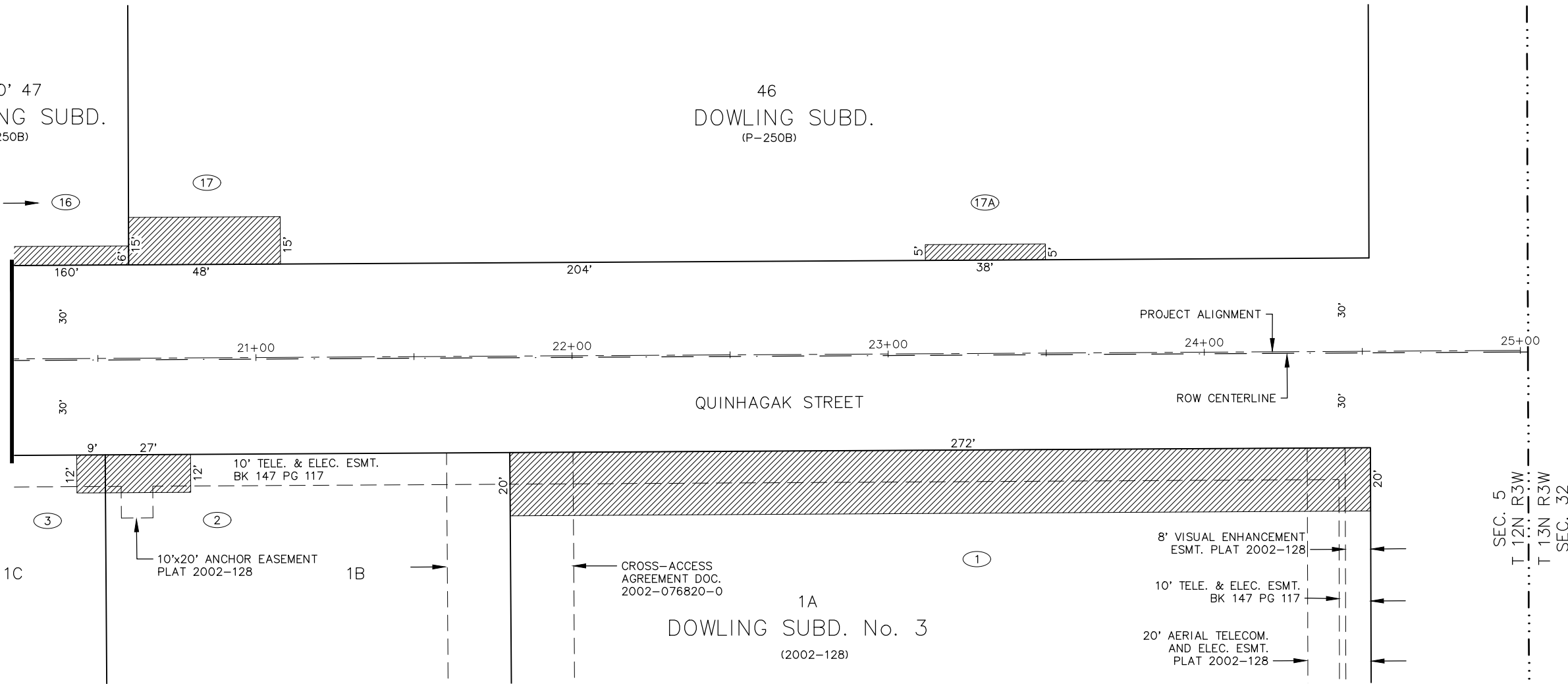
SCALE HOR. 1"=20'
 VER. N/A
 GRID SW2033
 DATE DEC 2024 STATUS 95% SHEET V4 of V5

N 160' 47
DOWLING SUBD.
(P-250B)

46
DOWLING SUBD.
(P-250B)

SEE NOTE 1 → (16)

MATCH LINE - STA. 20+25



TEMPORARY EASEMENT AND PERMIT TABLE		
PARCEL	LEGAL DESCRIPTION	TYPE
1	DOWLING #3 LT 1A	TCP
2	DOWLING #3 LT 1B	TCP
3	DOWLING #3 LT 1C	TCP
17	DOWLING LOT 46	TCP
17A	DOWLING LOT 46	TCP

LEGEND

- (1) PARCEL NUMBER
- [Hatched Box] TEMPORARY CONSTRUCTION PERMIT (TCP)
- [Cross-hatched Box] TEMPORARY CONSTRUCTION EASEMENT (TCE)

NOTES

1. ACQUISITION INFORMATION FOR PARCEL 16 SHOWN ON SHEET V4.

TEMPORARY CONSTRUCTION PERMITS (TCP) ARE DIMENSIONED ON THIS SHEET. TEMPORARY CONSTRUCTION EASEMENTS (TCE) ARE DIMENSIONED ON A SEPARATE PARCEL MAP EXHIBIT.

File: I:\Jobs\10155\00_Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\02_Survey\04_Easements\21-13_Quinhagak Temp_Esmt_and_Permi_Map.dwg

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 1. DATA PROVIDED BY: _____ TITLE: _____
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 CONTRACTOR: _____ DATE: _____
 BY: _____
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 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST

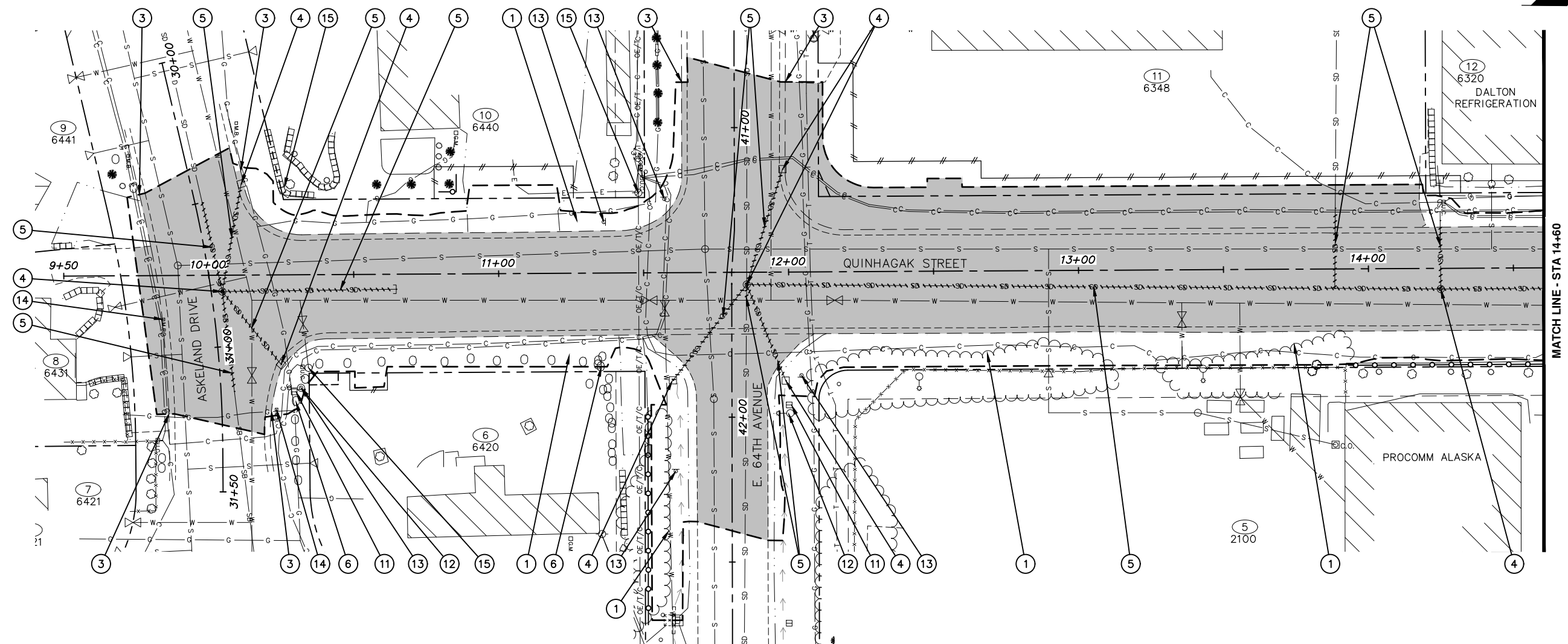


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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION ALL
 E. DOWLING ROAD TO ASKELAND DRIVE
TEMPORARY EASEMENT & PERMIT MAP
 QUINHAGAK STREET STA 20+25 TO 25+00
 SCALE HOR. 1"=20' GRID SW2033
 VER. N/A DATE DEC 2024 STATUS 95% SHEET V5 of V5



LEGEND

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TREE PROTECTION ZONE FENCES (SECTION 75.14) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
 - ③ REMOVE P.C.C. CURB AND GUTTER (SECTION 20.08).
 - ④ REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
 - ⑤ REMOVE PIPE (SECTION 70.07).
 - ⑥ SALVAGE AND RELOCATE OR DISPOSE EXISTING BOULDER AS DIRECTED BY ENGINEER IN THE FIELD (SECTION 75.12).
 - ⑪ REMOVE JUNCTION BOX (SECTION 80.08).
 - ⑫ REMOVE LUMINAIRE POLE (SECTION 80.28).
 - ⑬ REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 85.04).
 - ⑭ RELOCATE MAILBOX (SECTION 85.09).
 - ⑮ PROTECT IN PLACE.
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
 - - - APPROXIMATE LIMITS OF DISTURBANCE.
 - REMOVE PIPE (SECTION 70.07).
 - TREE PROTECTION ZONE FENCING (SECTION 75.14) LOCATIONS TO BE FIELD VERIFIED, SEE MASS DETAIL 75-10.

NOTES:

1. SEE SUMMARY TABLE SHEETS B4-B6 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Demolition Plan.dwg

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
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 BY: _____

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 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

CONTRACTOR

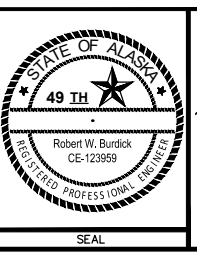
INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST



CRW ENGINEERING GROUP

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 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECLE882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

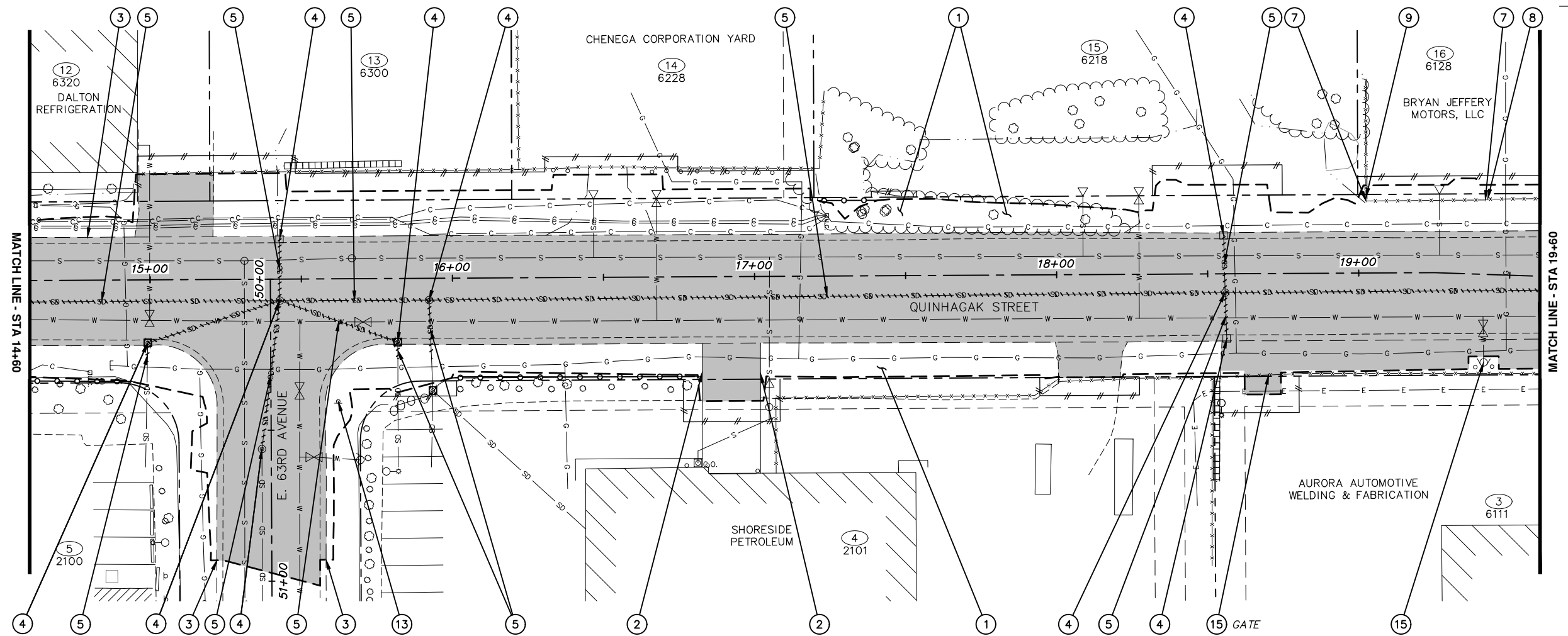
21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE ALL

DEMOLITION PLAN

QUINHAGAK STREET BOP TO STA 14+60

SCALE HOR. 1"=20'
 VER. N/A

GRID SW2033
 DATE DEC 2024 STATUS 95% SHEET B1 of B6



LEGEND

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TREE PROTECTION ZONE FENCES (SECTION 75.14) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
 - ② REMOVE P.C.C. SIDEWALK OR APRON (SECTION 20.07).
 - ③ REMOVE P.C.C. CURB AND GUTTER (SECTION 20.08).
 - ④ REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
 - ⑤ REMOVE PIPE (SECTION 70.07).
 - ⑦ REMOVE AND RESET FENCE (SECTION 75.16).
 - ⑧ REMOVE AND RESET GATE (SECTION 75.16).
 - ⑨ REMOVE FENCE (SECTION 75.16).
 - ⑬ REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 85.04).
 - ⑮ PROTECT IN PLACE
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
 - - - APPROXIMATE LIMITS OF DISTURBANCE.
 - REMOVE PIPE (SECTION 70.07).
 - TREE PROTECTION ZONE FENCING (SECTION 75.14) LOCATIONS TO BE FIELD VERIFIED, SEE MASS DETAIL 75-10.

NOTES:

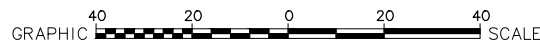
- 1. SEE SUMMARY TABLE SHEETS B4-B6 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
- 2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Demolition Plan.dwg

RECORD DRAWING	
1. DATA PROVIDED BY: _____ TITLE: _____	
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CONTRACTOR: _____	
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TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				



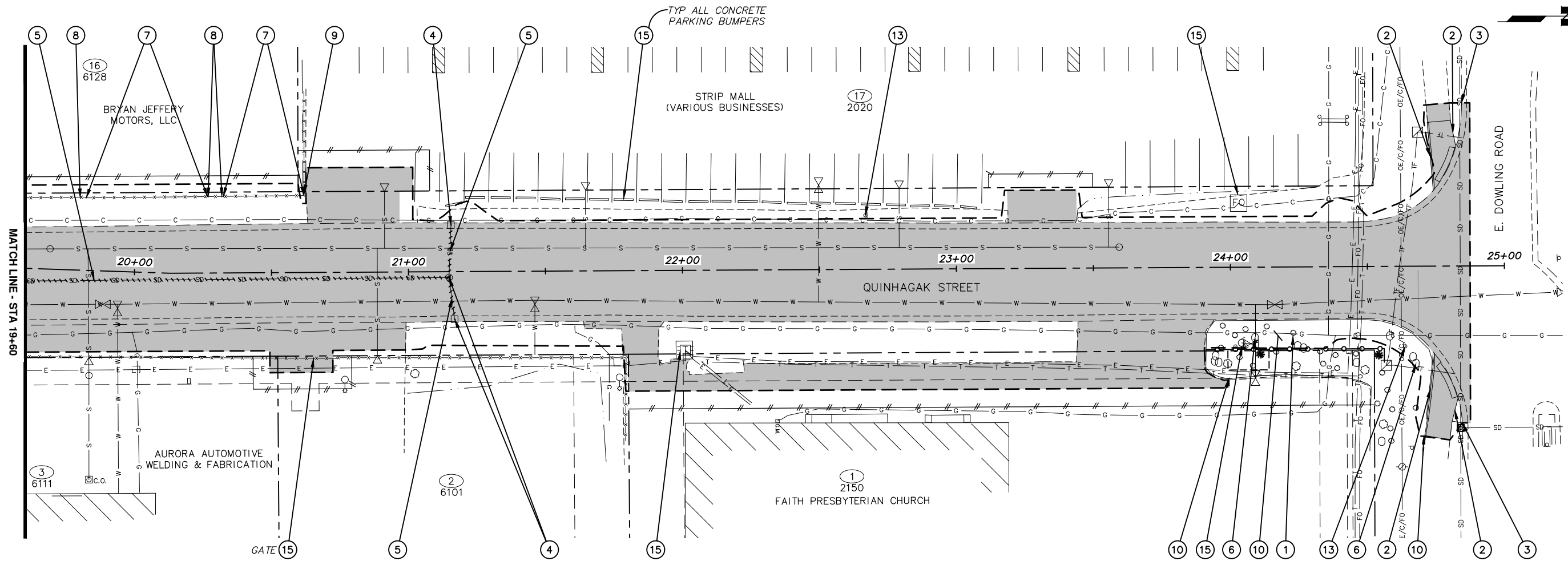
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 PHONE: (907) 562-3252
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STATE OF ALASKA
 49 TH
 Robert W. Burdick
 CE-123959
 REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE ALL
DEMOLITION PLAN
 QUINHAGAK STREET STA 14+60 TO STA 19+60
 SCALE HOR. 1"=20' GRID SW2033
 VER. N/A DATE DEC 2024 STATUS 95% SHEET B2 of B6

MATCH LINE - STA 19+60



LEGEND

- ① CLEAR AND GRUB WITHIN LIMITS OF DISTURBANCE AFTER CLEARING LIMITS HAVE BEEN APPROVED AND AFTER TREE PROTECTION ZONE FENCES (SECTION 75.14) HAVE BEEN ESTABLISHED AS SHOWN, OR AS DIRECTED BY THE ENGINEER IN THE FIELD (SECTION 20.04). NOT ALL TREES, SHRUBS, AND VEGETATION ARE SPECIFICALLY CALLED OUT OR SHOWN.
 - ② REMOVE P.C.C. SIDEWALK OR APRON (SECTION 20.07).
 - ③ REMOVE P.C.C. CURB AND GUTTER (SECTION 20.08).
 - ④ REMOVE MANHOLE OR CATCH BASIN (SECTION 55.11).
 - ⑤ REMOVE PIPE (SECTION 70.07).
 - ⑥ SALVAGE AND RELOCATE OR DISPOSE EXISTING BOULDER AS DIRECTED BY ENGINEER IN THE FIELD (SECTION 75.12).
 - ⑦ REMOVE AND RESET FENCE (SECTION 75.16).
 - ⑧ REMOVE AND RESET GATE (SECTION 75.16).
 - ⑨ REMOVE FENCE (SECTION 75.16).
 - ⑩ REMOVAL/DISPOSAL AND/OR SALVAGE/INSTALLATION OF OBSTRUCTIONS (SECTION 75.18).
 - ⑬ REMOVE AND SALVAGE SIGN. THIS WORK SHALL BE INCIDENTAL TO THE BID ITEM STANDARD SIGNS (SECTION 85.04).
 - ⑮ PROTECT IN PLACE
- REMOVAL OF PAVEMENT (SECTION 20.09) AND/OR, SIDEWALK, CURB & GUTTER, AND CONCRETE, AS SHOWN & NOTED IN SUMMARY TABLES.
 - APPROXIMATE LIMITS OF DISTURBANCE.
 - REMOVE PIPE (SECTION 70.07).
 - TREE PROTECTION ZONE FENCING (SECTION 75.14)
LOCATIONS TO BE FIELD VERIFIED, SEE MASS DETAIL 75-10.

NOTES:

1. SEE SUMMARY TABLE SHEETS B4-B6 FOR STATION AND OFFSET OF DEMOLITION ITEMS.
2. SEE ROADWAY IMPROVEMENTS (R) SHEETS FOR DRIVEWAY RECONSTRUCTION LIMITS.

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\01 Civil\10155.00 Demolition Plan.dwg

RECORD DRAWING

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STORM SEWER	JM	JH
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MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

BASIS OF THIS DATUM GAAB 1972 ADJUST

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE ALL

DEMOLITION PLAN

QUINHAGAK STREET STA 19+60 TO EOP

SCALE HOR. 1"=20'
 VER. N/A

GRID SW2033
 DATE DEC 2024 STATUS 95% SHEET B3 of B6

20.07						
REMOVE P.C.C. SIDEWALK OR APRON ②						
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	AREA (SY)	REMARKS
B2	16+82.5	31.6 RT	17+01.8	32.1 RT	41	PARCEL 4 DRIVEWAY
B3	24+72.8	44.7 RT	24+81.7	52.6 RT	26	
B3	24+73.2	40.0 LT	24+81.5	48.7 LT	19	


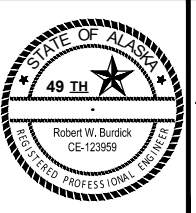

20.08						
REMOVE P.C.C. CURB AND GUTTER ③						
SHEET	APPX STATION BEGIN	APPX OFFSET (FT)	APPX STATION END	APPX OFFSET (FT)	LENGTH (FT)	REMARKS
B1	30+42.4	18.0 RT	31+22.5	18.3 RT	77	ASKELAND DRIVE
B1	10+11.2	36.0 LT	11+63.5	65.7 LT	208	QUINHAGAK STREET / E. 64TH AVE
B1	10+21.4	47.1 RT	14+60.0	21.5 RT	454	QUINHAGAK STREET
B1	11+99.2	65.6 LT	14+60.0	14.3 LT	304	QUINHAGAK STREET / E. 64TH AVE
B2	14+60.0	21.5 RT	15+21.5	93.0 RT	125	QUINHAGAK STREET / E. 63RD AVE
B2	14+60.0	14.3 LT	19+60.0	15.1 LT	501	QUINHAGAK STREET
B2	15+58.0	93.0 RT	19+60.0	20.8 RT	464	QUINHAGAK STREET / E. 63RD AVE
B3	19+60.0	20.8 RT	24+84.3	57.3 RT	547	QUINHAGAK STREET / E. DOWLING ROAD
B3	19+60.0	15.1 LT	24+84.7	59.5 LT	551	QUINHAGAK STREET / E. DOWLING ROAD

20.09				
REMOVE A.C.P.				
SHEET	STATION TO STATION	OFFSET	AREA (SY)	REMARKS
B1	9+75 TO 14+60	LT & RT	2,798	QUINHAGAK STREET, ASKELAND DRIVE, E. 64TH AVENUE, DRIVEWAYS
B2	14+60 TO 19+60	LT & RT	2,279	QUINHAGAK STREET, E. 63RD AVENUE, DRIVEWAYS
B3	19+60 TO 24+88	LT & RT	2,573	QUINHAGAK STREET, E. DOWLING ROAD, DRIVEWAYS

- REMOVE A.C.P. NOTES:**
- SEE ROADWAY IMPROVEMENT SHEETS FOR ROADWAY PAVEMENT REMOVAL LIMITS.
 - SEE DRIVEWAY RECONSTRUCTION TABLE ON SHEET T1 FOR DRIVEWAY PAVEMENT REMOVAL LIMITS.

55.11					
REMOVE MANHOLE OR CATCH BASIN ④					
SHEET	APPX STATION	APPX OFFSET (FT)	CATCH BASIN	MANHOLE	REMARKS
B1	10+04.7	5.6 RT		X	ASKELAND DRIVE / QUINHAGAK STREET
B1	10+11.4	31.5 LT	X		ASKELAND DRIVE
B1	10+24.8	30.9 RT	X		ASKELAND DRIVE
B1	11+59.9	37.2 RT	X		E. 64TH AVENUE
B1	11+85.5	4.3 RT		X	QUINHAGAK STREET
B1	11+98.1	35.5 LT	X		E. 64TH AVENUE
B1	11+98.9	37.5 RT	X		E. 64TH AVENUE
B1	14+24.9	7.1 RT		X	QUINHAGAK STREET
B2	14+99.1	20.9 RT		X	QUINHAGAK STREET
B2	15+36.7	56.3 RT		X	QUINHAGAK STREET
B2	15+42.8	7.1 RT		X	QUINHAGAK STREET
B2	15+42.9	13.3 LT	X		QUINHAGAK STREET
B2	15+81.8	21.0 RT		X	QUINHAGAK STREET
B2	15+92.3	7.2 RT		X	QUINHAGAK STREET
B2	18+55.3	12.7 LT	X		QUINHAGAK STREET
B2	18+55.7	6.2 RT		X	QUINHAGAK STREET
B2	18+56.2	21.4 RT	X		QUINHAGAK STREET
B3	21+14.8	2.2 RT		X	QUINHAGAK STREET
B3	21+15.6	17.0 LT	X		QUINHAGAK STREET
B3	21+16.8	17.4 RT	X		QUINHAGAK STREET

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Demolition Summary Tables.dwg

RECORD DRAWING 1. DATA PROVIDED BY: _____ TITLE: _____ THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED. CONTRACTOR: _____ TITLE: _____ DATE: _____ BY: _____		<table border="1"> <tr> <th>DATA</th> <th>DRAWN BY</th> <th>CHECKED BY</th> </tr> <tr> <td>BASE</td> <td>CB</td> <td>BW</td> </tr> <tr> <td>TOPOGRAPHY</td> <td>CB</td> <td>BW</td> </tr> <tr> <td>PROFILE</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>STORM SEWER</td> <td>JM</td> <td>JH</td> </tr> <tr> <td>WATER/SANITARY SEWER</td> <td>CK</td> <td>JK</td> </tr> <tr> <td>GAS</td> <td>CB</td> <td>BW</td> </tr> <tr> <td>TELEPHONE</td> <td>CB</td> <td>BW</td> </tr> <tr> <td>ELECTRIC</td> <td>JH</td> <td>TK</td> </tr> <tr> <td>DESIGN</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>QUANTITIES</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>PRELIMINARY/FINAL</td> <td>RB</td> <td>JK</td> </tr> <tr> <td>MUNICIPAL/STATE</td> <td>RB</td> <td>JK</td> </tr> </table>		DATA	DRAWN BY	CHECKED BY	BASE	CB	BW	TOPOGRAPHY	CB	BW	PROFILE	RB	JK	STORM SEWER	JM	JH	WATER/SANITARY SEWER	CK	JK	GAS	CB	BW	TELEPHONE	CB	BW	ELECTRIC	JH	TK	DESIGN	RB	JK	QUANTITIES	RB	JK	PRELIMINARY/FINAL	RB	JK	MUNICIPAL/STATE	RB	JK	<table border="1"> <tr> <th>FIELD BOOKS</th> <th>BM NO.</th> <th>LOCATION</th> <th>ELEV.</th> <th>REV</th> <th>DATE</th> <th>DESCRIPTION</th> <th>BY</th> </tr> <tr> <td>DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830</td> <td>GAAB 22</td> <td>See MOA Benchmark Book, Page D-29</td> <td>162.82'</td> <td></td> <td></td> <td></td> <td></td> </tr> <tr> <td></td> <td>GAAB 20</td> <td>See MOA Benchmark Book, Page D-35</td> <td>183.44'</td> <td></td> <td></td> <td></td> <td></td> </tr> </table>		FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'						GAAB 20	See MOA Benchmark Book, Page D-35	183.44'					 <p>3940 ARCTIC BLVD. SUITE 300 ANCHORAGE, ALASKA 99503 PHONE: (907) 562-3252 #AEC1882-AK</p>						PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE ALL DEMOLITION SUMMARY TABLES		SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET B4 of B6	
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	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'																																																																											

70.07

REMOVE PIPE

⑤

SHEET	APPX STA BEGIN	APPX OFFSET (FT)	APPX STA END	APPX OFFSET (FT)	SIZE (INCH)	LENGTH (FT)	REMARKS
B1	9+97.9	25.0 LT	10+04.7	5.6 RT	18	31.4	
B1	10+04.7	5.6 RT	10+08.6	40.1 RT	10	34.7	
B1	10+04.7	5.6 RT	10+11.4	31.5 LT	10	37.7	
B1	10+04.7	5.6 RT	10+24.8	30.9 RT	10	32.3	
B1	10+04.7	5.6 RT	10+64.7	5.2 RT	15	60.0	
B1	11+85.5	4.3 RT	11+59.9	37.2 RT	12	41.7	
B1	11+85.5	4.3 RT	11+85.5	8.3 RT	36	4.0	
B1	11+85.5	4.3 RT	11+98.9	37.5 RT	12	35.8	
B1	11+85.5	4.3 RT	11+98.1	35.5 LT	10	41.7	
B1	11+85.5	4.3 RT	14+24.9	7.1 RT	12	239.4	
B1	13+88.4	6.7 RT	13+88.7	21.0 LT	12	27.6	
B1	14+24.9	7.1 RT	14+24.8	16.5 LT	12	23.6	
B1/B2	14+24.9	7.1 RT	15+42.8	7.1 RT	12	117.9	
B2	14+99.0	25.7 RT	14+99.1	20.9 RT	10	4.8	
B2	14+99.1	20.9 RT	15+42.8	7.1 RT	10	45.9	
B2	15+36.7	56.3 RT	15+42.8	7.1 RT	18	49.5	
B2	15+42.8	7.1 RT	15+42.9	13.3 LT	10	20.4	
B2	15+42.8	7.1 RT	15+81.8	21.0 RT	10	41.4	
B2	15+42.8	7.1 RT	15+92.3	7.2 RT	12	49.5	
B2	15+81.8	21.0 RT	15+81.8	26.5 RT	10	5.4	
B2	15+92.3	7.2 RT	15+93.0	26.5 RT	12	19.3	
B2	15+92.3	7.2 RT	18+55.7	6.2 RT	12	263.5	
B2	18+55.3	12.7 LT	18+55.7	6.2 RT	10	19.0	
B2	18+55.7	6.2 RT	18+56.2	21.4 RT	10	15.2	
B2/B3	18+55.7	6.2 RT	21+14.8	2.2 RT	12	259.0	
B3	21+14.8	2.2 RT	21+15.6	17.0 LT	10	19.1	
B3	21+14.8	2.2 RT	21+16.8	17.4 RT	10	15.4	

75.12

SALVAGE AND RELOCATE OR DISPOSE EXISTING BOULDER

⑥

SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	10+29.8	43.6 RT	
B1	10+33.5	36.2 RT	
B1	10+38.0	30.7 RT	
B1	10+47.7	30.5 RT	
B1	10+59.4	30.1 RT	
B1	10+70.2	29.8 RT	
B1	10+81.2	30.3 RT	
B1	10+90.2	29.2 RT	
B1	10+99.2	30.0 RT	
B1	11+08.7	29.6 RT	
B1	11+18.0	29.9 RT	
B1	11+28.9	30.5 RT	
B1	11+34.0	31.5 RT	
B1	11+34.1	30.1 RT	
B1	11+35.0	32.2 RT	
B1	11+35.6	28.8 RT	
B3	24+08.8	25.8 RT	
B3	24+24.4	28.8 RT	
B3	24+26.6	29.6 RT	
B3	24+58.1	25.4 RT	
B3	24+66.5	33.0 RT	
B3	24+67.5	35.5 RT	

75.14

TREE PROTECTION ZONE FENCING

SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	REMARKS
B1	10+79.5	28.4 LT	10+85.5	28.4 LT	6.0	
B1	10+85.5	28.4 LT	10+85.5	34.3 LT	6.0	
B1	11+51.2	46.5 RT	11+51.0	121.5 RT	75.0	
B1	13+92.9	33.3 RT	14+60.0	33.3 RT	67.0	
B2	14+60.0	33.3 RT	14+93.9	33.6 RT	34.0	
B2	16+00.0	33.0 RT	16+80.0	33.0 RT	80.0	
B2	17+21.0	25.0 LT	17+41.0	25.0 LT	20.0	
B3	23+91.0	30.0 RT	24+56.0	30.0 RT	65.0	
B3	24+56.0	30.0 RT	24+56.0	35.0 RT	5.0	

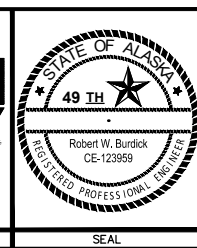
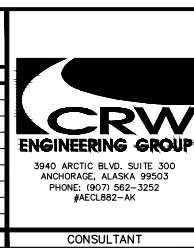
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 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE ALL

DEMOLITION SUMMARY TABLES

SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: B5 of B6

75.16 REMOVE AND RESET FENCE ⑦											
SHEET	EXISTING LOCATION					PROPOSED LOCATION					REMARKS
	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	
B2	19+02.5	23.9 LT	19+41.05	24.6 LT	39.5	19+02.5	26.0 LT	19+41.0	26.7 LT	39.5	CHAIN LINK W/BARBED WIRE
B3	19+81.5	26.5 LT	20+27.1	27.9 LT	44.5	19+81.4	28.3 LT	20+27.1	29.5 LT	44.5	CHAIN LINK W/BARBED WIRE
B3	20+32.1	27.9 LT	20+62.1	28.1 LT	30.0	20+32.1	29.5 LT	20+62.1	29.5 LT	30.0	CHAIN LINK W/BARBED WIRE

- REMOVE AND RESET FENCE NOTES:**
1. PROVIDE TEMPORARY FENCING PER SECTION 75.19 FOR FENCES REMOVED OR AS DIRECTED BY THE ENGINEER.
 2. STAKE RESET FENCE LAYOUT IN THE FIELD FOR ENGINEER TO REVIEW AND APPROVE PRIOR TO INSTALLATION. THIS WORK SHALL BE INCIDENTAL TO SECTION 75.16 PAY ITEM.

75.16 REMOVE AND RESET GATE ⑧											
SHEET	EXISTING LOCATION					PROPOSED LOCATION					REMARKS
	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)	
B1/B2	19+41.0	24.9 LT	19+81.5	26.7 LT	40.5	19+41.0	26.7 LT	19+81.4	28.3 LT	40.5	CHAIN LINK W/BARBED WIRE & SLATS
B2	20+27.1	27.9 LT	20+32.1	27.9 LT	5.0	20+27.1	29.5 LT	20+32.1	29.5 LT	5.0	CHAIN LINK W/BARBED WIRE & SLATS

75.16 REMOVE FENCE ⑨							REMARKS
SHEET	APPX BEGIN STATION	APPX BEGIN OFFSET (FT)	APPX END STATION	APPX END OFFSET (FT)	LENGTH (FT)		
B2	19+02.5	23.9 LT	19+02.5	26.0 LT	2.1		CHAIN LINK W/BARBED WIRE
B3	20+62.1	28.1 LT	20+62.1	29.5 LT	1.4		CHAIN LINK W/BARBED WIRE

75.18 REMOVAL/DISPOSAL AND/OR SALVAGE/INSTALLATION OF OBSTRUCTIONS ⑩						
SHEET	APPX STATION	APPX OFFSET (FT)	OBSTRUCTION ITEM	QUANTITY	ACTION	REMARKS
B3	23+99.2	40.8 RT	LANDSCAPE ROCK EDGING	141.5 LF	PLACE ON PROPERTY	
B3	24+34.6	25.0 RT	LANDSCAPE ROCK MULCH	660.4 SF	SALVAGE AND INSTALL BEHIND PROPOSED RETAINING WALL	

80.08 REMOVE JUNCTION BOX ⑪			
SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	10+29.1	40.5 RT	
B1	12+00.1	45.9 RT	

80.28 REMOVE LUMINAIRE POLE ⑫			
SHEET	APPX STATION	APPX OFFSET (FT)	REMARKS
B1	10+31.5	39.0 RT	
B1	12+00.3	48.7 RT	


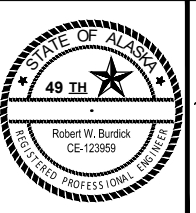

85.04 REMOVE AND SALVAGE SIGN ⑬					
SHEET NO.	APPROX STATION	APPROX OFFSET	SIGN TYPE	LEGEND	SIGN POST
B1	10+32	39.0 RT	D3-101	ASKELAND DR 6400	STREET LIGHT POLE
B1			D3-101	QUINHAGAK ST 6400	
B1			R1-1	STOP	
B1	11+36	17.7 LT	W1-1	TURN	PERFORATED STEEL TUBE
B1			W13-1	ADVISORY SPEED (PLAQUE)	
B1	11+57	25.5 LT	D3-101	E 64TH AVE 2000	PERFORATED STEEL TUBE
B1			D3-101	QUINHAGAK ST 6400	
B1			R1-1	STOP	
B1	11+61	68.5 RT	W14-2	NO OUTLET	PERFORATED STEEL TUBE
B1	12+04	35.7 RT	R1-1	STOP	PERFORATED STEEL TUBE
B2			D3-101	E 63RD AVE 2100	
B2			D3-101	QUINHAGAK ST 6200	
B2	15+62	40.6 RT	R1-1	STOP	PERFORATED STEEL TUBE
B2					
B3	22+67	19.7 LT	R2-1	SPEED LIMIT 25	PERFORATED STEEL TUBE
B3	24+64	29.2 RT	D3-101	DOWLING RD	PERFORATED STEEL TUBE
B3			D3-101	QUINHAGAK ST	
B3			R6-1	ONE WAY RIGHT	
B3			R1-1	STOP	

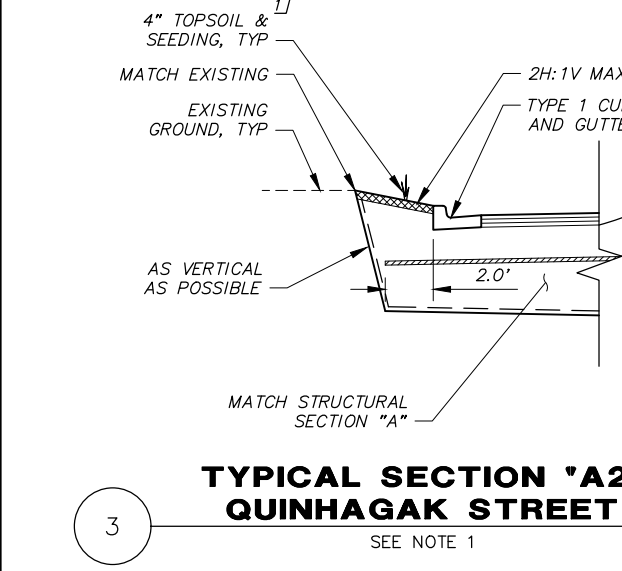
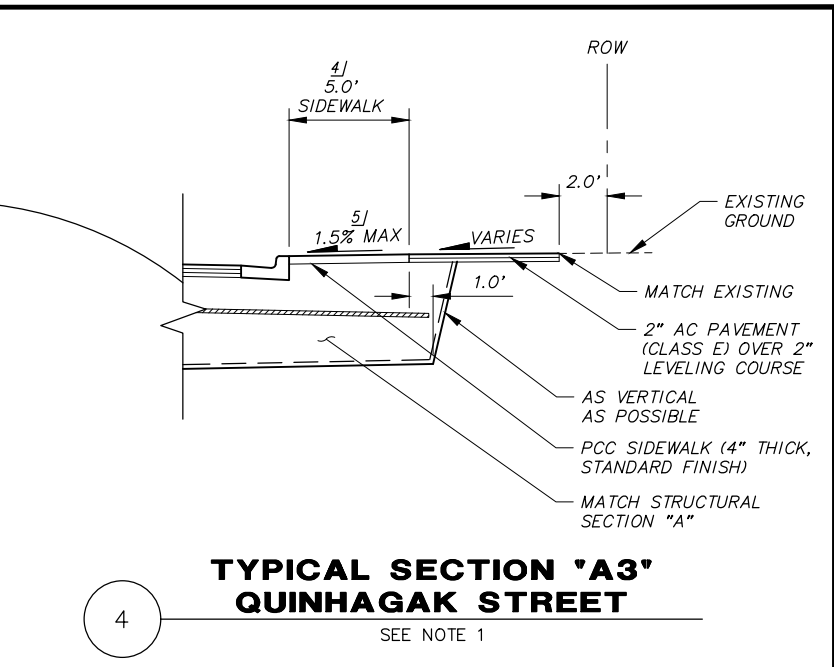
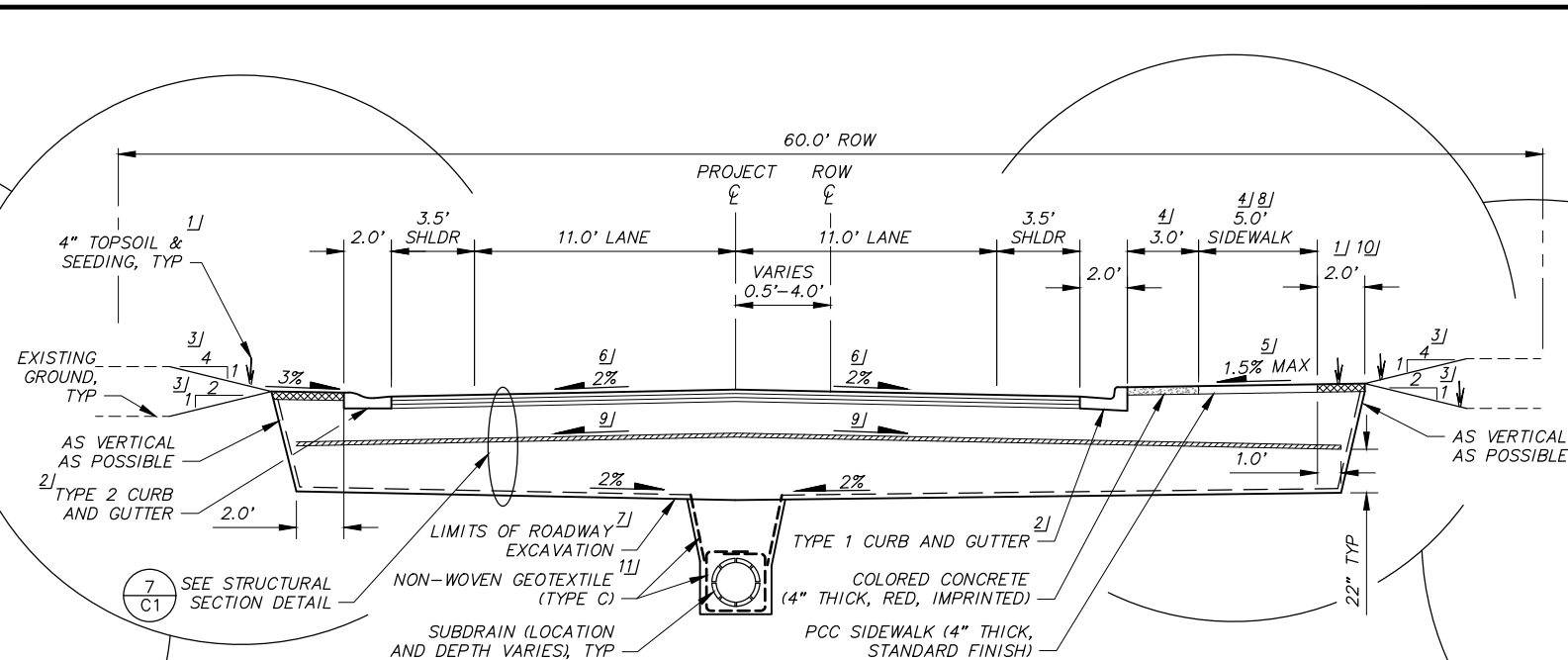
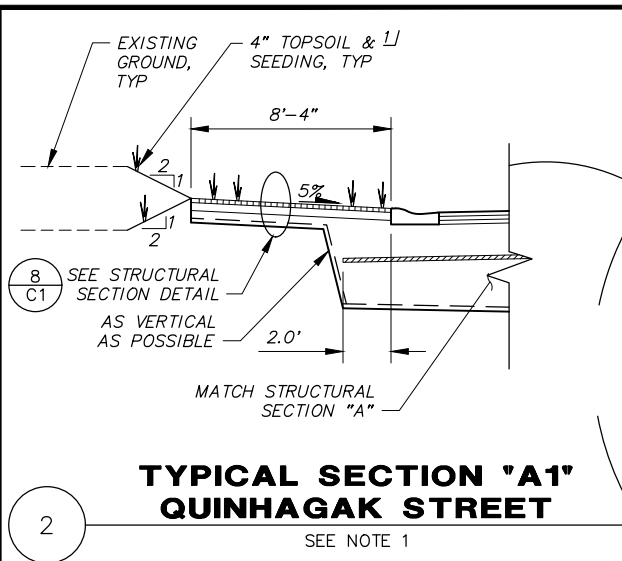
- REMOVE AND SALVAGE SIGN NOTES:**
1. WORK TO REMOVE AND SALVAGE EXISTING SIGNS & POSTS SHALL BE INCIDENTAL TO SECTION 85.04 STANDARD SIGN PAY ITEM.
 2. CONTRACTOR SHALL DELIVER REMOVED SIGN FACES AND ASSOCIATED HARDWARE TO THE MUNICIPAL PAINT AND SIGN SHOP. CONTACT 907-343-4384 TO COORDINATE DELIVERY. DELIVERY OF EXISTING SIGNS IS INCIDENTAL TO SECTION 85.04 STANDARD SIGN PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.

85.09 RELOCATE MAILBOX ⑭					
SHEET	EXISTING LOCATION		NEW LOCATION		REMARKS
	APPX STATION	APPX OFFSET (FT)	APPX STATION	APPX OFFSET (FT)	
B1	9+84.3	15.1 RT	9+83.9	53.7 RT	
B1	10+23.1	46.4 RT	10+24.1	45.5 RT	6 MAILBOXES

- RELOCATE MAILBOX NOTES:**
1. SEE SHEET D6 FOR MAILBOX INSTALLATION DETAILS.

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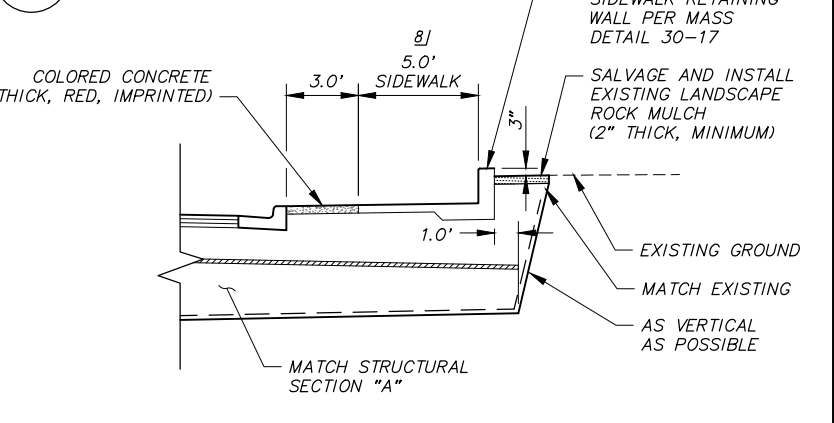
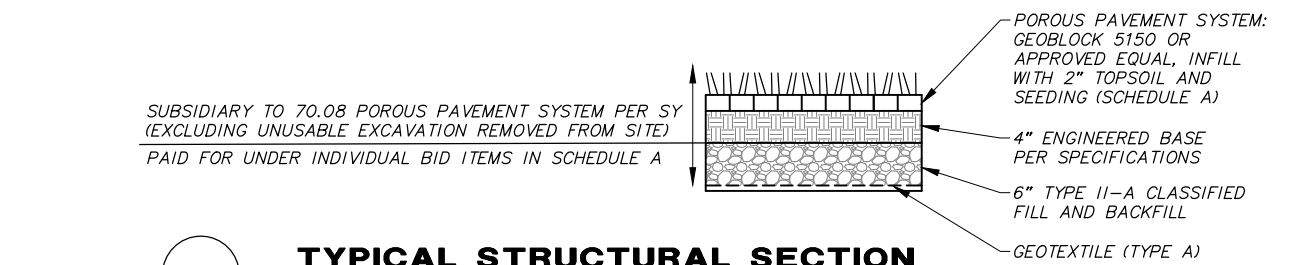
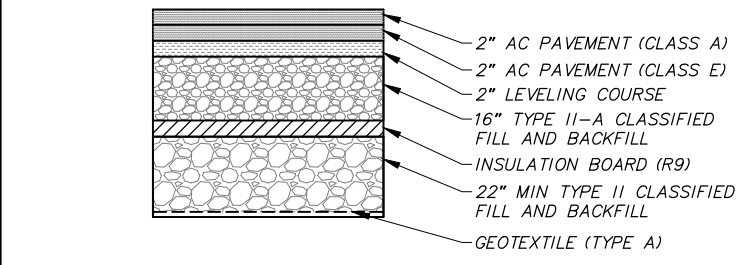
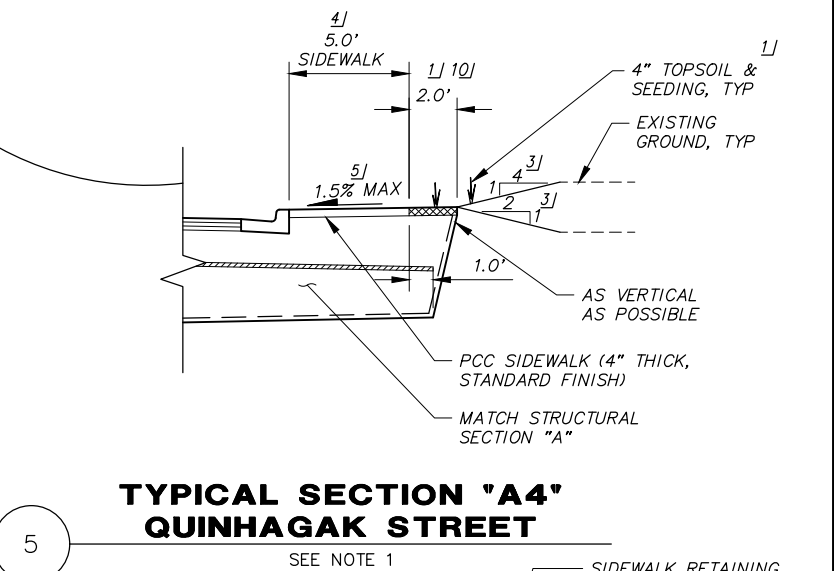


FOOT NOTES:

- PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
- TOP AC PAVEMENT SHALL BE 1/8" - 1/4" ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 5, SHEET C3.
- TYPICAL FILL SLOPES ARE 2 (HORIZONTAL) : 1 (VERTICAL). FILL SLOPES MAY VARY ALONG ROADWAY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY, SEE DETAIL 3, SHEET C3. SEE ROADWAY SHEETS FOR LOCATIONS. CUT SLOPES ARE 4H:1V UNLESS OTHERWISE NOTED IN SPECIAL CUT SLOPE TABLE SHEET C2. THE ENGINEER MAY ADJUST THE TYPICAL SLOPES IN THE FIELD.
- INCREASE SIDEWALK/BUFFER THICKNESS TO 6" ACROSS ALL DRIVEWAYS/PARKING AREAS & ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
- THE MAXIMUM SIDEWALK GRADE IS 2% AT DRIVEWAYS. SIDEWALK GRADE SHALL BE 1% MINIMUM IN ALL CASES.
- ROADWAY CROSS SLOPE SHALL BE 2% UNLESS OTHERWISE NOTED. CROSS SLOPE VARIES FROM STA 24+31 TO 24+44 SEE INTERSECTION LAYOUT SHEET R9 FOR MORE INFORMATION.
- PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
- SIDEWALK WIDTH VARIES AT TRANSITION TO/FROM BUFFER PER DETAIL 3 ON SHEET D5.
- INSULATION SLOPE SHALL MATCH ROADWAY CROSS SLOPE.
- CONSTRUCT SHOULDER PER DETAIL 2, SHEET C2 AT LUMINAIRE LOCATIONS UNLESS OTHERWISE NOTED.
- DELETE NON-WOVEN GEOTEXTILE (TYPE C) AND EXTEND GEOTEXTILE (TYPE A) ALONG THE BOTTOM OF ROADWAY EXCAVATION WHEN PROPOSED STORM DRAIN IS PRESENT OR WHEN NO SUBDRAIN IS PRESENT.

SHEET NOTES:

- SEE TYPICAL SECTION SUMMARY TABLE, SHEET C2, FOR STATION RANGES. THE STATION RANGES ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.



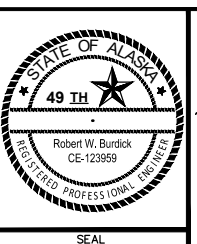
7 TYPICAL STRUCTURAL SECTION
QUINHAGAK STREET

8 TYPICAL STRUCTURAL SECTION
POROUS PAVEMENT SYSTEM

6 TYPICAL SECTION 'A5'
QUINHAGAK STREET
SEE NOTE 1

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PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
PRELIMINARY/FINAL	RB	JK	INSPECTOR							
MUNICIPAL/STATE	RB	JK								
PLAN CHECK			CONSTRUCTION RECORD							
			VERTICAL DATUM							
			REVISIONS							
			CONSULTANT							
			SEAL							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE	SCHED A	
TYPICAL SECTIONS			
SCALE	HOR. N/A VER. N/A	GRID SW2033 DATE DEC 2024	STATUS 95% SHEET C1 of C3

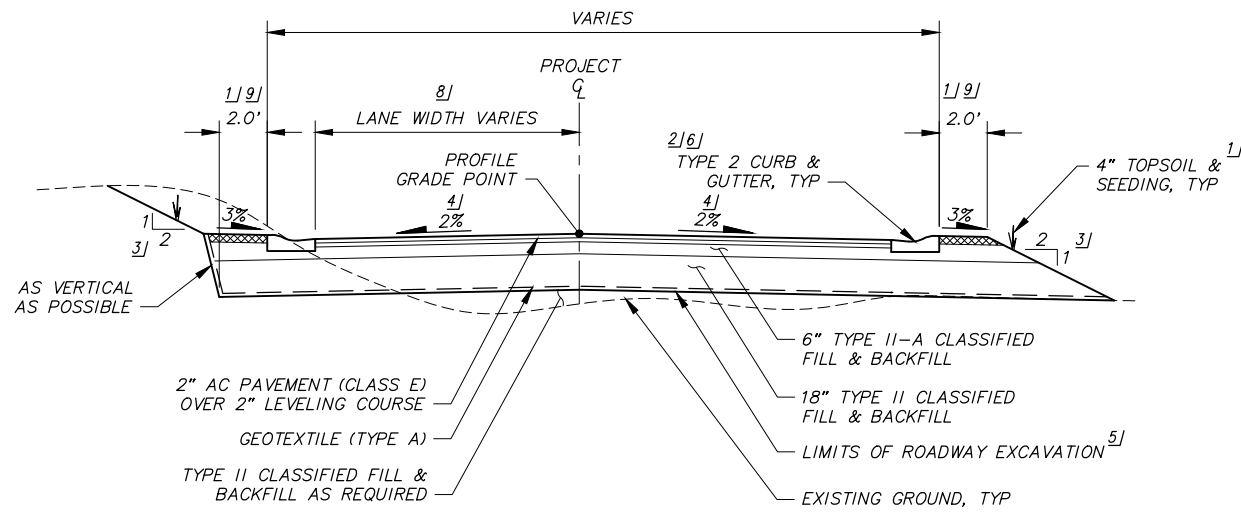
File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Typical Sections.dwg

NOTES:

1. SEE SIDE STREET TYPICAL SECTION SUMMARY TABLE, THIS SHEET. THE STATION RANGES ARE APPROXIMATE AND MAY BE MODIFIED IN THE FIELD BY THE ENGINEER.

#/ FOOT NOTES:

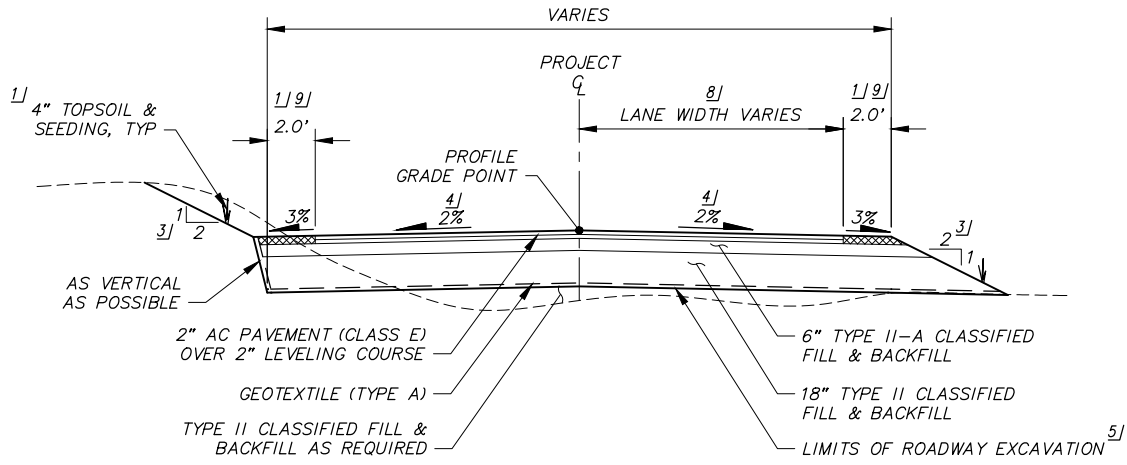
1. PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
2. TOP AC PAVEMENT SHALL BE 1/8" - 1/4" ABOVE LIP OF CURB, UNLESS OTHERWISE NOTED. SEE DETAIL 5, SHEET C3.
3. THE MAXIMUM (STEEPEST) AND TYPICAL CUT/FILL SLOPES ARE 2 (HORIZONTAL) : 1 (VERTICAL). FILL SLOPES MAY VARY ALONG ROADWAY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. SEE DETAIL 3, SHEET C3. SEE ROADWAY SHEETS FOR LOCATIONS. THE ENGINEER MAY ADJUST THE TYPICAL SLOPES IN THE FIELD.
4. ROADWAY CROSS SLOPE SHALL BE 2% UNLESS OTHERWISE NOTED. SEE INTERSECTION LAYOUT SHEETS FOR ROADWAY CROSS SLOPES AT BEGIN/END OF SIDE STREET CURB RETURNS. MODIFY ROADWAY CROSS SLOPE AS REQUIRED TO MATCH INTO EXISTING ROADWAY OR AS DIRECTED IN THE FIELD BY THE ENGINEER. PROVIDE SMOOTH TRANSITION TO MATCH EXISTING AND POSITIVE DRAINAGE TOWARD STORM DRAIN STRUCTURES.
5. PRIOR TO PLACEMENT OF FILL, NATIVE MATERIAL SHALL BE SCARIFIED, PROOF-ROLLED AND COMPACTED AS DIRECTED BY ENGINEER. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
6. INSTALL TYPE 2 CURB & GUTTER UNLESS OTHERWISE NOTED, SEE INTERSECTION LAYOUT SHEETS FOR CURB TYPES ON SIDE STREETS.
7. BEGIN TRANSITION FROM MAIN STREET TYPICAL SECTION TO SIDE STREET TYPICAL SECTION AT END OF SIDE STREET CURB RETURN & INSTALL INSULATION WITHIN SIDE STREET PER DETAIL 1, SHEET D5.
8. SEE ROADWAY PLAN & PROFILE FOR SIDE STREET WIDTHS.
9. CONSTRUCT SHOULDER PER DETAIL 3, THIS SHEET AT LUMINAIRE LOCATIONS.



TYPICAL SECTION 'B' - SIDE STREETS WITH CURB (BEYOND CURB RETURN)

1

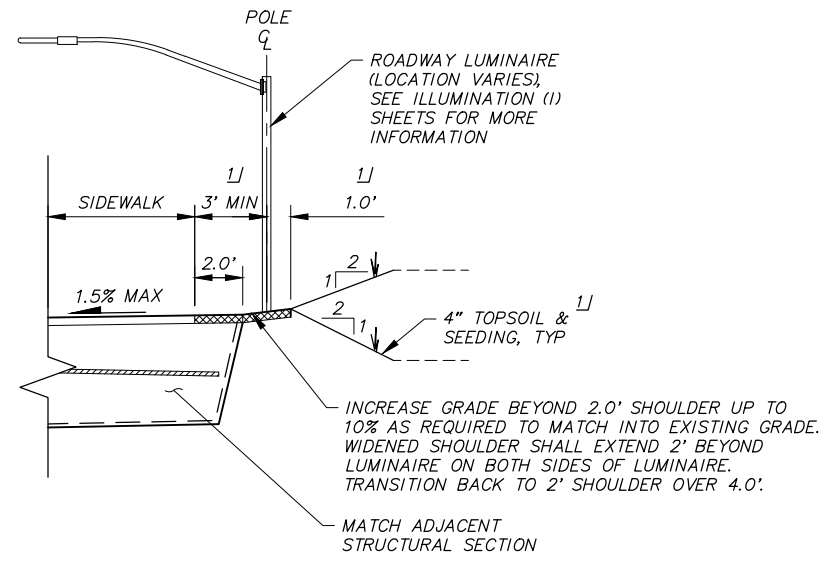
SEE NOTE 1



TYPICAL SECTION 'C' - SIDE STREETS NO CURB (BEYOND CURB RETURN)

2

SEE NOTE 1



TYPICAL SHOULDER SECTION AT LUMINAIRES

3

TYPICAL SECTION TABLE - QUINHAGAK STREET			
FROM STA	TO STA	TYPICAL SECTION	
		LEFT	RIGHT
BOP	15+45	A	A
15+45	17+19	A1	A
17+19	18+50	A	A
18+50	20+99	A	A3
20+99	22+51	A2	A4
22+51	23+79	A2	A
23+79	EOP	A2	A5

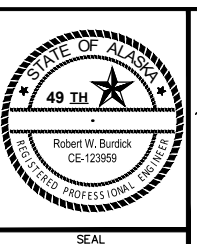
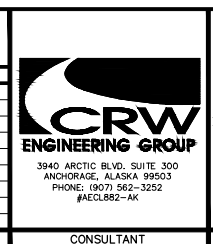
TYPICAL SECTION TABLE - SIDE STREETS			
SIDE STREET	FROM STA	TO STA	TYPICAL SECTION
ASKELAND DRIVE	30+42	31+22	B
64TH AVENUE	40+84	BEGIN CURB RETURN	B
	END CURB RETURN	42+31	B
63RD AVENUE	42+31	42+36	C
	END CURB RETURN	50+93	B

SPECIAL CUT SLOPE TABLE - QUINHAGAK STREET			
FROM STA	TO STA	OFFSET	CUT SLOPE
12+00	15+15	RT	3H:1V
15+45	18+25	LT	2H:1V
20+99	EOP	LT	2H:1V

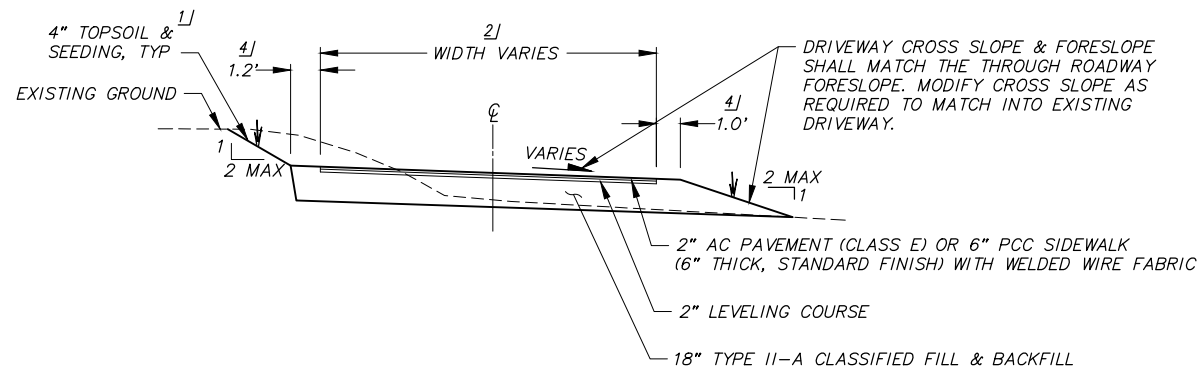
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RECORD DRAWING
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 CONTRACTOR: _____ TITLE: _____ DATE: _____
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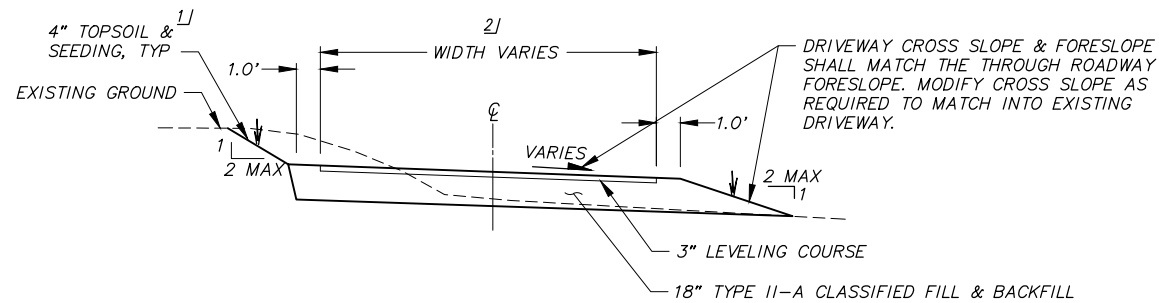
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BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								
PLAN CHECK										
CONSTRUCTION RECORD										
VERTICAL DATUM										
REVISIONS										



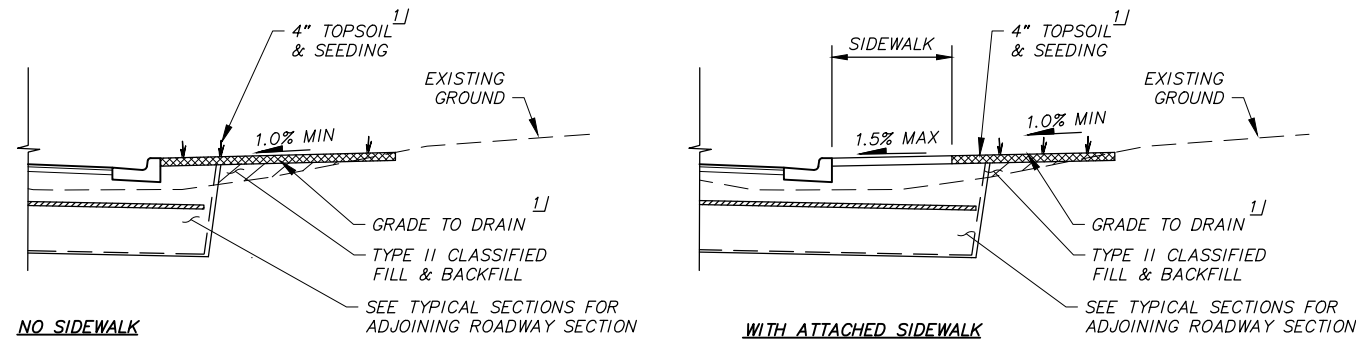
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE	SCHED A	
TYPICAL SECTIONS			
SCALE	HOR. N/A VER. N/A	GRID SW2033 DATE DEC 2024	STATUS 95% SHEET C2 of C3



**TYPICAL SECTION 'D' DRIVEWAY/PARKING AREA
PAVED OR CONCRETE**



TYPICAL SECTION 'E' DRIVEWAY UNPAVED



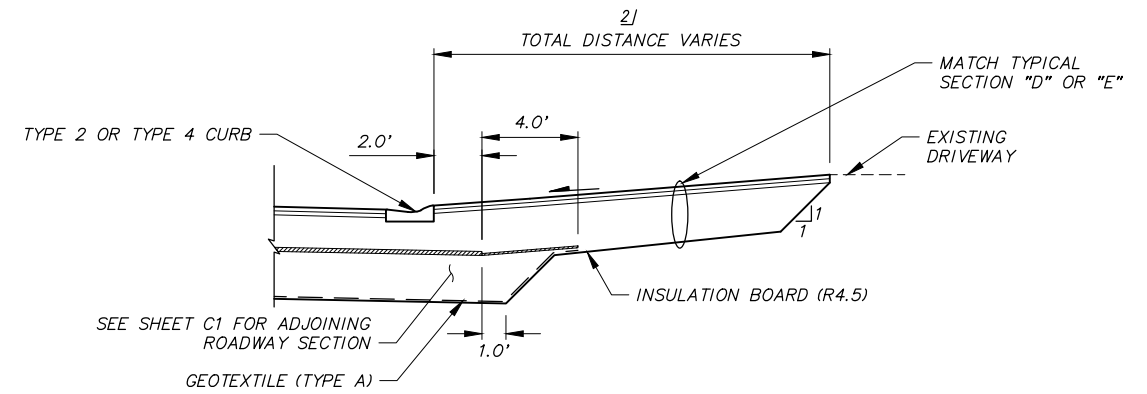
SPECIAL FILL GRADING DETAILS

SHEET NOTES:

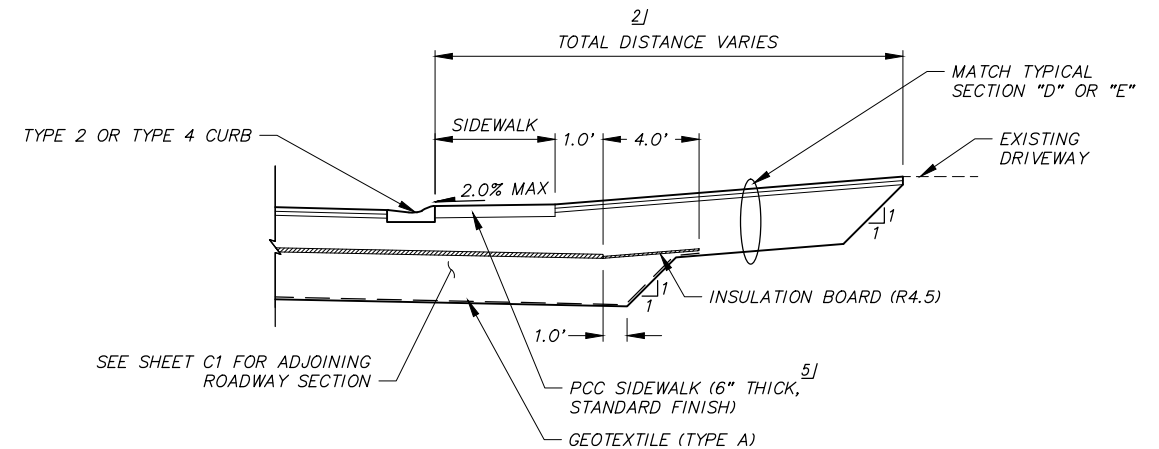
1. SEE SHEETS C1-C2 FOR ADJOINING ROADWAY SECTION.

FOOT NOTES:

- PLACE 4" OF TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS.
- SEE RECONSTRUCT DRIVEWAY SUMMARY TABLE ON THE ROADWAY SUMMARY TABLE (T) SHEETS, DRIVEWAY RECONSTRUCTION PLANS & DRIVEWAY DETAILS FOR DRIVEWAY RECONSTRUCTION INFORMATION.
- INSTALL INSULATION ADJACENT TO DRIVEWAY AND TRANSITION TO DRIVEWAY SECTION PER DETAIL 4, THIS SHEET. INSTALL INSULATION TO THE WIDTH OF DRIVEWAY AT THE BACK OF CURB INCLUDING SHOULDERS.
- 1.0' SHOULDER NOT REQUIRED WHEN DRIVEWAY IS ADJACENT TO PAVED SURFACES.
- ADD WELDED STEEL WIRE REINFORCEMENT TO ALL 6" SIDEWALKS PER THE SPECIFICATIONS.

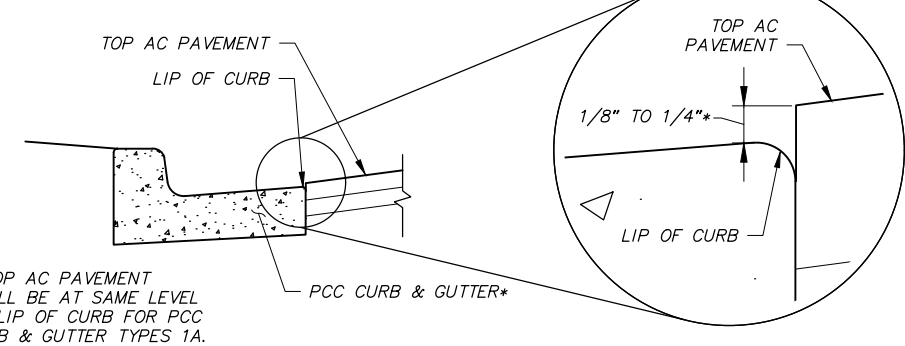


TYPICAL DRIVEWAY CONNECTION SECTION



4

CURB AND GUTTER & AC PAVEMENT EDGE DETAIL



5

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Typical Sections.dwg

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CONTRACTOR: _____ TITLE: _____ DATE: _____

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BY: _____

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BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
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WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
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MUNICIPAL/STATE	RB	JK								
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			CONSULTANT							

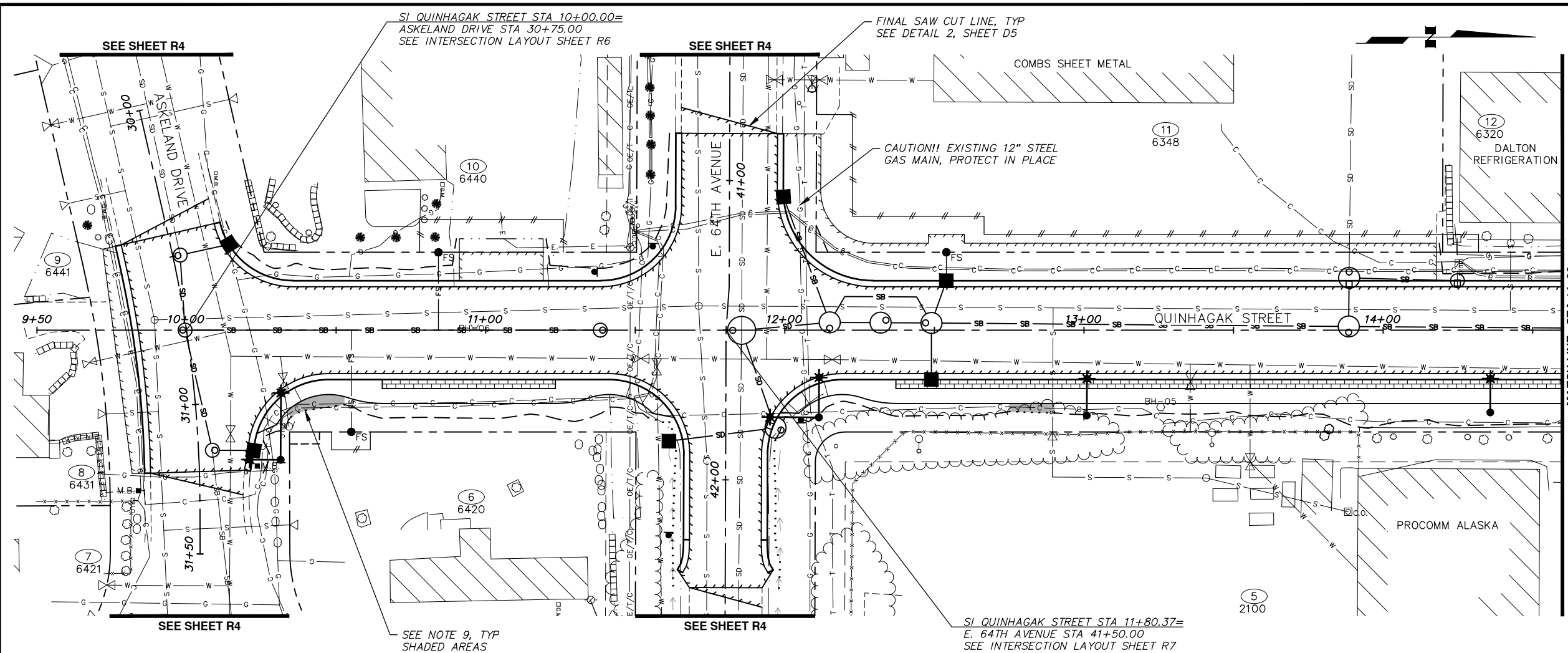
CRW ENGINEERING GROUP

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AECL882-AK

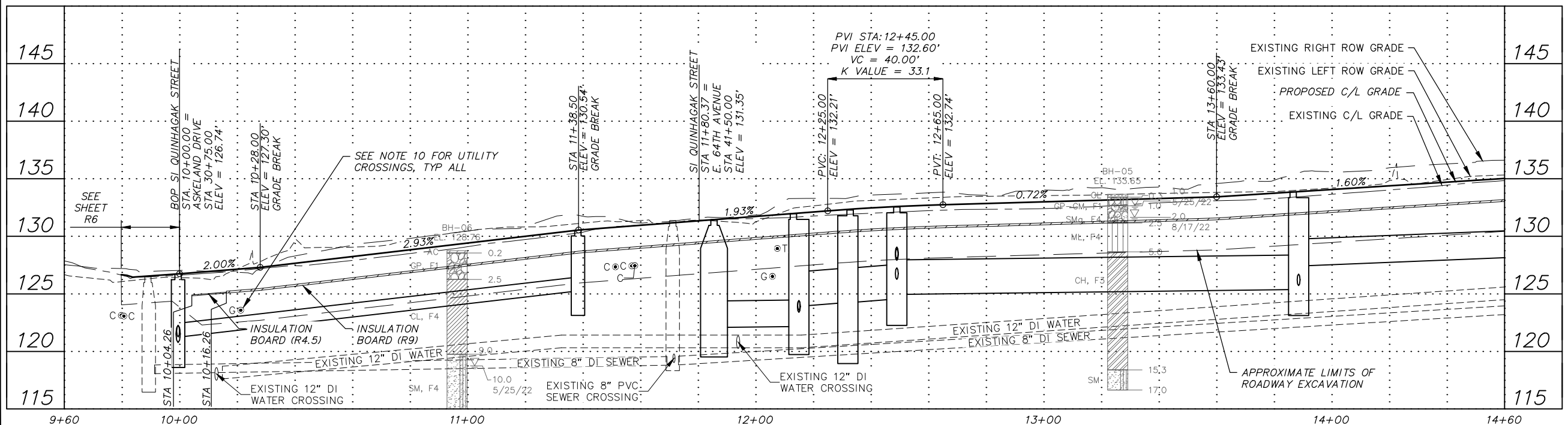
STATE OF ALASKA
49 TH
Robert W. Burdick
CE-123959
REGISTERED PROFESSIONAL ENGINEER

UNIVERSITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT		21-13		QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE		SCHED A	
TYPICAL SECTIONS							
SCALE	HOR. N/A VER. N/A	GRID	SW2033	DATE	DEC 2024	STATUS	95%
						SHEET	C3 of C3



- NOTES:**
- SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
 - SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
 - SEE TYPICAL SECTION (C) SHEETS FOR ROADWAY CROSS SECTIONS.
 - FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
 - SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
 - SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
 - SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
 - THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
 - GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 3, SHEET C3. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.0% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
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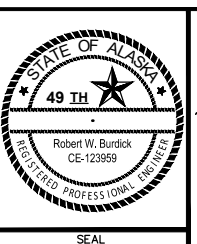
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DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								



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#AEC0882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

ROADWAY PLAN & PROFILE

QUINHAGAK STREET - BOP TO STA 14+60

SCALE: HOR. 1"=20' VER. 1"=5'

GRID: 9W2033 DATE: DEC 2024 STATUS: 95% SHEET: R1 of R11

SI QUINHAGAK STREET STA 15+39.75=
E. 63RD AVENUE STA 50+00.00
SEE INTERSECTION LAYOUT SHEET R8

INSTALL POROUS PAVEMENT
SYSTEM PER TYPICAL SECTION
A1 AND DETAIL 2, SHEET D8

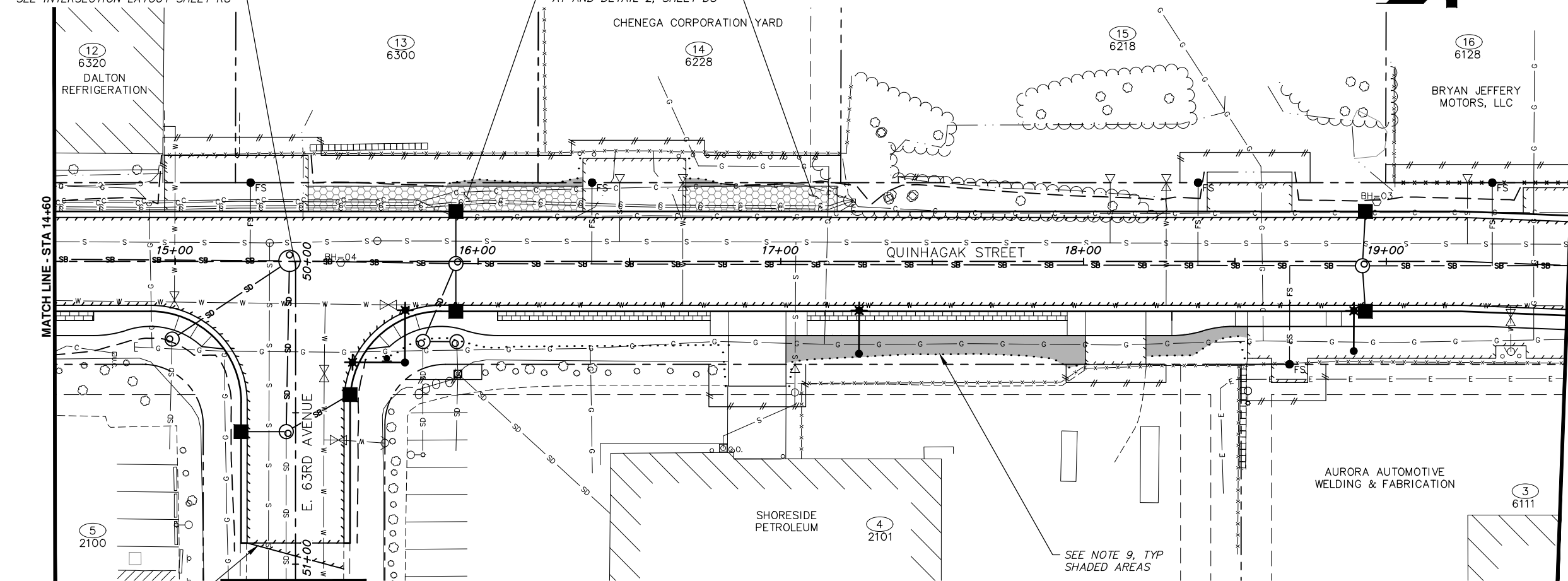
CHENEGA CORPORATION YARD

BRYAN JEFFERY
MOTORS, LLC

AURORA AUTOMOTIVE
WELDING & FABRICATION

SHORESIDE
PETROLEUM

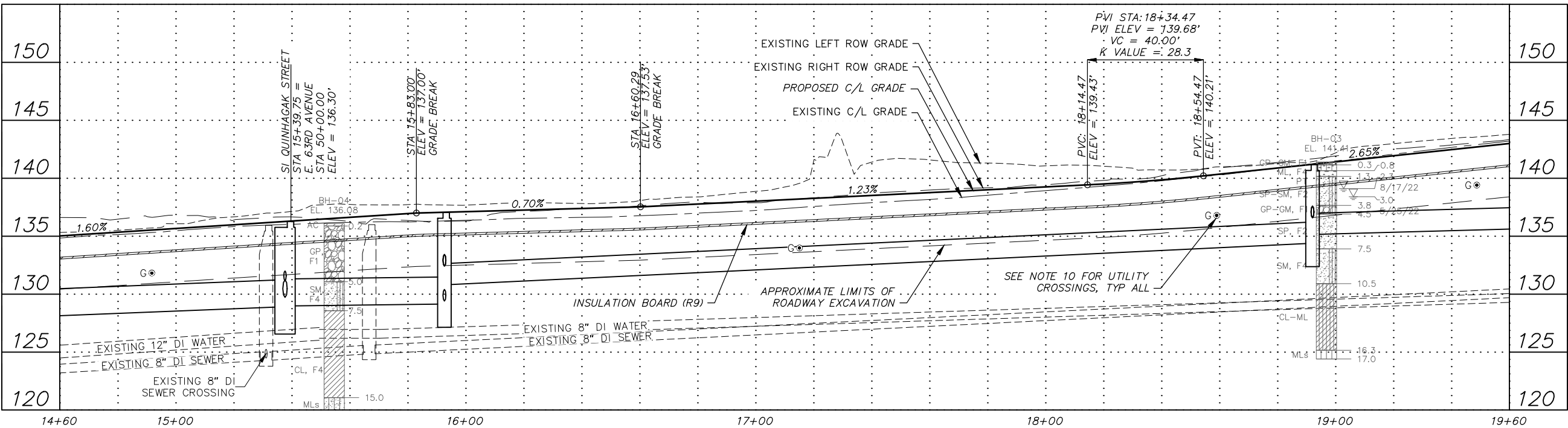
- NOTES:**
- SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
 - SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
 - SEE TYPICAL SECTION (C) SHEETS FOR ROADWAY CROSS SECTIONS.
 - FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
 - SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
 - SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
 - SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
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FINAL SAW CUT LINE, TYP
SEE DETAIL 2, SHEET D5

SEE SHEET R5

SEE NOTE 9, TYP
SHADED AREAS



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RECORD DRAWING

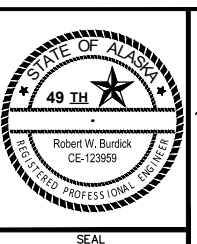
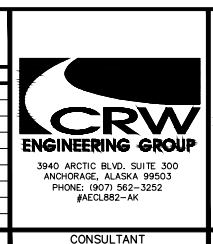
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ELECTRIC	JH	TK							
DESIGN	RB	JK	ASBUILT						
QUANTITIES	RB	JK	CONTRACTOR						
PRELIMINARY/FINAL	RB	JK	INSPECTOR						
MUNICIPAL/STATE	RB	JK							

PLAN CHECK CONSTRUCTION RECORD VERTICAL DATUM REVISIONS CONSULTANT SEAL



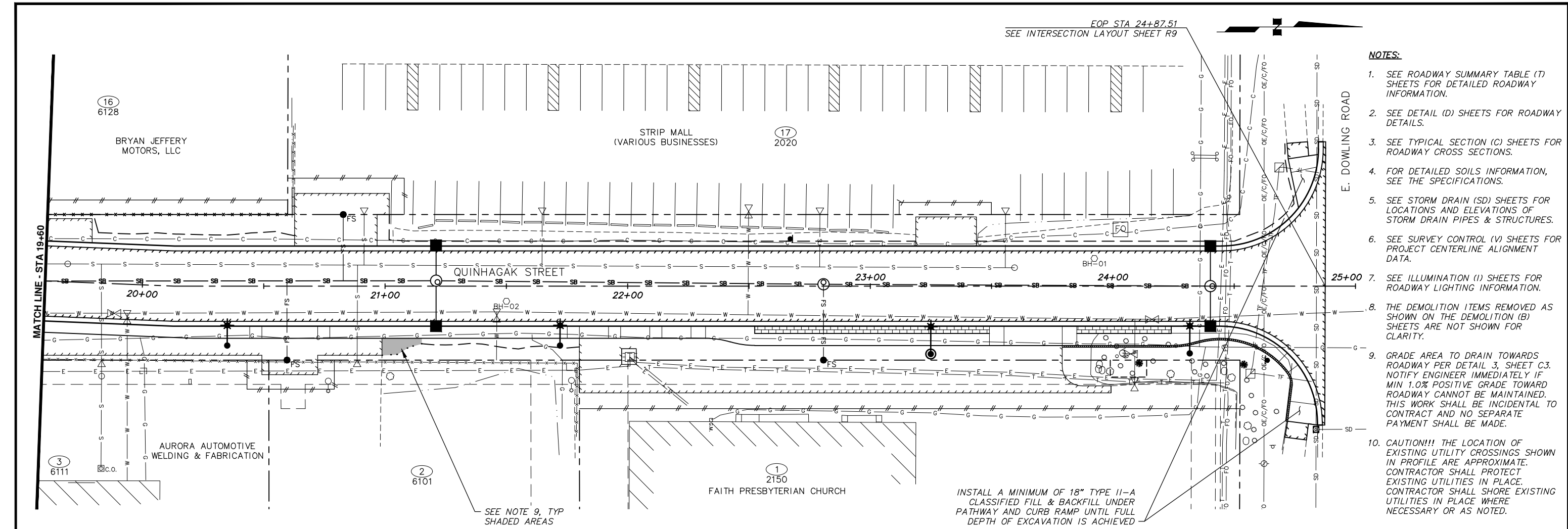
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED A

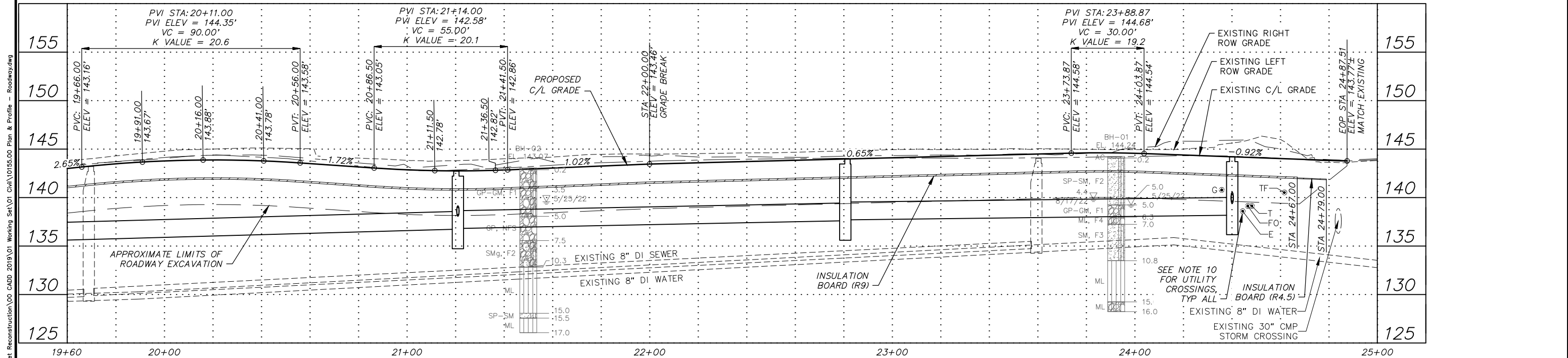
ROADWAY PLAN & PROFILE

QUINHAGAK STREET - STA 14+60 TO STA 19+60

SCALE HOR. 1"=20' VER. 1"=5' GRID 9W2033 DATE DEC 2024 STATUS 95% SHEET R2 of R11



- NOTES:**
- SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
 - SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
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MUNICIPAL/STATE	RB	JK							

CRW ENGINEERING GROUP

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STATE OF ALASKA
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Robert W. Burdick
CE-123959
REGISTERED PROFESSIONAL ENGINEER

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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED A

ROADWAY PLAN & PROFILE

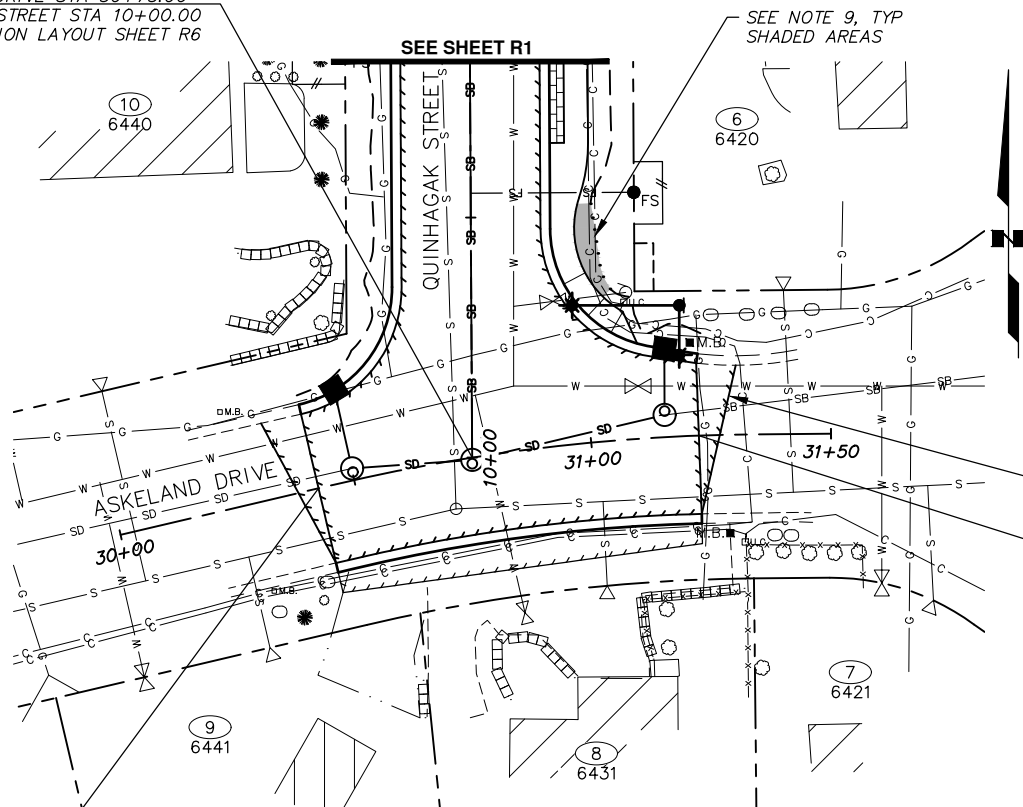
QUINHAGAK STREET - STA 19+60 TO EOP

SCALE HOR. 1"=20'
VER. 1"=5'

GRID 5W2033
DATE DEC 2024
STATUS 95%

SHEET R3 of R11

SI ASKELAND DRIVE STA 30+75.00=
QUINHAGAK STREET STA 10+00.00
SEE INTERSECTION LAYOUT SHEET R6

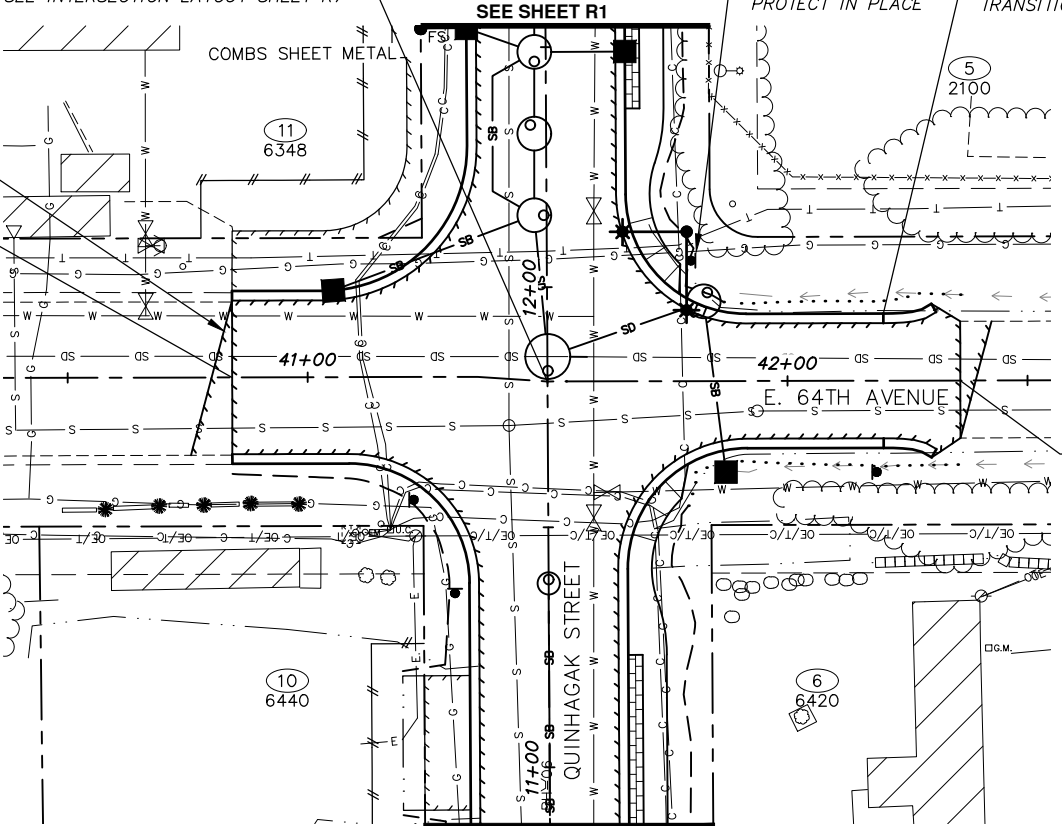


STA 30+42.35
MATCH EXISTING AC PAVEMENT,
17.9' LT & 18.0' RT
MATCH EXISTING C&G

STA 40+84.28
MATCH EXISTING AC PAVEMENT,
18.0' LT & 18.0' RT MATCH EXISTING C&G

STA 31+22.46
MATCH EXISTING AC PAVEMENT,
17.1' LT & 18.3' RT
MATCH EXISTING C&G

SI E. 64TH AVENUE STA 41+50.00=
QUINHAGAK STREET STA 11+80.37
SEE INTERSECTION LAYOUT SHEET R7

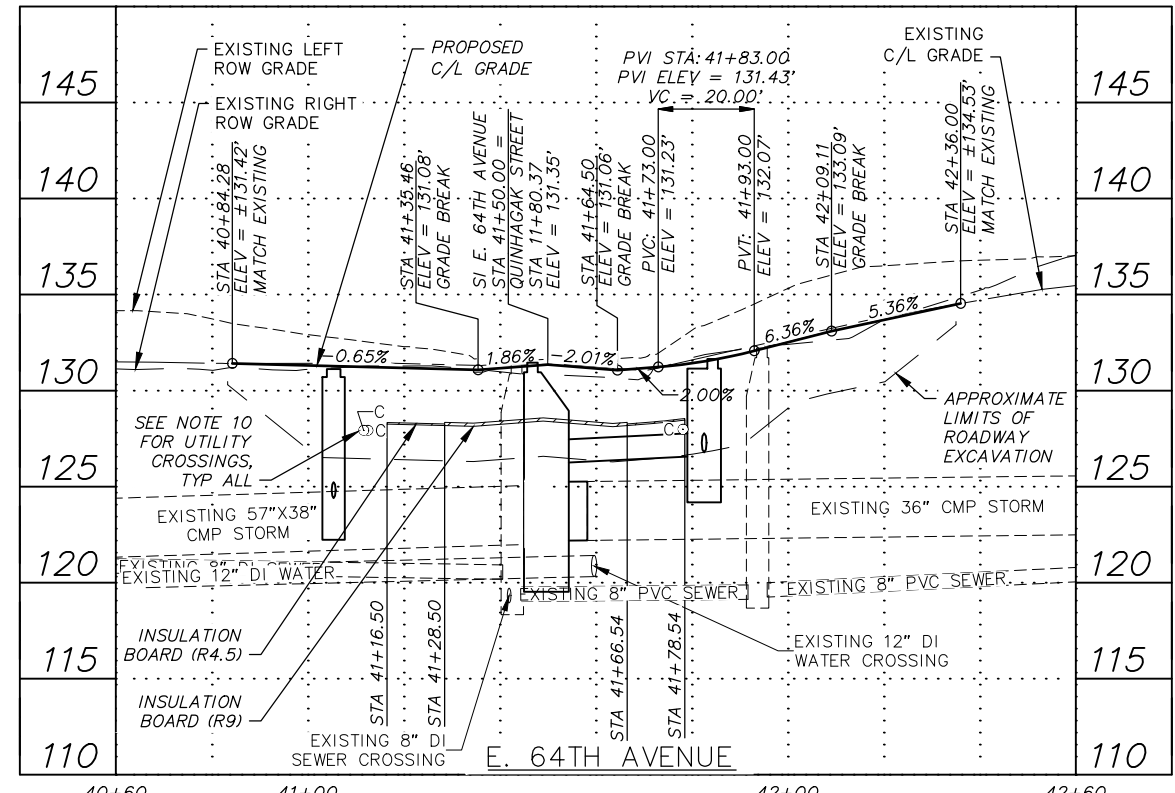
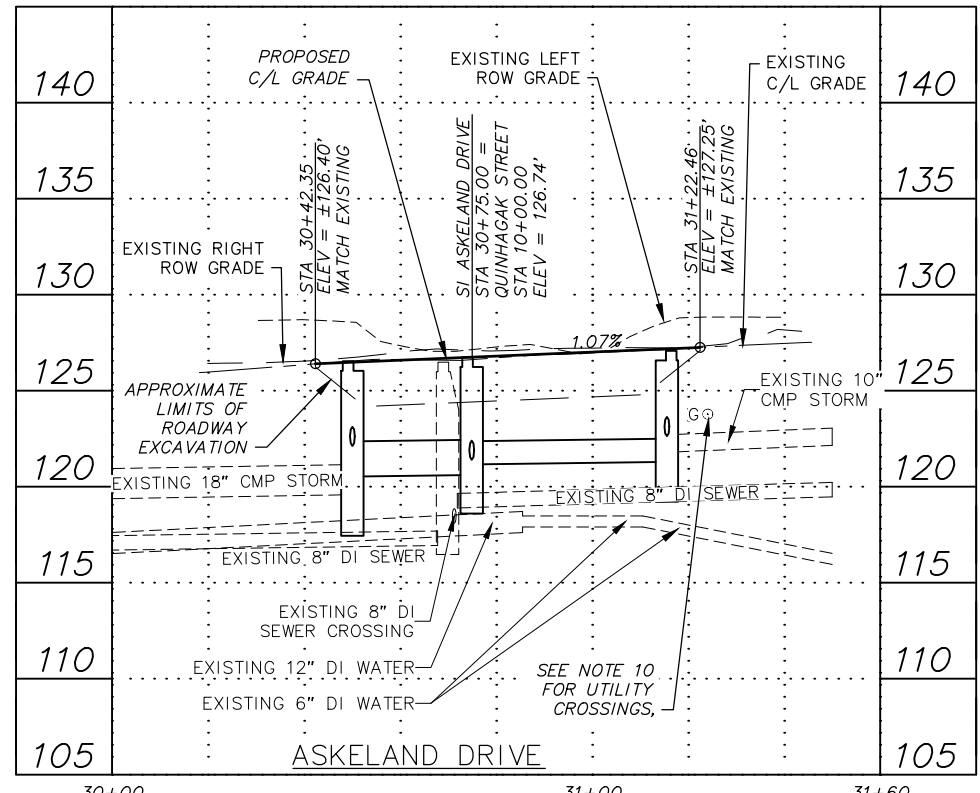


CAUTION!! EXISTING
12" STEEL GAS MAIN,
PROTECT IN PLACE

STA 42+36.00
12.3' LT & 12.0' RT, MATCH
EXISTING AC PAVEMENT

NOTES:

1. SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
2. SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
3. SEE TYPICAL SECTION (C) SHEETS FOR ROADWAY CROSS SECTIONS.
4. FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
5. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
6. SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
7. SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
8. THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
9. GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 3, SHEET C3. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.0% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
10. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. CONTRACTOR SHALL PROTECT EXISTING UTILITIES IN PLACE. CONTRACTOR SHALL SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED.

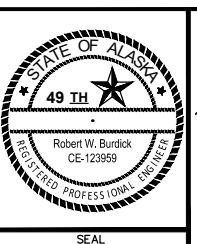
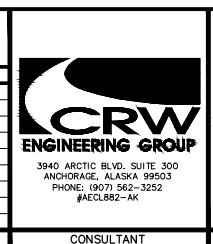


RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ TITLE: _____ DATE: _____
BY: _____
2. DATA TRANSFERRED BY: _____ TITLE: _____ DATE: _____
COMPANY: _____
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____ DATE: _____
COMPANY: _____
BY: _____

DATA	DRAWN BY	CHECKED BY	DATE
BASE	CB	BW	
TOPOGRAPHY	CB	BW	
PROFILE	RB	JK	
STORM SEWER	JM	JH	
WATER/SANITARY SEWER	CK	JK	
GAS	CB	BW	
TELEPHONE	CB	BW	
ELECTRIC	JH	TK	
DESIGN	RB	JK	
QUANTITIES	RB	JK	
PRELIMINARY/FINAL	RB	JK	
MUNICIPAL/STATE	RB	JK	

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV.	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44				

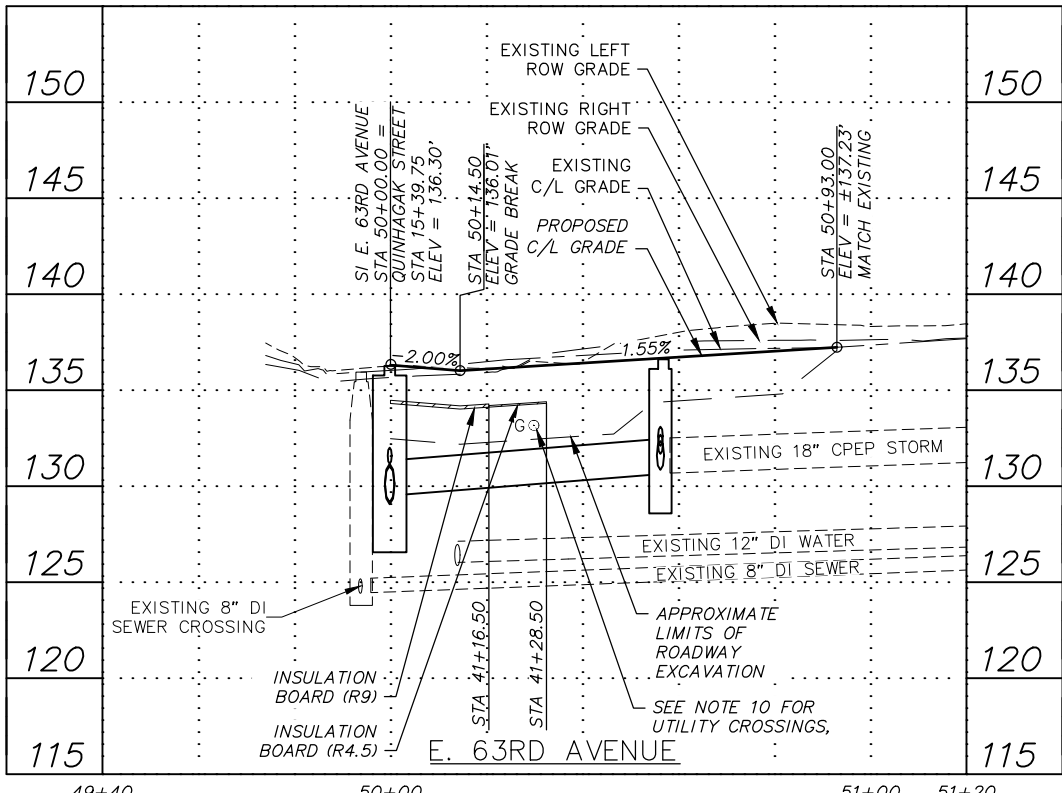
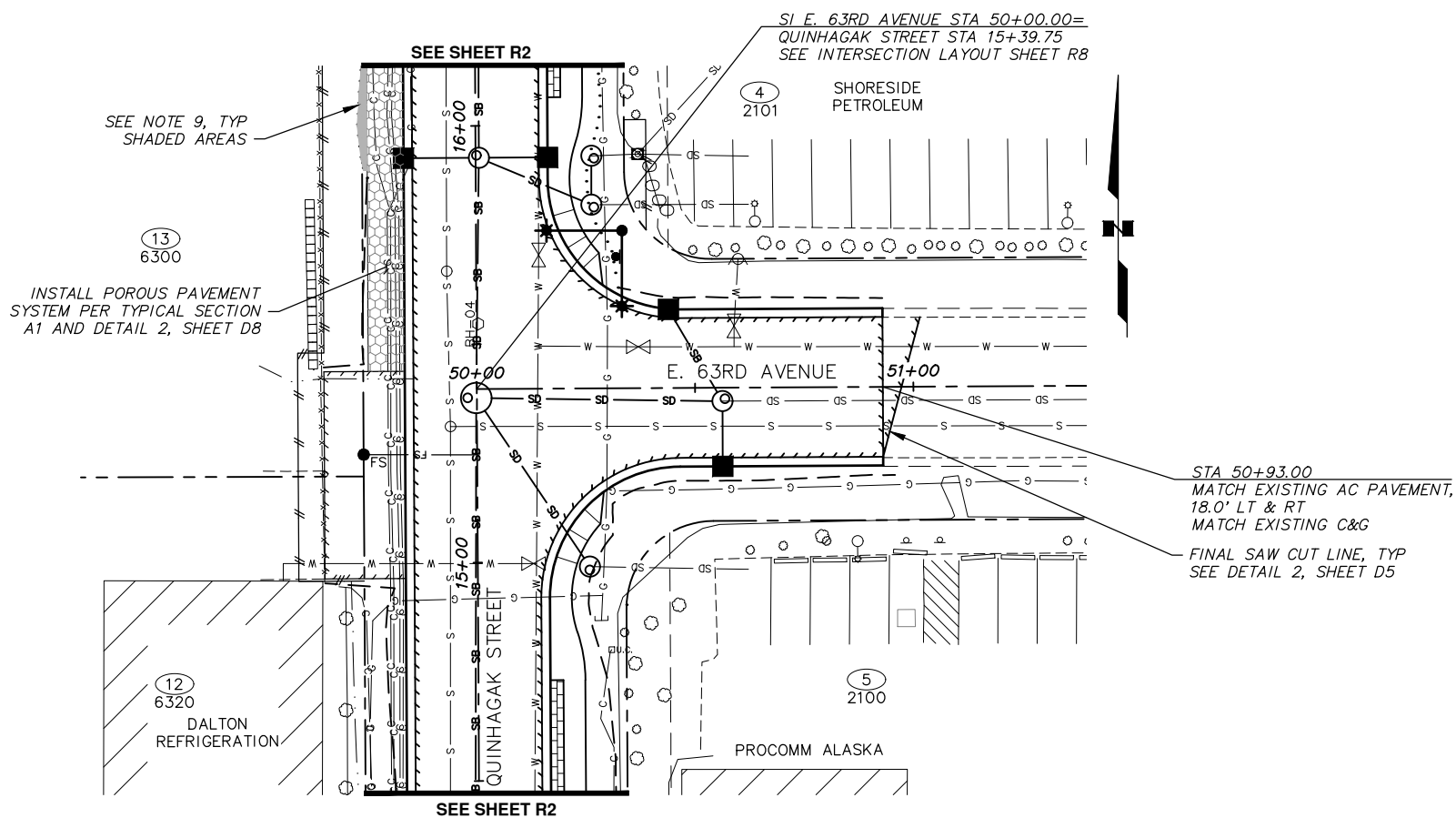
PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY PLAN & PROFILE
ASKELAND DRIVE & E. 64TH AVENUE
SCALE HOR. 1"=20' VER. 1"=5' GRID 9W2033 DATE DEC 2024 STATUS 95% SHEET R4 of R11

File: E:\webdata\10155.00_Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00_Plan & Profile - Roadway - Sidestreets.dwg

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01\10155.00 Plan & Profile - Roadway - Sidestreets.dwg



NOTES:

1. SEE ROADWAY SUMMARY TABLE (T) SHEETS FOR DETAILED ROADWAY INFORMATION.
2. SEE DETAIL (D) SHEETS FOR ROADWAY DETAILS.
3. SEE TYPICAL SECTION (C) SHEETS FOR ROADWAY CROSS SECTIONS.
4. FOR DETAILED SOILS INFORMATION, SEE THE SPECIFICATIONS.
5. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS AND ELEVATIONS OF STORM DRAIN PIPES & STRUCTURES.
6. SEE SURVEY CONTROL (V) SHEETS FOR PROJECT CENTERLINE ALIGNMENT DATA.
7. SEE ILLUMINATION (I) SHEETS FOR ROADWAY LIGHTING INFORMATION.
8. THE DEMOLITION ITEMS REMOVED AS SHOWN ON THE DEMOLITION (B) SHEETS ARE NOT SHOWN FOR CLARITY.
9. GRADE AREA TO DRAIN TOWARDS ROADWAY PER DETAIL 3, SHEET C3. NOTIFY ENGINEER IMMEDIATELY IF MIN 1.0% POSITIVE GRADE TOWARD ROADWAY CANNOT BE MAINTAINED. THIS WORK SHALL BE INCIDENTAL TO CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.
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 CONTRACTOR: _____ TITLE: _____ DATE: _____
 BY: _____
 2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
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 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

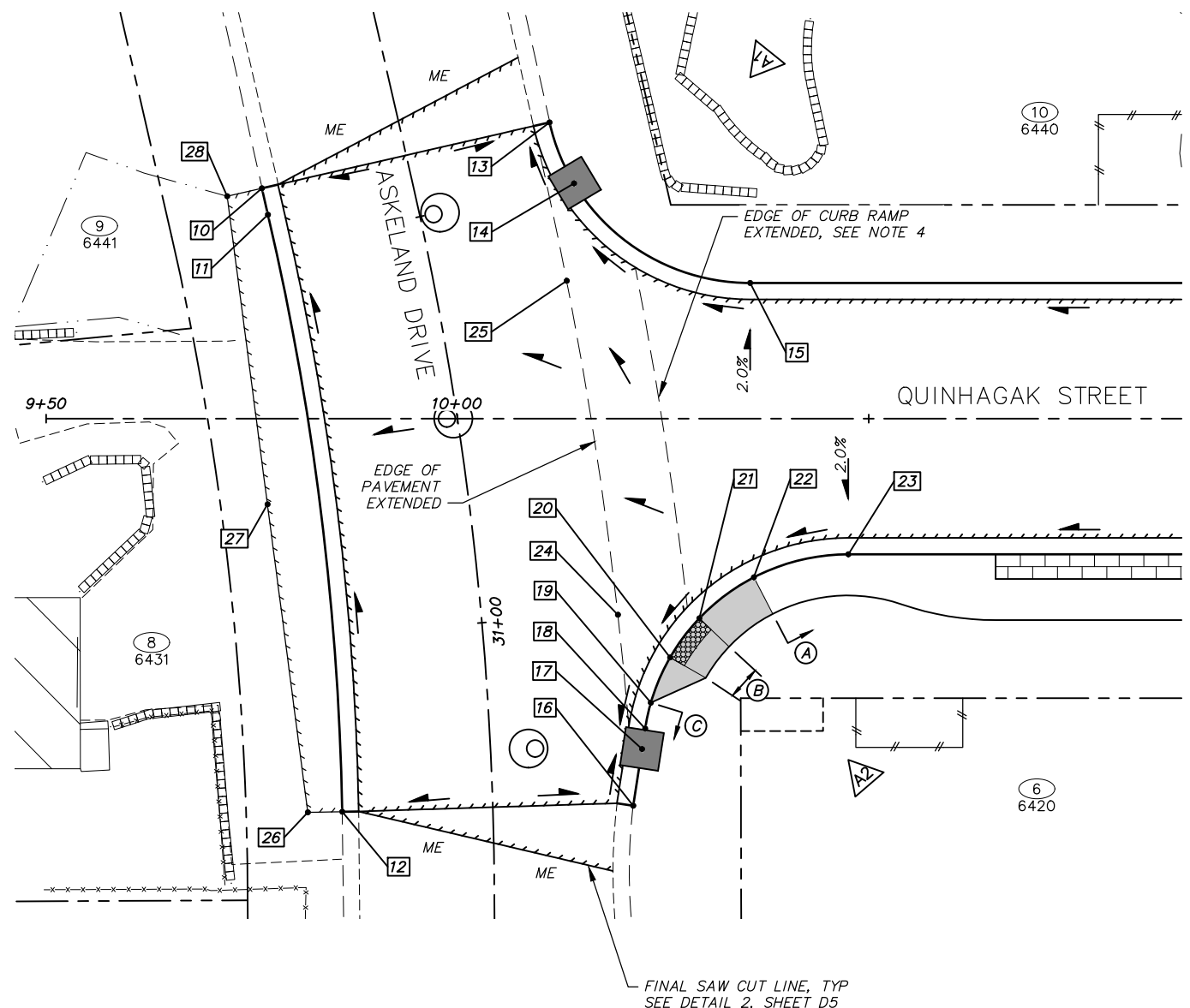


CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AEC0882-AK

STATE OF ALASKA
 49 TH
 Robert W. Burdick
 CE-123959
 REGISTERED PROFESSIONAL ENGINEER

UNIVERSITY OF ALASKA

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED A
ROADWAY PLAN & PROFILE
 E. 63RD AVENUE
 SCALE HOR. 1"=20' VER. 1"=5' GRID SW2033 DATE DEC 2024 STATUS 95% SHEET R5 of R11



□ POINT SUMMARY – QUINHAGAK STREET AT ASKELAND DRIVE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
10	9+76.23	28.0 LT	126.21±	2	126.04±	–	3.30	1.40%	MATCH EXISTING
11	9+76.97	24.8 LT	126.26	2	126.09	–	73.72	1.41%	PC
12	9+86.00	47.8 RT	127.30±	2	127.13±	–	–	–	PT, MATCH EXISTING
13	10+11.20	36.0 LT	126.56±	2	126.39±	–	8.66	0.83%	PC, MATCH EXISTING
14	10+14.17	28.6 LT	126.63	2	126.46	–	27.79	2.81%	CATCH BASIN
15	10+35.59	16.5 LT	127.41	2	127.24	–	–	–	PT
16	10+21.37	47.1 RT	127.12±	2	126.95±	–	6.99	-0.63%	MATCH EXISTING
17	10+22.44	40.2 RT	127.08	2	126.91	–	2.50	2.00%	CATCH BASIN
18	10+22.82	37.7 RT	127.13	2	126.96	–	3.49	1.72%	PC
19	10+23.51	34.6 RT	127.19	2	127.02	–	6.48	2.01%	BEGIN FLARE
20	10+25.85	29.0 RT	127.15	1A	127.15	–	6.43	1.56%	END FLARE, BEGIN LANDING
21	10+29.40	24.3 RT	127.25	1A	127.25	–	9.00	1.56%	END LANDING BEGIN RAMP
22	10+36.03	19.3 RT	127.79	1	127.39	–	12.90	1.55%	END RAMP
23	10+47.53	16.5 RT	127.99	1	127.59	–	–	–	PT
24	10+19.51	23.9 RT	–	–	–	127.08	–	–	EDGE OF PAVEMENT EXTENDED
25	10+13.28	16.8 LT	–	–	–	126.68	–	–	EDGE OF PAVEMENT EXTENDED
26	9+81.81	47.9 RT	–	–	–	127.29±	–	–	EDGE OF PAVEMENT, MATCH EXISTING
27	9+76.90	10.4 RT	–	–	–	126.96±	–	–	EDGE OF PAVEMENT, MATCH EXISTING
28	9+72.00	27.0 LT	–	–	–	126.33±	–	–	EDGE OF PAVEMENT, MATCH EXISTING

△ CURB RADIUS TABLE

POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	10+35.59	41.5 LT	25.0	ASKELAND DRIVE
A2	10+47.53	41.5 RT	25.0	ASKELAND DRIVE

- NOTES**
- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
 - SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
 - SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
 - THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
 - PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
 - SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
 - LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- ➔ APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▒ PCC CURB RAMP
- ▒ COLORED CONCRETE (RED, 4" THICK, IMPRINTED)
- ▒ DETECTABLE WARNING PANEL

DESIGNATION | CURB TYPE

- (A) TYPE 1 CURB
- (B) TYPE 1A CURB
- (C) TYPE 2 CURB

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\01 CADD\10155.00 Intersection Layout Plan.dwg

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
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 CONTRACTOR: _____ DATE: _____
 BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

GRAPHIC SCALE: 20 10 0 10 20

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

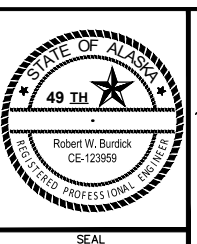
CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST

CRW ENGINEERING GROUP

3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECLE82-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

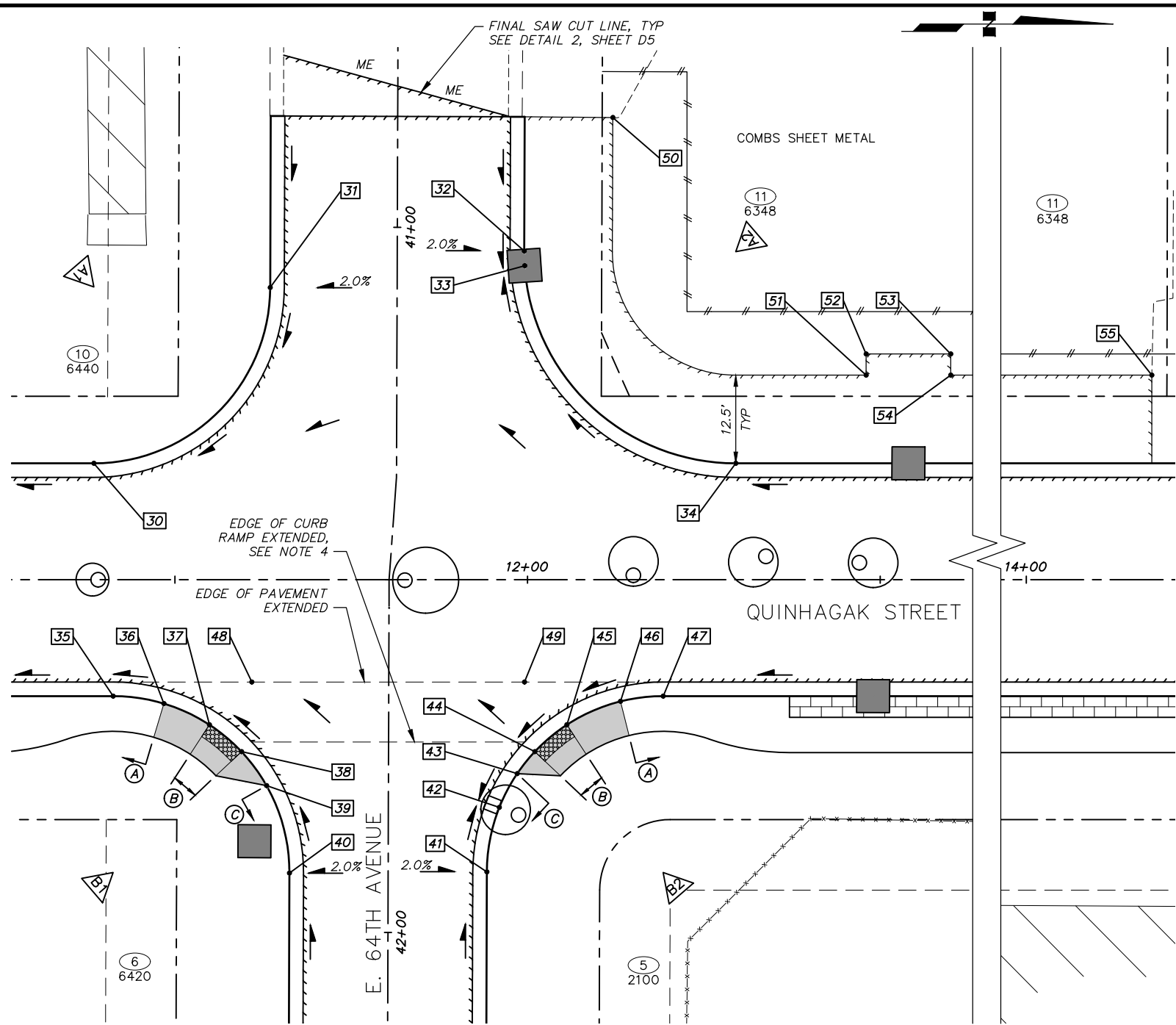
INTERSECTION LAYOUT PLAN

ASKELAND DRIVE

SCALE: HOR. 1"=10'
 VER. N/A

GRID: 5W2033
 DATE: DEC 2024
 STATUS: 95%

SHEET R6 of R11



□ POINT SUMMARY – QUINHAGAK STREET AT E. 64TH AVENUE									
POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
30	11+38.50	16.5 LT	130.42	2	130.25	-	42.32	1.63%	PC
31	11+63.50	41.4 LT	131.11	2	130.94	-	-	-	PT
32	11+99.52	46.6 LT	131.14	2	130.97	-	2.22	-0.90%	PC
33	11+99.59	44.5 LT	131.12	2	130.95	-	48.15	2.20%	CATCH BASIN
34	12+29.52	16.5 LT	132.18	2	132.01	-	-	-	PT
35	11+41.23	16.5 RT	130.71	1	130.31	-	7.89	2.41%	PC
36	11+48.43	17.6 RT	130.90	1	130.50	-	7.71	2.33%	BEGIN RAMP
37	11+54.89	20.6 RT	130.68	1A	130.68	-	6.43	1.87%	END RAMP, BEGIN LANDING
38	11+59.44	24.4 RT	130.80	1A	130.80	-	6.48	4.63%	END LANDING, BEGIN FLARE
39	11+62.99	29.2 RT	131.27	2	131.10	-	13.99	4.57%	END FLARE
40	11+66.23	41.6 RT	131.91	2	131.74	-	-	-	PT
41	11+94.23	41.4 RT	131.90	2	131.73	-	10.11	-2.47%	PC
42	11+95.99	32.3 RT	131.65	2	131.48	-	5.86	0.85%	CATCH BASIN MANHOLE
43	11+98.53	27.5 RT	131.70	2	131.53	-	4.32	0.69%	BEGIN FLARE
44	12+01.02	24.4 RT	131.56	1A	131.56	-	6.43	0.78%	END FLARE, BEGIN LANDING
45	12+05.58	20.6 RT	131.61	1A	131.61	-	9.00	1.22%	END LANDING, BEGIN RAMP
46	12+13.18	17.2 RT	132.12	1	131.72	-	6.60	1.39%	END RAMP
47	12+19.23	16.5 RT	132.21	1	131.81	-	-	-	PT
48	11+60.90	14.5 RT	-	-	-	130.69	-	-	EDGE OF PAVEMENT EXTENDED
49	11+99.57	14.5 RT	-	-	-	131.43	-	-	EDGE OF PAVEMENT EXTENDED
50	12+12.08	65.5 LT	-	-	-	133.54±	-	-	EDGE OF PAVEMENT MATCH EXISTING
51	12+48.00	29.0 LT	-	-	-	133.14±	-	-	EDGE OF PAVEMENT MATCH EXISTING
52	12+48.00	32.0 LT	-	-	-	133.31±	-	-	EDGE OF PAVEMENT MATCH EXISTING
53	12+60.00	32.0 LT	-	-	-	133.43±	-	-	EDGE OF PAVEMENT MATCH EXISTING
54	12+60.00	29.0 LT	-	-	-	133.27±	-	-	EDGE OF PAVEMENT MATCH EXISTING
55	14+17.78	29.0 LT	-	-	-	134.51±	-	-	EDGE OF PAVEMENT MATCH EXISTING

△ CURB RADIUS TABLE				
POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	11+38.50	41.5 LT	25.0	E. 64TH AVENUE
A2	12+29.52	46.5 LT	30.0	E. 64TH AVENUE
B1	11+41.23	41.5 RT	25.0	E. 64TH AVENUE
B2	12+19.23	41.5 RT	25.0	E. 64TH AVENUE

- NOTES**
- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
 - SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
 - SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
 - THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
 - PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
 - SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
 - LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- ➔ APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▒ PCC CURB RAMP
- ▒ COLORED CONCRETE (RED, 4" THICK, IMPRINTED)
- ▒ DETECTABLE WARNING PANEL

DESIGNATION | CURB TYPE

- (A) TYPE 1 CURB
- (B) TYPE 1A CURB
- (C) TYPE 2 CURB

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00 CAD\2018\01 Working Set\01\10155.00 Intersection Layout Plan.dwg

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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CONTRACTOR: _____ TITLE: _____ DATE: _____

BY: _____

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DATA TRANSFER CHECKED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

GRAPHIC SCALE: 20 10 0 10 20

SCALE

PLAN CHECK CONSTRUCTION RECORD VERTICAL DATUM REVISIONS CONSULTANT SEAL

CRW ENGINEERING GROUP

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC1882-AK

STATE OF ALASKA
49 TH
Robert W. Burdick
CE-123959
REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

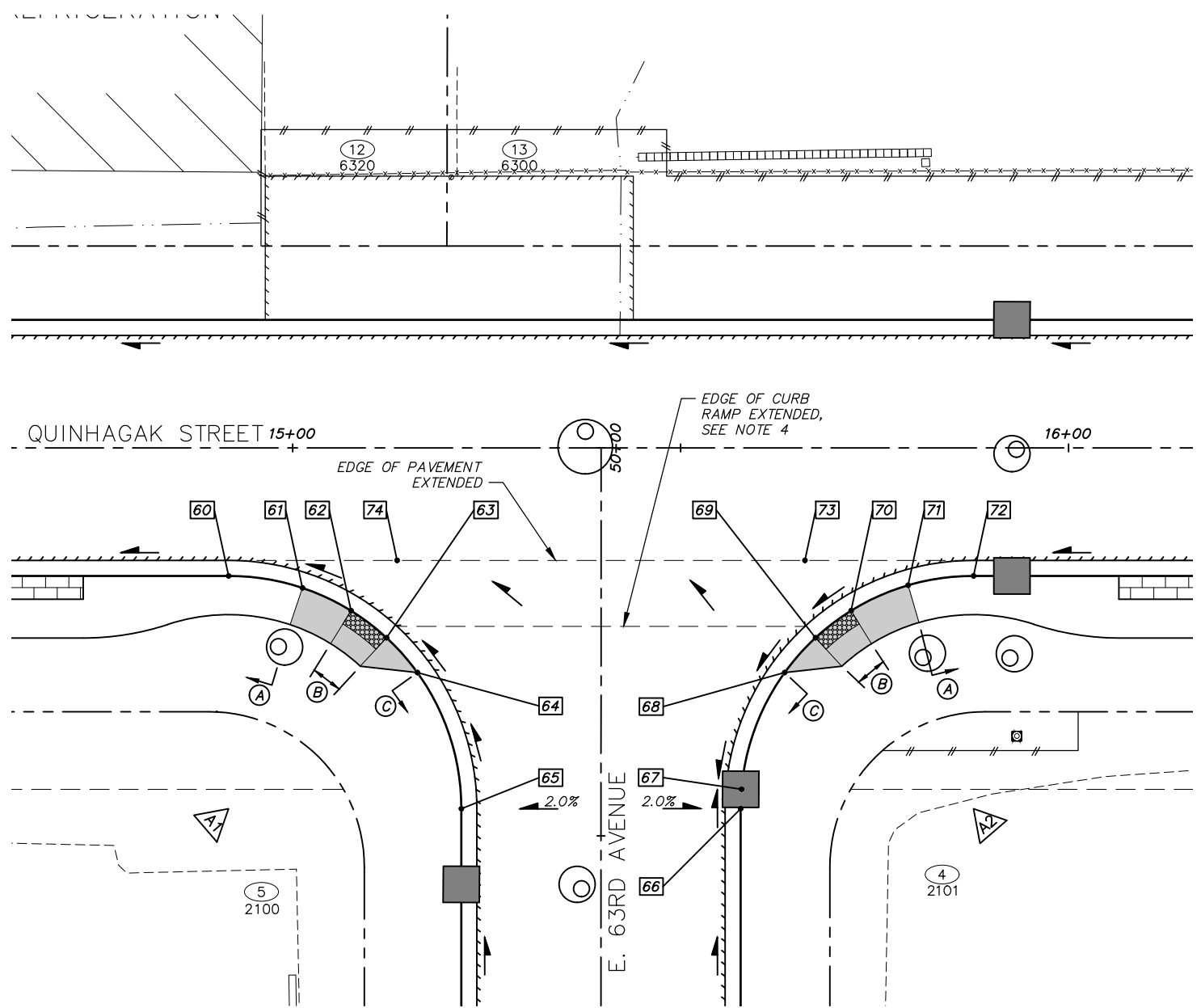
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED A

INTERSECTION LAYOUT PLAN

E. 64TH AVENUE

SCALE HOR. 1"=10' VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET R7 of R11



□ POINT SUMMARY – QUINHAGAK STREET AT E. 63RD AVENUE

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TO NEXT POINT*		DESCRIPTION
							LENGTH (FT)	SLOPE (%)	
60	14+91.74	16.5 RT	135.65	1	135.25	–	10.34	1.94%	PC
61	15+01.26	18.1 RT	135.85	1	135.45	–	7.39	1.90%	BEGIN RAMP
62	15+07.51	21.0 RT	135.59	1A	135.59	–	6.15	1.95%	END RAMP, BEGIN LANDING
63	15+12.10	24.5 RT	135.71	1A	135.71	–	6.40	2.03%	END LANDING, BEGIN FLARE
64	15+16.07	29.0 RT	136.01	2	135.84	–	20.00	1.95%	END FLARE
65	15+21.74	46.5 RT	136.40	2	136.23	–	–	–	PT
66	15+57.74	46.5 RT	136.40	2	136.23	–	2.67	-2.25%	PC
67	15+57.84	44.0 RT	136.34	2	136.17	–	17.32	1.39%	CATCH BASIN
68	15+63.40	29.0 RT	136.58	2	136.41	–	6.40	1.41%	BEGIN FLARE
69	15+67.38	24.5 RT	136.50	1A	136.50	–	6.15	0.97%	END FLARE, BEGIN LANDING
70	15+71.96	21.0 RT	136.56	1A	136.56	–	8.62	1.04%	END LANDING, BEGIN RAMP
71	15+79.32	17.7 RT	137.05	1	136.65	–	9.10	0.99%	END RAMP
72	15+87.74	16.5 RT	137.14	1	136.74	–	–	–	PT
73	15+66.02	14.5 RT	–	–	–	136.43	–	–	EDGE OF PAVEMENT EXTENDED
74	15+13.46	14.5 RT	–	–	–	135.59	–	–	EDGE OF PAVEMENT EXTENDED

△ CURB RADIUS TABLE

POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	14+91.74	46.5 RT	30.0	E. 63RD AVENUE
A2	15+87.74	46.5 RT	30.0	E. 63RD AVENUE

- NOTES**
- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
 - SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
 - SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
 - THE MAXIMUM CROSS-SLOPE BETWEEN EDGE OF PAVEMENT EXTENDED AND EDGE OF CURB RAMP EXTENDED SHALL BE 2%. IF A 2% CROSS-SLOPE CANNOT BE MAINTAINED NOTIFY ENGINEER PRIOR TO INSTALLATION OF AC PAVEMENT.
 - PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
 - SEE DETAIL (D) SHEETS FOR CURB RAMP DETAILS.
 - LIP OF CURB IS FRONT OF CURB AND GUTTER AT EDGE OF PAVEMENT.

LEGEND

- ➔ APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- ▭ PCC CURB RAMP
- ▨ COLORED CONCRETE (RED, 4" THICK, IMPRINTED)
- ▩ DETECTABLE WARNING PANEL

DESIGNATION | CURB TYPE

- (A) TYPE 1 CURB
- (B) TYPE 1A CURB
- (C) TYPE 2 CURB

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\01 Civil\10155.00 Intersection Layout Plan.dwg

RECORD DRAWING

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TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

20 10 0 10 20
GRAPHIC SCALE

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK CONSTRUCTION RECORD VERTICAL DATUM REVISIONS CONSULTANT SEAL

CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECLE882-AK

STATE OF ALASKA
 49 TH
 Robert W. Burdick
 CE-123959
 REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

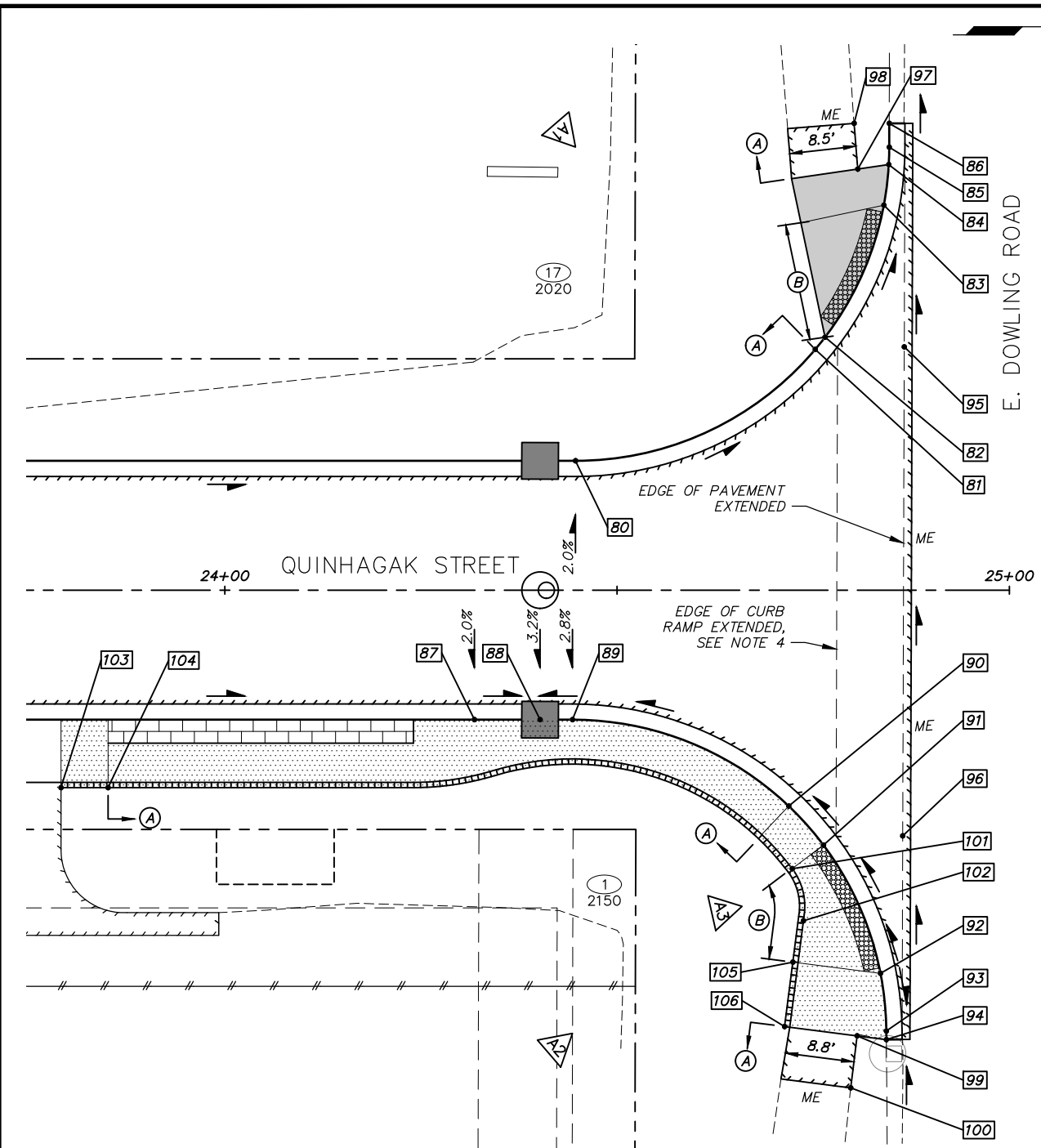
21-13 QUINHAGAK STREET RECONSTRUCTION
 E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

INTERSECTION LAYOUT PLAN

E. 63RD AVENUE

SCALE HOR. 1"=10'
 VER. N/A

GRID 5W2033
 DATE DEC 2024 STATUS 95% SHEET R8 of R11



□ POINT SUMMARY – QUINHAGAK STREET AT E. DOWLING ROAD

POINT	STATION	OFFSET (FT)	TBC ELEV (FT)	CURB TYPE	LIP OF CURB ELEV (FT)	TOP AC ELEV (FT)	TOP SW ELEV (FT)	TOP RW ELEV (FT)	TO NEXT POINT*		DESCRIPTION
									LENGTH (FT)	SLOPE (%)	
80	24+44.69	16.5 LT	144.28	1	143.88	-	-	-	36.53	-0.63%	PC
81	24+75.26	30.7 LT	144.05	1	143.65	-	-	-	2.00	-0.50%	BEGIN CURB TRANSITION
82	24+76.51	32.3 LT	143.64	1A	143.64	-	-	-	19.51	-0.82%	END CURB TRANSITION, BEGIN LANDING
83	24+84.00	49.1 LT	143.48	1A	143.48	-	-	-	5.49	-1.09%	END LANDING, BEGIN RAMP
84	24+84.63	54.3 LT	143.82	1	143.42	-	-	-	2.32	-0.86%	END RAMP
85	24+84.69	56.5 LT	143.80	1	143.40	-	-	-	3.04	-1.02%	PT
86	24+84.69	59.5 LT	143.77±	1	143.37±	-	-	-	-	-	MATCH EXISTING
87	24+31.82	16.5 RT	144.39	1	143.99	-	-	-	8.37	-2.99%	END 2% LANE SLOPE
88	24+40.18	16.5 RT	144.14	1	143.74	-	-	-	4.14	0.73%	CATCH BASIN
89	24+44.32	16.5 RT	144.17	1	143.77	-	-	-	31.93	0.54%	PC
90	24+71.88	27.5 RT	144.34	1	143.94	-	-	-	7.00	0.56%	BEGIN RAMP
91	24+76.31	32.5 RT	143.98	1A	143.98	-	-	-	18.95	0.58%	END RAMP, BEGIN LANDING
92	24+83.58	48.8 RT	144.09	1A	144.09	-	-	-	7.77	-1.80%	END LANDING, BEGIN RAMP
93	24+84.32	56.2 RT	144.35	1	143.95	-	-	-	1.10	-1.82%	PT
94	24+84.32	57.3 RT	144.34±	1	143.93±	-	-	-	-	-	END RAMP, MATCH EXISTING
95	24+86.60	31.0 LT	-	-	-	143.60	-	-	-	-	EDGE OF PAVEMENT EXTENDED
96	24+86.40	31.3 RT	-	-	-	143.86	-	-	-	-	EDGE OF PAVEMENT EXTENDED
97	24+80.68	53.7 LT	-	-	-	143.88	-	-	-	-	EDGE OF PAVEMENT AT RAMP
98	24+80.21	59.5 LT	-	-	-	143.92±	-	-	-	-	EDGE OF PAVEMENT, MATCH EXISTING
99	24+80.60	56.8 RT	-	-	-	144.45	-	-	-	-	EDGE OF PAVEMENT AT RAMP
100	24+79.77	63.4 RT	-	-	-	144.49±	-	-	-	-	EDGE OF PAVEMENT, MATCH EXISTING
101	24+72.32	35.5 RT	-	-	-	-	144.06	-	-	-	BACK OF SIDEWALK, PC
102	24+73.75	42.1 RT	-	-	-	-	144.16	-	-	-	BACK OF SIDEWALK, PT
103	23+79.11	25.2 RT	-	-	-	-	-	144.42	-	-	BEGIN RETAINING WALL TRANSITION
104	23+85.11	25.2 RT	-	-	-	-	-	145.65	-	-	END RETAINING WALL TRANSITION
105	24+72.40	47.4 RT	-	-	-	-	-	145.65	-	-	BEGIN RETAINING WALL TRANSITION
106	24+71.35	55.6 RT	-	-	-	-	-	144.54	-	-	END RETAINING WALL TRANSITION

△ CURB RADIUS TABLE

POINT	TBC RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	24+44.69	56.5 LT	40.0	E. DOWLING ROAD
A2	24+44.32	56.5 RT	40.0	E. DOWLING ROAD
A3	24+65.92	40.3 RT	8.0	BACK OF SIDEWALK

- NOTES**
- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
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LEGEND

- ▲ APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- PCC CURB RAMP
- ▒ COLORED CONCRETE (RED, 4" THICK, IMPRINTED)
- ▒ DETECTABLE WARNING PANEL
- ▒ P.C.C. STRUCTURE/RETAINING WALL (CLASS AA-3)

DESIGNATION | CURB TYPE

- (A) TYPE 1 CURB
- (B) TYPE 1A CURB
- (C) TYPE 2 CURB

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DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

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CRW ENGINEERING GROUP

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UNIVERSITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

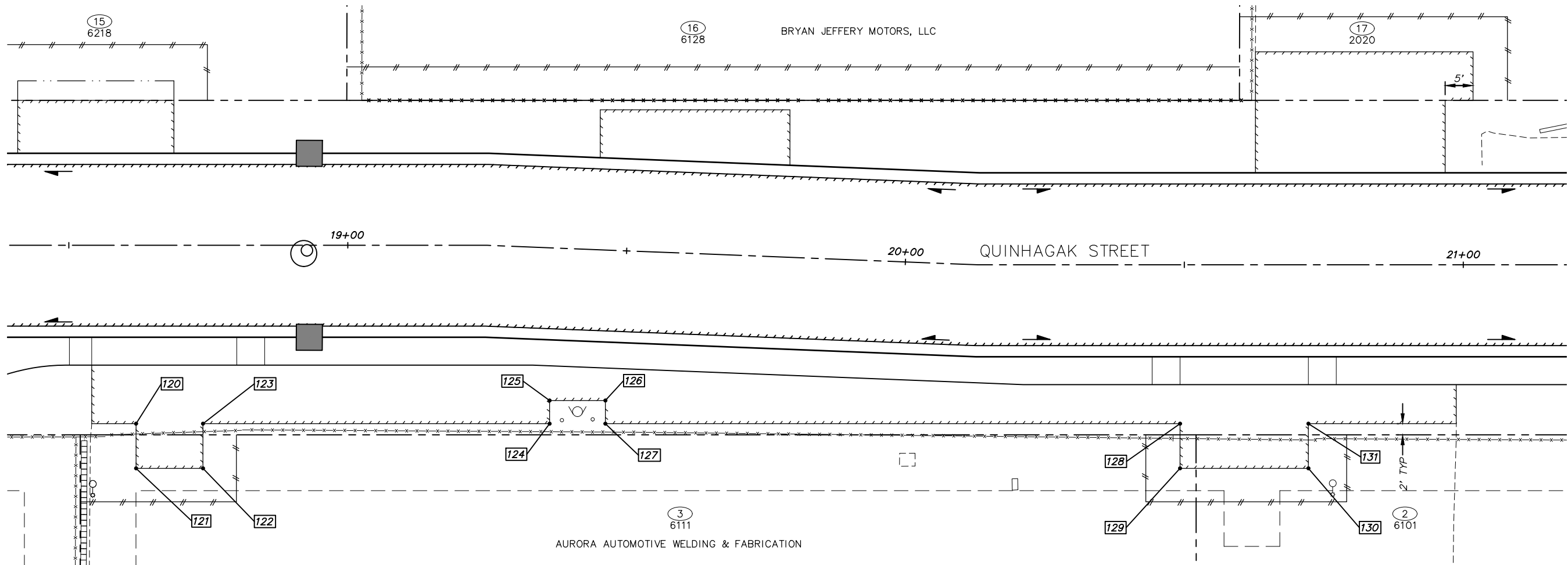
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

INTERSECTION LAYOUT PLAN

E. DOWLING ROAD

SCALE HOR. 1"=10' VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET R9 of R11

File: E:\lab\data\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\01\10155.00 Intersection Layout_Plan.dwg



NOTES

1. SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
2. SEE STORM DRAIN (SD) SHEETS FOR LOCATIONS & ELEVATIONS OF SD PIPES & STRUCTURES.
3. SEE SIGNING & STRIPING (S) SHEETS FOR LOCATIONS & TYPES OF SIGNS & TRAFFIC MARKINGS.
4. SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLE (T) SHEETS FOR INDIVIDUAL DRIVEWAY SPECIFICS.

LEGEND

➔ APPROXIMATE DIRECTION OF DRAINAGE FLOWS

☐ POINT SUMMARY – PARCELS 2, 3, & 17				
POINT	STATION	OFFSET (FT)	TOP AC ELEV (FT)	DESCRIPTION
120	18+62.04	32.0 RT	141.05±	EDGE OF PAVEMENT, MATCH EXISTING
121	18+62.04	40.0 RT	141.46±	EDGE OF PAVEMENT, MATCH EXISTING
122	18+74.04	40.0 RT	141.67±	EDGE OF PAVEMENT, MATCH EXISTING
123	18+74.04	32.0 RT	141.33±	EDGE OF PAVEMENT, MATCH EXISTING
124	19+37.43	31.5 RT	142.87±	EDGE OF PAVEMENT, MATCH EXISTING
125	19+37.26	27.4 RT	142.69±	EDGE OF PAVEMENT, MATCH EXISTING
126	19+47.25	27.0 RT	142.91±	EDGE OF PAVEMENT, MATCH EXISTING
127	19+47.42	31.1 RT	143.09±	EDGE OF PAVEMENT, MATCH EXISTING
128	20+49.24	28.5 RT	144.24±	EDGE OF PAVEMENT, MATCH EXISTING
129	20+49.24	36.5 RT	144.27±	EDGE OF PAVEMENT, MATCH EXISTING
130	20+72.24	36.5 RT	143.82±	EDGE OF PAVEMENT, MATCH EXISTING
131	20+72.24	28.5 RT	143.72±	EDGE OF PAVEMENT, MATCH EXISTING

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00 CADD 2018\01 Working Set\01\10155.00 Driveway Reconstruction Plan.dwg

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PROFILE	RB	JK
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GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

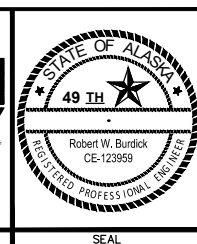
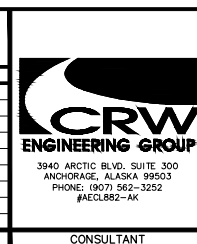
STAKING

ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

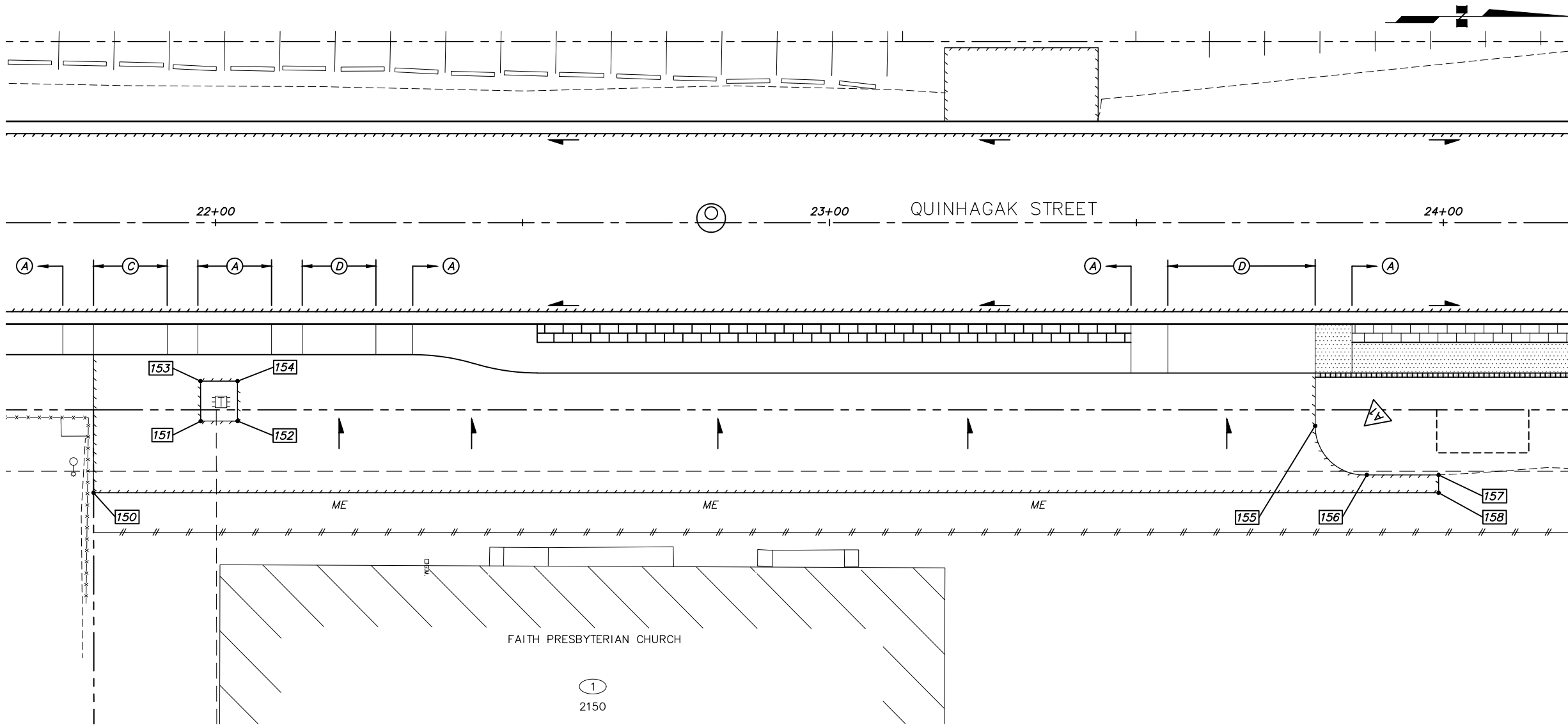
21-13 QUINHAGAK STREET RECONSTRUCTION SCHED A
 E. DOWLING ROAD TO ASKELAND DRIVE

DRIVEWAY RECONSTRUCTION PLAN

PARCELS 2, 3, & 17

SCALE HOR. 1"=10'
 VER. N/A

GRID SW2033
 DATE DEC 2024 STATUS 95% SHEET R10 of R11



NOTES

- SEE ROADWAY (R) SHEETS FOR ROADWAY & SIDEWALK LOCATIONS.
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- SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLE (T) SHEETS FOR INDIVIDUAL DRIVEWAY SPECIFICS.

LEGEND

- APPROXIMATE DIRECTION OF DRAINAGE FLOWS
- COLORED CONCRETE (RED, 4" THICK, IMPRINTED)
- COLORED CONCRETE (RED, 6" THICK, IMPRINTED)
- P.C.C. STRUCTURE/RETAINING WALL (CLASS AA-3)

DESIGNATION	CURB TYPE
(A)	TYPE 1 CURB
(C)	TYPE 2 CURB
(D)	TYPE 4 CURB

□ POINT SUMMARY – PARCEL 1				
POINT	STATION	OFFSET (FT)	TOP AC ELEV (FT)	DESCRIPTION
150	21+80.11	44.0 RT	145.01±	EDGE OF PAVEMENT, MATCH EXISTING
151	21+97.57	32.3 RT	144.49±	EDGE OF PAVEMENT & EDGE OF EXPOSED CONCRETE BASE, MATCH EXISTING
152	22+03.58	32.3 RT	144.42±	EDGE OF PAVEMENT & EDGE OF EXPOSED CONCRETE BASE, MATCH EXISTING
153	21+97.52	25.8 RT	144.20±	EDGE OF PAVEMENT & EDGE OF EXPOSED CONCRETE BASE, MATCH EXISTING
154	22+03.56	25.8 RT	144.29±	EDGE OF PAVEMENT & EDGE OF EXPOSED CONCRETE BASE, MATCH EXISTING
155	23+79.11	33.1 RT	144.51	EDGE OF PAVEMENT, PC
156	23+87.52	41.1 RT	144.66	EDGE OF PAVEMENT, PT
157	23+99.21	41.1 RT	144.79±	EDGE OF PAVEMENT, MATCH EXISTING
158	23+99.21	44.0 RT	144.80±	EDGE OF PAVEMENT, MATCH EXISTING

△ EOP RADIUS TABLE				
POINT	EOP RADIUS POINT		RADIUS (FT)	DESCRIPTION
	STATION	OFFSET (FT)		
A1	23+87.11	33.1 RT	8.0	EDGE OF PAVEMENT

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 CADD\10155.00 Driveway Reconstruction Plan.dwg

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QUANTITIES/FINAL	RB	JK
PRELIMINARY/STATE	RB	JK
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FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

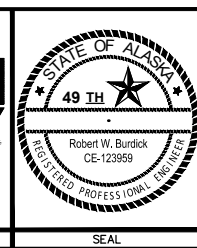
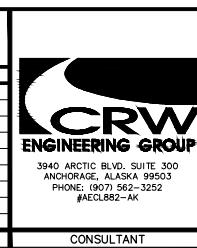
ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST

GRAPHIC SCALE: 20 10 0 10 20



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION SCHED A
 E. DOWLING ROAD TO ASKELAND DRIVE

DRIVEWAY RECONSTRUCTION PLAN

PARCEL 1

SCALE HOR. 1"=10'
 VER. N/A

GRID SW2033
 DATE DEC 2024 STATUS 95%

R11 of R11 SHEET

20.28

RECONSTRUCT DRIVEWAY

SHEET	PARCEL	CENTERLINE REFERENCE		DRIVEWAY WIDTH AT TOP BACK CURB (FT)	DRIVEWAY WIDTH AT ROW (FT)	CURB CUT TYPE	SKEW ANGLE (DEGREES)	LANDING LENGTH (FT)	LANDING GRADE	TOTAL DISTANCE (FT)	EXISTING GRADE	PROPOSED GRADE	SURFACE TYPE ON PROPERTY	L1 (FT)	L2 (FT)	CONSTRUCT PER DETAIL	REMARKS
		STATION	OFFSET														
R1	9	30+51.4	RT	15.6	N/A	2	90	N/A	N/A	VARIES	2.8%	2.8%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	ASKELAND DRIVE - SEE INTERSECTION LAYOUT SHEET R6
R1	8	30+91.6	RT	60.0	N/A	2	90	N/A	N/A	VARIES	3.6%	3.6%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	ASKELAND DRIVE - SEE INTERSECTION LAYOUT SHEET R6
R1	10	11+05.2	LT	28.0	28.0	2	-90	N/A	N/A	14.0	11.4%	8.5%	GRAVEL	N/A	N/A	DETAIL 3, SHEET D4	
R1	11	12+90.5	LT	SEE REMARKS		2	-90	N/A	N/A	VARIES	VARIES	VARIES	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	SEE INTERSECTION LAYOUT SHEET R7 FOR LAYOUT
R2	12	15+08.3	LT	23.4	23.4	2	-90	N/A	N/A	18.5	4.4%	4.0%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	
R2	13	15+31.9	LT	24.0	24.0	2	-90	N/A	N/A	18.5	6.3%	6.8%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	
R2	14	16+51.7	LT	34.0	34.0	2	-90	N/A	N/A	17.5	7.5%	5.2%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	
R2	4 SOUTH	16+92.2	RT	19.0	19.0	2	90	8.0	2.0%	24.7	13.3%	8.6%	CONCRETE	6.0	7.0	DETAIL 1, SHEET D4	
R2	4 NORTH	18+10.6	RT	20.0	20.0	4	90	8.0	2.0%	17.5	2.8%	5.8%	ASPHALT	6.0	8.6	DETAIL 1, SHEET D4	
R2	15	18+54.8	LT	28.0	28.0	2	-90	N/A	N/A	13.0	3.2%	5.5%	GRAVEL	N/A	N/A	DETAIL 3, SHEET D4	
R2	3	18+67.1	RT	26.0	26.0	2	90	5.0	2.0%	15.5	5.4%	5.4%	ASPHALT	4.0	5.0	DETAIL 2, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R10
R3	16	19+61.6	LT	34.0	N/A	2	-88	N/A	N/A	9.3	7.3%	6.3%	ASPHALT	N/A	N/A	DETAIL 3, SHEET D4	
R3	2	20+60.8	RT	23.0	23.0	2	90	5.0	2.0%	20.0	1.1%	3.9%	ASPHALT	5.0	5.0	DETAIL 2, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R10
R3	17 SOUTH	20+79.8	LT	34.0	39.0	2	-90	N/A	N/A	21.7	6.1%	7.0%	ASPHALT	6.0	6.0	DETAIL 4, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R10
R3	1 SOUTH	21+86.3	RT	12.0	12.0	2	90	5.0	2.0%	27.5	7.4%	7.6%	ASPHALT	5.0	5.0	DETAIL 2, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R3	1 MIDDLE	21+86.3	RT	12.0	12.0	4	90	5.0	2.0%	27.5	6.4%	6.9%	ASPHALT	5.0	6.0	DETAIL 2, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11
R3	17 NORTH	23+31.3	LT	25.0	N/A	4	-90	N/A	N/A	12.0	8.3%	7.6%	ASPHALT	6.0	6.0	DETAIL 4, SHEET D4	
R3	1 NORTH	23+67.1	RT	24.0	24.0	4	90	8.0	1.5%	27.5	0.8%	0.9%	ASPHALT	6.0	6.0	DETAIL 1, SHEET D4	SEE DRIVEWAY RECONSTRUCTION PLAN SHEET R11

RECONSTRUCT DRIVEWAY NOTES:

- "LANDING LENGTH" BEGINS AT THE BACK OF CURB & GUTTER.
- "LANDING GRADE" IS THE GRADE OF THE LANDING FROM THE BACK OF CURB & GUTTER TO THE END OF LANDING.
- "SKEW ANGLE" ("+" IS CLOCKWISE AND "-" IS COUNTER CLOCKWISE) IS MEASURED FROM PROJECT CENTERLINE WITH 0 DEGREES ALIGNED ALONG INCREASING STATIONS.
- "TOTAL DISTANCE" IS THE LIMIT OF RECONSTRUCTION BEGINNING AT THE BACK OF CURB & GUTTER.
- "PROPOSED GRADE" IS APPROXIMATE GRADE FROM THE END OF THE LANDING TO THE LIMIT OF RECONSTRUCTION. ACTUAL CONSTRUCTION GRADE MAY VARY.
- WIDTHS, LENGTHS & GRADES PRESENTED IN THE DRIVEWAY SUMMARY TABLE ARE MEASURED ALONG SKEW ANGLE AND MAY NOT BE PERPENDICULAR TO ROADWAY CENTERLINE ALIGNMENT.
- MATCH EXISTING DRIVEWAY WIDTH AT LIMITS OF DRIVEWAY RECONSTRUCTION. WIDTH OF DRIVEWAY AS SHOWN IN SUMMARY TABLE SHALL EXTEND TO BACK OF SIDEWALK OR BACK OF CURB ALONG SKEW ANGLE.

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\01_Civil\10155.00_Roadway_Summary_Tables.dwg

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
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 BY: _____

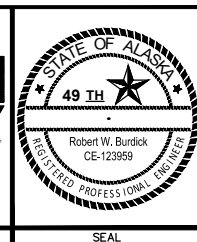
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 COMPANY: _____ DATE: _____

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DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AEC0882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

ROADWAY SUMMARY TABLES

SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET T1 of T3

30.02

P.C.C. CURB AND GUTTER (ALL TYPES)

Table with 5 columns: SHEET, STATION TO STATION, OFFSET (FT), LENGTH (FT), REMARKS. Rows include R1, R2, R3 with stationing and remarks like 'INCLUDES SIDE STREETS' and 'ASKELAND DRIVE'.

30.03

P.C.C. SIDEWALK

Table with 8 columns: SHEET, APPX BEGIN STA, APPX OFFSET (FT), APPX END STA, APPX OFFSET (FT), 4" THICK, AREA (SY), 6" THICK, AREA (SY), REMARKS. Rows include R1, R2, R3 with stationing and remarks like 'PARCEL 4 SOUTH DRIVEWAY'.

30.04

P.C.C. CURB RAMP (6" THICK) & DETECTABLE WARNINGS

Table with 7 columns: SHEET, APPX STA, APPX OFFSET (FT), CURB RAMP AREA (SY), DETECTABLE WARNING AREA (SF), CURB RAMP TYPE, REMARKS. Rows include R1, R2, R3 with stationing and remarks like 'ASKELAND DRIVE' and 'E. DOWLING ROAD'.

PCC CURB RAMP & DETECTABLE WARNING NOTES:

- 1. SEE INTERSECTION LAYOUT SHEETS R6-R9 FOR FOR LOCATIONS OF CURB RAMPS AND DETECTABLE WARNINGS.
2. CURB RAMP PAID FOR UNDER CY QUANTITY OF 30.05 PCC STRUCTURES/RETAINING WALL (CLASS AA-3).

30.10

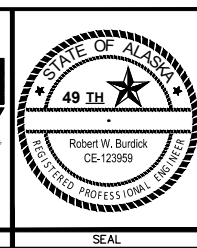
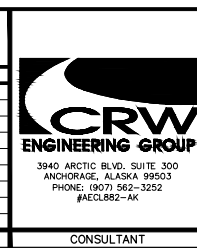
COLORED CONCRETE (RED, IMPRINTED)

Table with 8 columns: SHEET, APPX BEGIN STA, APPX OFFSET (FT), APPX END STA, APPX OFFSET (FT), 4" THICK, AREA (SY), 6" THICK, AREA (SY), REMARKS. Rows include R1, R2, R3 with stationing and remarks like 'PARCEL 1 PARKING AREA'.

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DATA TRANSFER CHECKED BY: TITLE:
COMPANY: DATE:
BY:

Table with columns: DATA, DRAWN BY, CHECKED BY, FIELD BOOKS, BM NO., LOCATION, ELEV., REV., DATE, DESCRIPTION, BY. Includes entries for TOPOGRAPHY, PROFILE, STORM SEWER, WATER/SANITARY SEWER, GAS, TELEPHONE, ELECTRIC, DESIGN, QUANTITIES, PRELIMINARY/FINAL, MUNICIPAL/STATE.



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY SUMMARY TABLES
SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET T2 of T3

50.06

REMOVE AND REPLACE MANHOLE CONE SECTION OR MANHOLE COVER AND FRAME					
SHEET	STATION	OFFSET (FT)	CONE SECTION	COVER AND FRAME	REMARKS
R1	9+89	3.4 LT		X	
R1	11+71	8.1 LT		X	
R1	11+74	43.7 RT		X	
R2	15+31	6.1 LT		X	
R2	15+67	6.8 LT	X		
R3	19+69	7.4 LT	X		
R3	23+59	7.6 LT	X		

REMOVE AND REPLACE MANHOLE CONE SECTION OR REMOVE AND REPLACE MANHOLE COVER AND FRAME NOTES:

- SEE MASS DETAIL 50-05, 50-25 AND 50-26.
- COORDINATE WITH ENGINEER IN FIELD TO VERIFY WHETHER CONE OR MANHOLE COVER AND FRAME ADJUSTMENT IS REQUIRED.
- PER THE SECTION 50.06 SPECIAL PROVISIONS THE REMOVE AND REPLACE MANHOLE CONE SECTION PAY ITEM INCLUDES REMOVING AND REPLACING THE MANHOLE COVER AND FRAME. SEE SECTION 50.06 SPECIAL PROVISIONS FOR A COMPLETE LIST OF INCIDENTAL ITEMS.

60.03 & 60.05

REMOVE AND REPLACE VALVE BOX TOP SECTION OR ADJUST KEY BOX					
SHEET	STATION	OFFSET (FT)	KEY BOX	VALVE BOX TOP SECTION	REMARKS
R1	10+15	34.6 LT		X	
R1	10+32	17.0 RT		X	
R1	11+52	9.5 RT		X	
R1	11+57	12.3 RT		X	
R1	12+16	9.9 RT		X	
R1	13+36	17.2 RT	X		
R2	15+00	12.8 RT	X		
R2	15+49	37.0 RT		X	
R2	15+54	59.0 RT		X	
R2	15+70	14.3 RT		X	
R2	16+68	26.0 LT	X		
R2	19+42	17.8 RT		X	
R3	19+89	12.1 RT		X	
R3	19+94	13.5 RT	X		
R3	21+46	12.1 RT	X		
R3	24+16	13.7 RT		X	

REMOVE AND REPLACE VALVE BOX TOP SECTION NOTES:

- SEE MASS DETAIL 60-08 AND 60-16.

SPECIAL FILL GRADING TABLE

SHEET	APPROX BEGIN STATION	APPROX END STATION	OFFSET	REMARKS
R1	10+33	10+53	RT	
R1	12+75	12+90	RT	
R2	15+90	16+35	LT	
R2	16+68	16+80	LT	
R2	17+02	18+00	RT	
R2	18+21	18+54	RT	
R3	20+99	21+15	RT	

SPECIAL FILL GRADING NOTES:

- SPECIAL FILL GRADING SHALL BE PER DETAIL 3, SHEET C3.
- LOCATIONS ARE APPROXIMATE, CONTRACTOR SHALL MODIFY LOCATIONS IN THE FIELD PER THE DIRECTION OF THE ENGINEER OR AS NECESSARY TO PROVIDE POSITIVE DRAINAGE TOWARD ROADWAY. THIS WORK SHALL BE INCIDENTAL TO THE CONTRACT AND NO SEPARATE PAYMENT SHALL BE MADE.

SIDEWALK TRANSITION SUMMARY

SHEET	PC		RADIUS 1 (FT)	PRC		RADIUS 2 (FT)	PT		REMARKS
	STATION	OFFSET (FT)		STATION	OFFSET (FT)		STATION	OFFSET (FT)	
R1	10+30.19	31.53 RT	20	10+54.04	22.59 RT	35	10+65.44	24.50 RT	ASKELAND DRIVE - SOUTHEAST
R1	11+23.31	24.50 RT	35	11+34.72	22.59 RT	20	11+55.80	27.80 RT	E. 64TH AVENUE - SOUTHEAST
R1	12+04.66	27.80 RT	20	12+25.75	22.59 RT	35	12+37.15	24.50 RT	E. 64TH AVENUE - NORTHEAST
R2	14+73.00	24.50 RT	35	14+83.93	22.75 RT	25	15+08.71	28.14 RT	E. 63RD AVENUE - SOUTHEAST
R2	15+70.77	28.14 RT	25	15+95.54	22.75 RT	35	16+06.47	24.50 RT	E. 63RD AVENUE - NORTHEAST
R2	18+29.16	24.50 RT	35	18+39.30	23.00 RT	35	18+49.44	21.50 RT	PARCEL 2
R3	22+32.11	21.50 RT	35	22+42.24	23.00 RT	35	22+52.38	24.50 RT	PARCEL 1
R3	24+24.04	24.50 RT	35	24+34.18	23.00 RT	35	24+72.32	35.50 RT	E. DOWLING ROAD NORTHEAST SEE INTERSECTION LAYOUT SHEET R9

SIDEWALK/PATHWAY TRANSITION SUMMARY NOTES:

- SEE SHEET D5, DETAIL 3.

File: I:\JobData\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Roadway Summary Tables.dwg

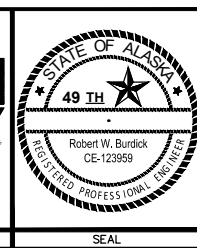
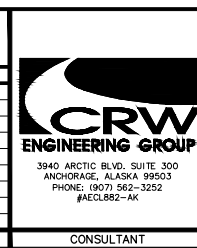
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 BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								



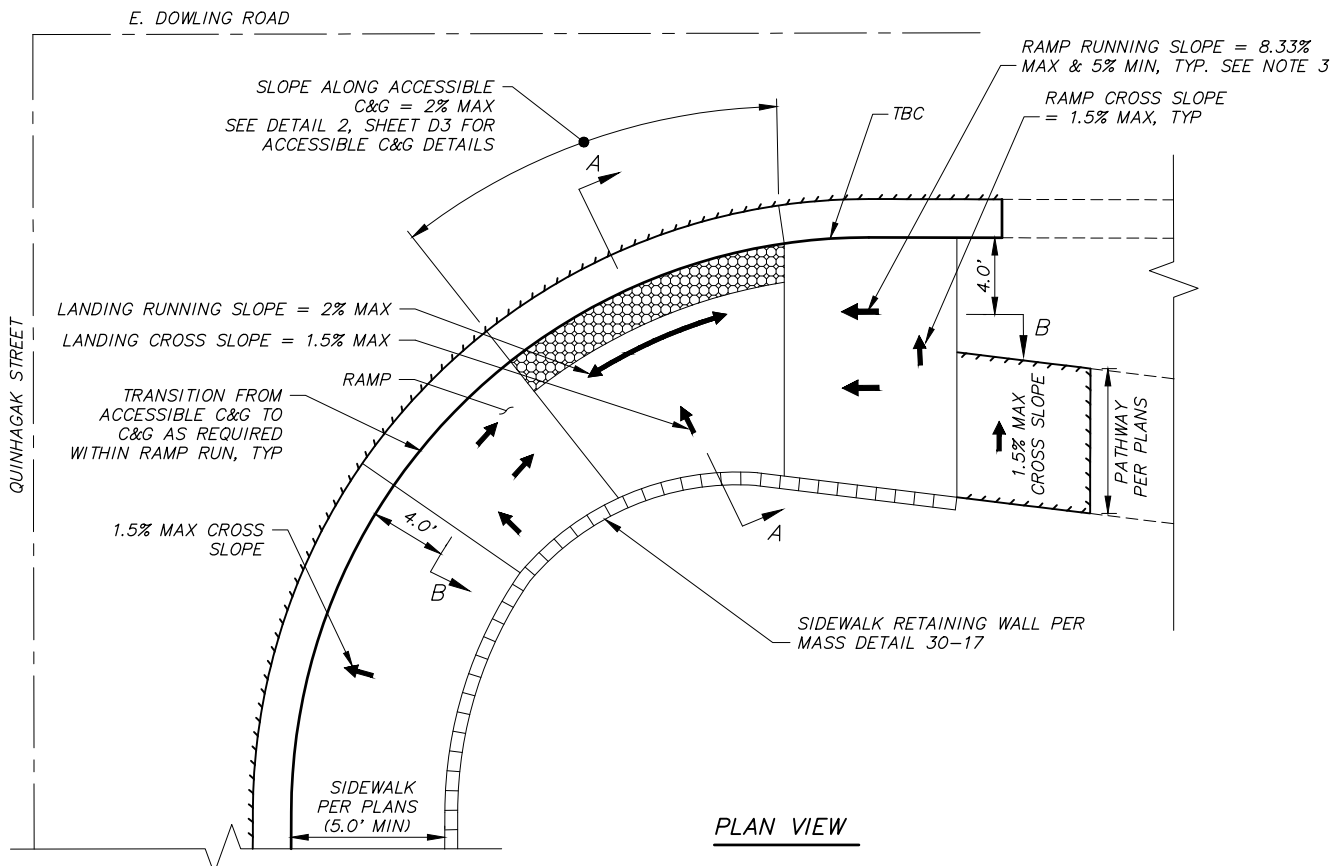
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION SCHED A
 E. DOWLING ROAD TO ASKELAND DRIVE

ROADWAY SUMMARY TABLES

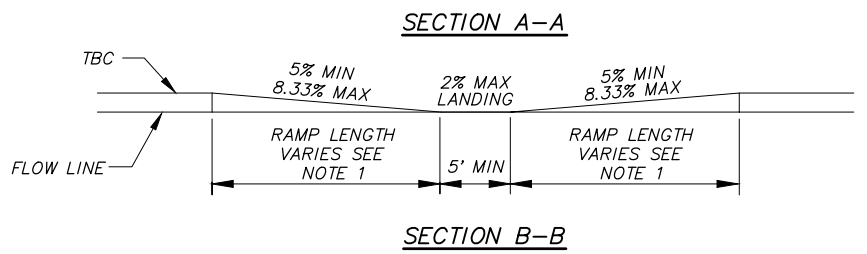
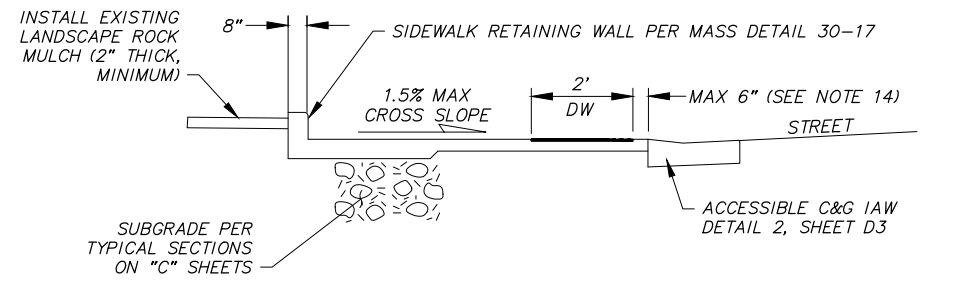
SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET T3 of T3

File: E:\webdata\10155.00_Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\01 Civil\10155.00_Roadway_Details.dwg



SHEET NOTES:

- SEE SHEETS R6-R9 FOR CURB RAMP TYPES, LOCATIONS, RAMP, LANDING AND FLARE LENGTHS AND ELEVATIONS. RAMP/FLARE/LANDING LENGTH FOR PARALLEL CURB RAMPS SHALL BE AS MEASURED 4' OFF BACK OF CURB.
- NOTIFY ENGINEER PRIOR TO INSTALLATION OF CONCRETE IF MAXIMUM/MINIMUM SLOPES CANNOT BE MAINTAINED.
- FOR PARALLEL CURB RAMPS, RAMPS SHALL BE 15 FEET MAXIMUM. RAMPS SHALL HAVE THE OUTSIDE EDGES AND JOINTS TRIMMED WITH A 1/4-INCH RADIUS EDGING TOOL.
- ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
- MINIMUM FLOWLINE SLOPE IN CURB RETURN IS 0.5%, UNLESS OTHERWISE NOTED.
- PROVIDE CONSTANT FLOWLINE BETWEEN CHANGE IN CURB TYPE.
- CONSTRUCT SIDEWALK ADJACENT TO CURB RAMP PER THE TYPICAL SECTIONS SHOWN ON THE "C" SHEETS.
- PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "P.C.C. CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
- FORM BACKING CURB AS DIRECTED BY THE ENGINEER TO MATCH EXISTING GROUND. PAYMENT FOR THIS CURB SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE. IF EXISTING GROUND BEHIND SIDEWALK IS GRAVEL OR GRASS, GRADE TO MATCH EXISTING GROUND. PAYMENT FOR GRADING SHALL BE MADE UNDER THE BID ITEM "P.C.C. CURB RAMP (6" THICK)" AND NO ADDITIONAL PAYMENT SHALL BE MADE. 4" TOPSOIL AND SEEDING SHALL BE PLACED ON DISTURBED GRASS AREAS.
- CONSTRUCT RAMPS AND LANDINGS WITH A BROOM FINISH RUNNING PERPENDICULAR TO THE DIRECTION OF TRAVEL.
- INSTALL YELLOW ADA APPROVED DETECTABLE WARNINGS (DW) PANELS UNLESS OTHERWISE NOTED IN ACCORDANCE WITH MANUFACTURER'S RECOMMENDATIONS AND THESE DRAWINGS. SET DETECTABLE WARNINGS SO THAT THE FIELD AREA AT THE BASE OF THE DOMES IS FLUSH WITH THE SURROUNDING CONCRETE. THERE SHALL BE NO LIP AT THE EDGE OF THE DETECTABLE CURB WARNINGS. SEE DETAIL 1, SHEET D3.
- DETECTABLE WARNINGS DOMES AT PARALLEL CURB RAMPS SHALL BE ALIGNED ON A SQUARE GRID IN THE PREDOMINATE DIRECTION OF TRAVEL TO PERMIT WHEELS TO ROLL BETWEEN DOMES.
- RAMP LOCATIONS MAY BE ADJUSTED TO ENSURE MINIMUM 48" CLEARANCE AROUND APPURTENANCES SUCH AS SIGNAL POLES, POWER POLES, LIGHT POLES, J-BOXES, SIGNS, CATCH BASINS AND MANHOLES. PRIOR TO PLACEMENT OF CONCRETE AND APPURTENANCES, THE RAMP LAYOUT AND LOCATION SHALL BE APPROVED BY THE ENGINEER.
- GAP BETWEEN DETECTABLE WARNING PANELS AND BACK OF CURB ONLY ALLOWABLE AT CENTER OF CURB RAMPS. CORNERS OF DETECTABLE WARNINGS SHALL BE FLUSH WITH BACK OF CURB. IF REQUIRED BY THE ENGINEER CONTRACTOR SHALL CUT DETECTABLE WARNING PANELS PER THE MANUFACTURER'S RECOMMENDATIONS. CUTTING DW PANELS SHALL BE INCIDENTAL TO 30.04 DETECTABLE WARNINGS PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.



TYPICAL PARALLEL CURB RAMP AT CORNER LOCATION WITH CONNECTING SIDE STREET PATHWAY

SCALE: NTS

1

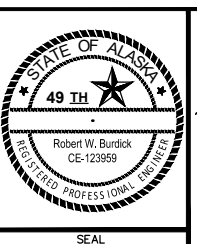
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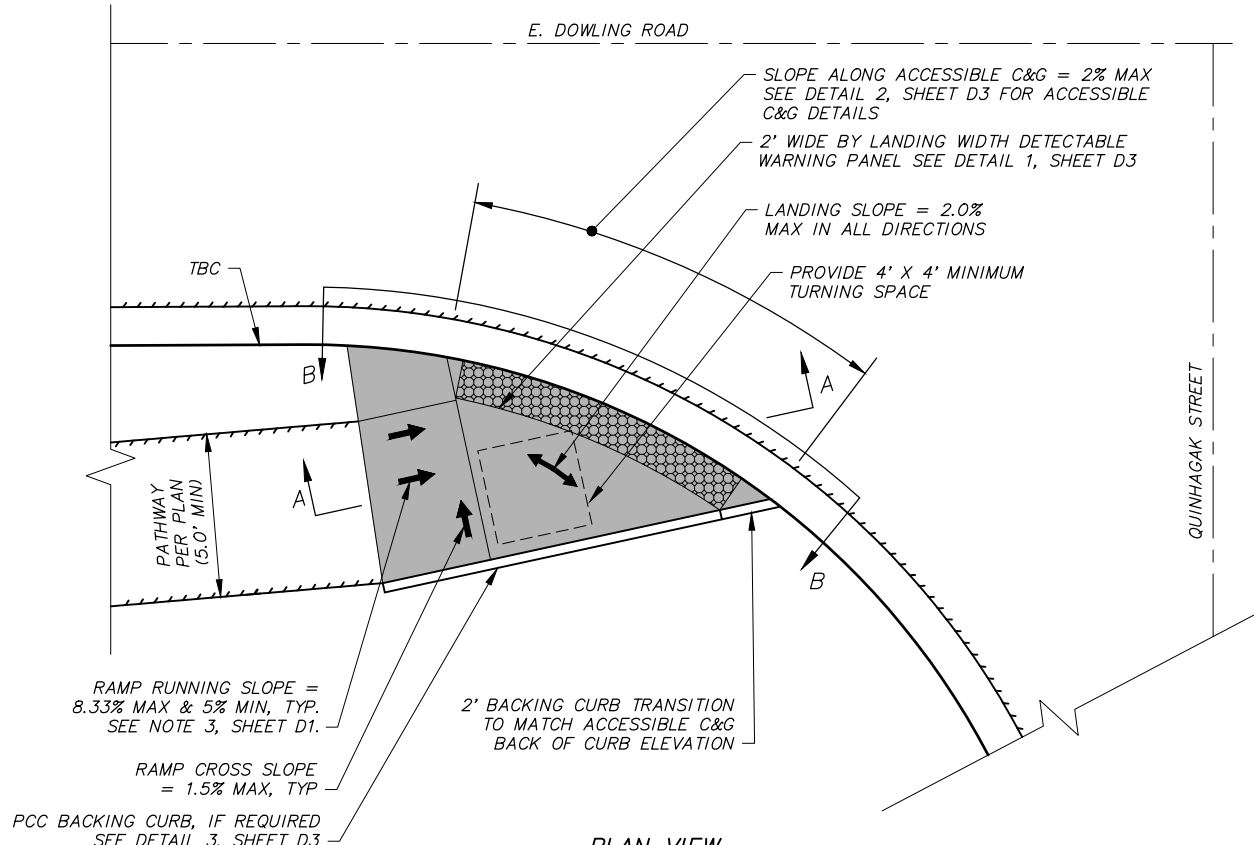
FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL

CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECL882-AK



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY DETAILS
 CURB RAMPS
 SCALE: HOR. N/A VER. N/A
 GRID: SW2033
 DATE: DEC 2024 STATUS: 95%
 SHEET: D1 of D8



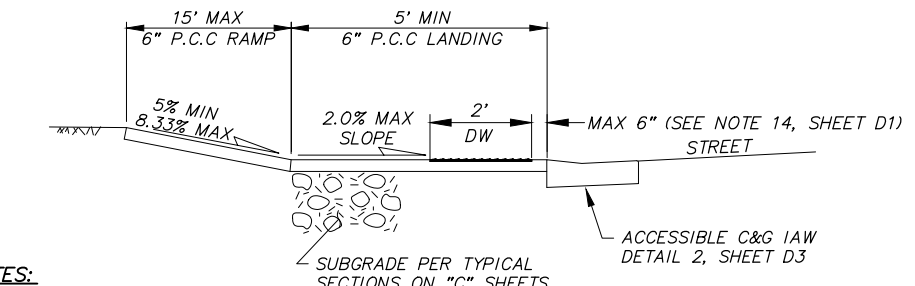
PLAN VIEW

LEGEND:

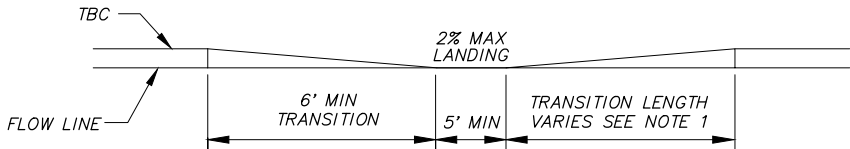
LIMITS OF BROOM FINISH, LIMITS OF 6" THICK PCC, LIMITS OF PAYMENT FOR CURB RAMP

CURB RAMP NOTES:

1. SEE SHEET NOTES ON SHEET D1.



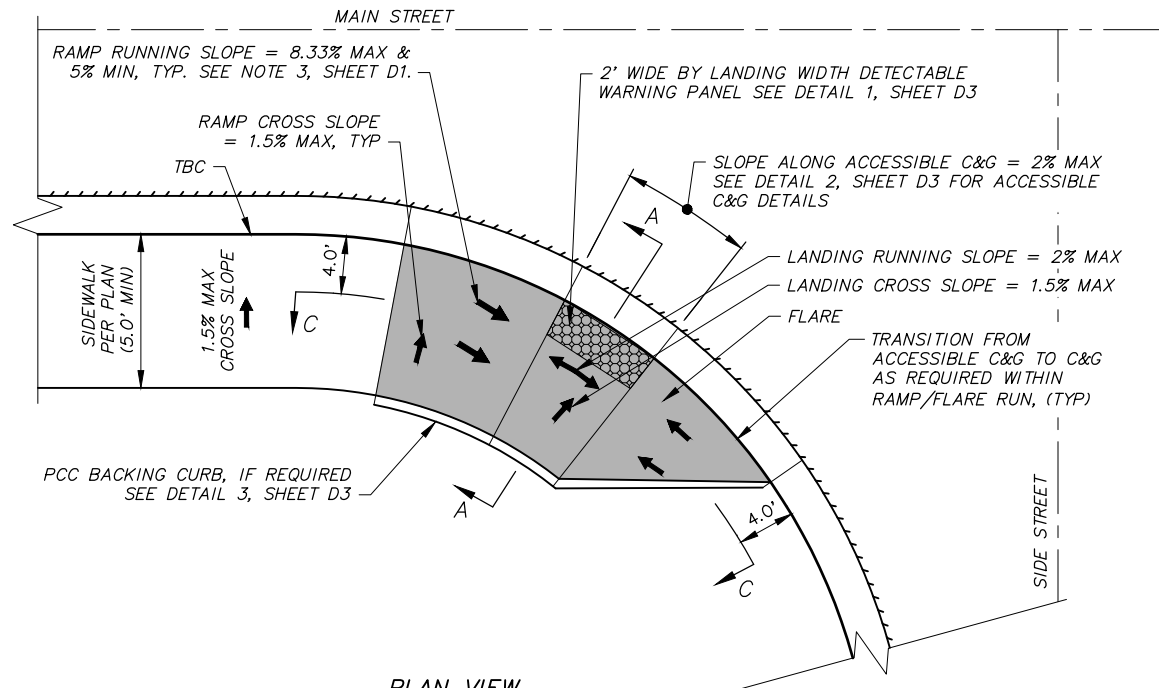
SECTION A-A



SECTION B-B

TYPICAL UNIDIRECTIONAL PATHWAY CURB RAMP

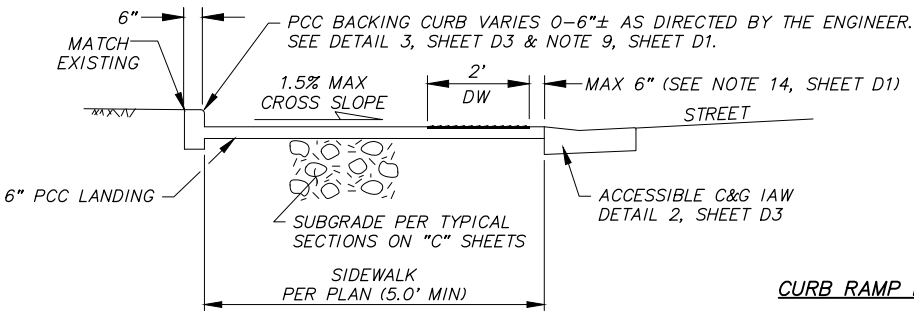
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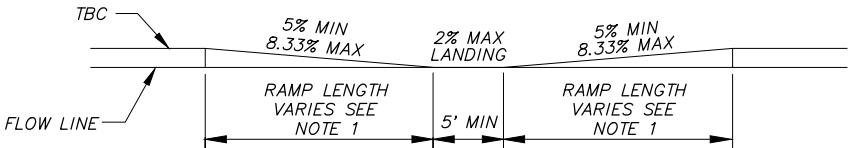
PLAN VIEW

LEGEND:

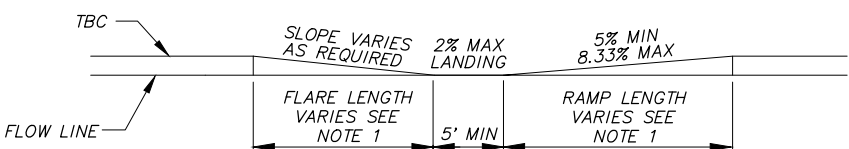
LIMITS OF BROOM FINISH, LIMITS OF 6" THICK PCC, LIMITS OF PAYMENT FOR CURB RAMP



SECTION A-A



SECTION B-B



SECTION C-C

CURB RAMP NOTES:

1. SEE SHEET NOTES ON SHEET D1.

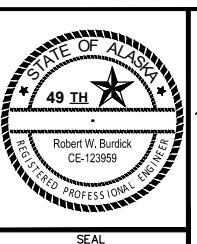
TYPICAL PARALLEL CURB RAMP AT CORNER LOCATION WITHOUT CONNECTING SIDE STREET SIDEWALK - PLAN VIEW

SCALE: NTS

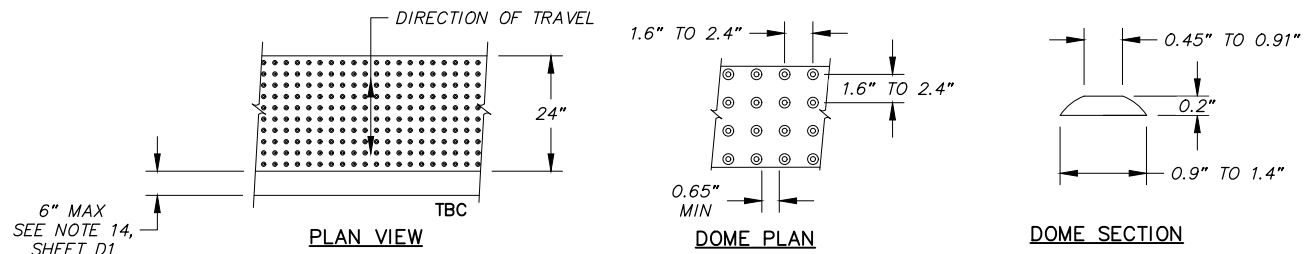
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DATA TRANSFER CHECKED BY: _____	TITLE: _____
COMPANY: _____	DATE: _____
BY: _____	DATE: _____

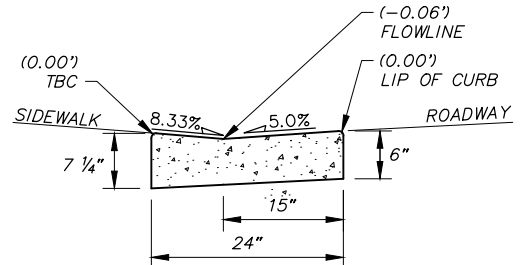
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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
PRELIMINARY/FINAL	RB	JK	INSPECTOR							
MUNICIPAL/STATE	RB	JK								
PLAN CHECK			CONSTRUCTION RECORD							
			VERTICAL DATUM							
			REVISIONS							
			CONSULTANT							
			SEAL							



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT	
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY DETAILS	
CURB RAMPS	
SCALE HOR. N/A VER. N/A	GRID 5W2033 DATE DEC 2024 STATUS 95% SHEET D2 of D8



1 DETECTABLE WARNING PANEL
SCALE: NTS

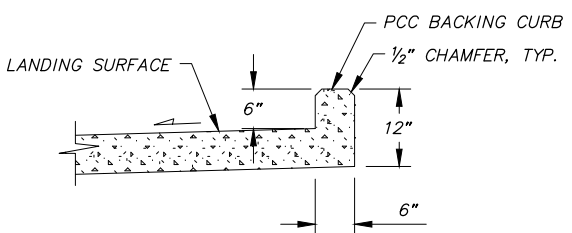


PCC CURB AND GUTTER TYPE 1A
FOR USE IN CURB RAMPS WITH TYPE 1 C&G.

ACCESSIBLE CURB & GUTTER NOTES:

1. TRANSITION CURBS TO MAINTAIN CONSTANT FLOWLINE ACROSS CURB RAMP AND AROUND CURB RETURN IAW PLANS.
2. PAYMENT FOR ALL PCC CURB AND GUTTER, INCLUDING MODIFIED AND TRANSITIONAL CURB, SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.

2 TYPE 1A ACCESSIBLE CURB & GUTTER SECTION
SCALE: NTS

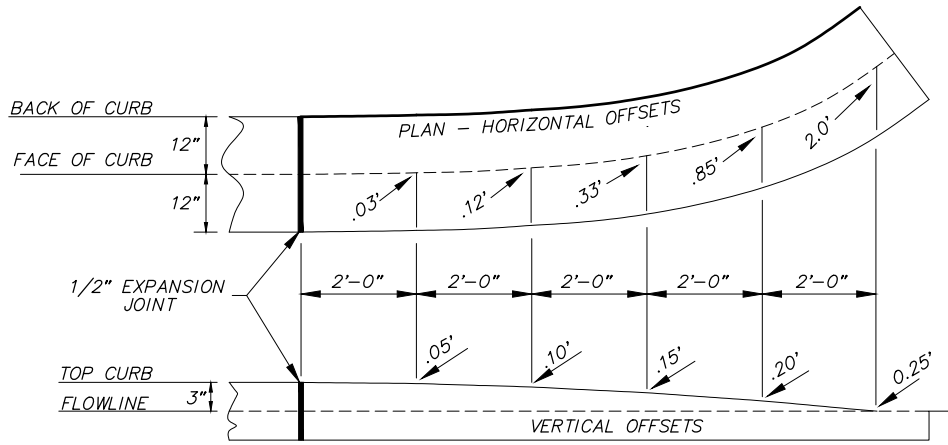


MONOLITHIC

BACKING CURB DETAIL NOTES:

1. THE TOP OF BACKING CURB SHALL TRANSITION BACK TO TOP OF SIDEWALK AT TOP RAMP SECTION OF CURB RAMP.

3 BACKING CURB DETAIL
SCALE: NTS



TYPE 2 CURB AND GUTTER TERMINATION TRANSITION NOTES:

1. PAYMENT FOR TYPE 2 CURB AND GUTTER TERMINATION TRANSITION SHALL BE PAID UNDER THE BID ITEM "P.C.C. CURB AND GUTTER (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.

4 TYPE 2 CURB AND GUTTER TERMINATION TRANSITION
SCALE: NTS

RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
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CONTRACTOR: _____ TITLE: _____ DATE: _____
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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
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WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								
PLAN CHECK			CONSTRUCTION RECORD							
			VERTICAL DATUM							
			REVISIONS							
			CONSULTANT							

CRW ENGINEERING GROUP
3940 ARCTIC BLVD. SUITE 300
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STATE OF ALASKA
49 TH
Robert W. Burdick
CE-123959
REGISTERED PROFESSIONAL ENGINEER

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

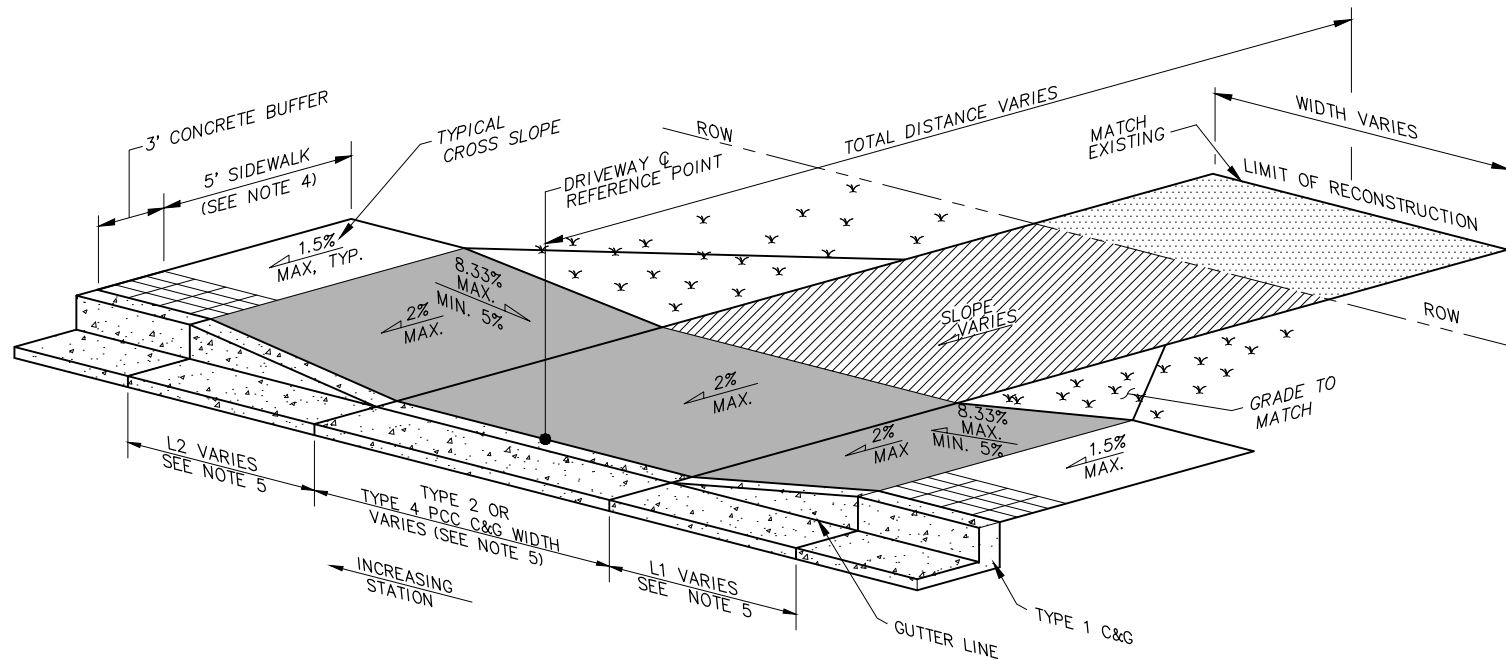
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A

ROADWAY DETAILS

CURB RAMPS

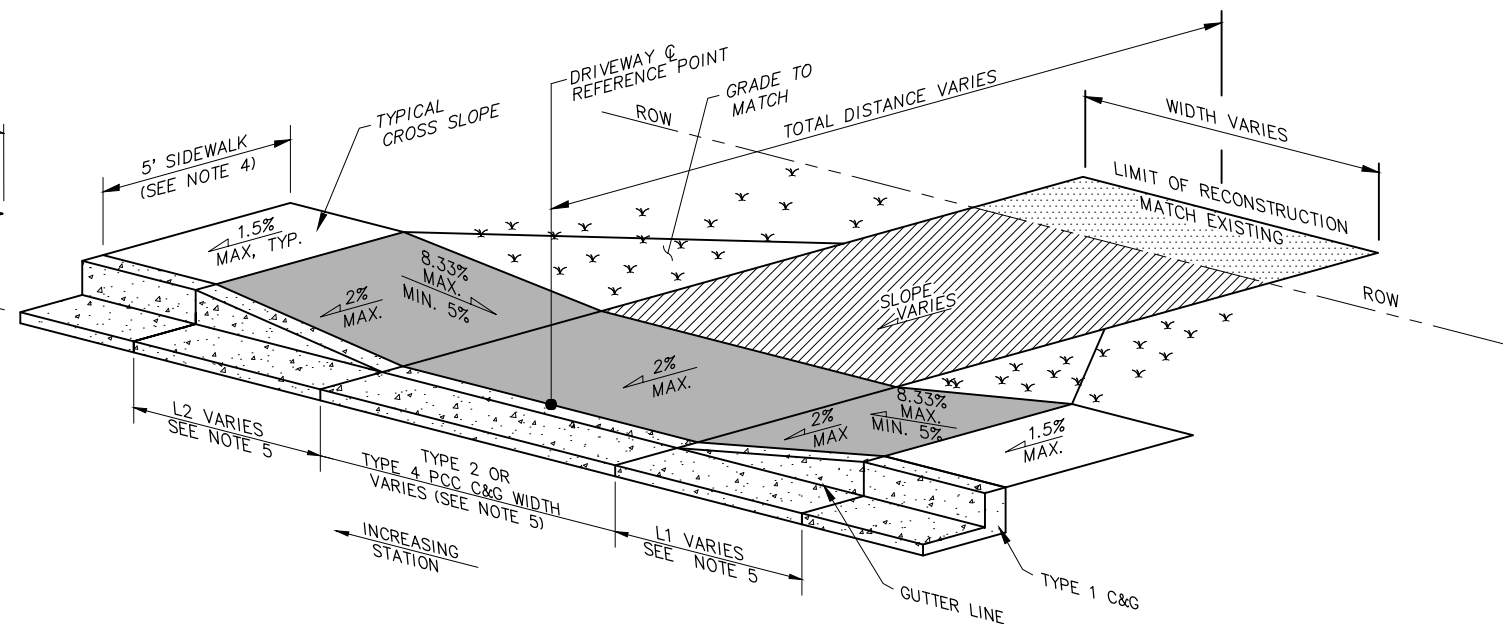
SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET D3 of D8

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\01_Civil\10155.00_Roadway_Details.dwg



TYPICAL DRIVEWAY CURB CUT WITH SIDEWALK & BUFFER

SCALE: NTS



TYPICAL DRIVEWAY CURB CUT WITH SIDEWALK & NO BUFFER

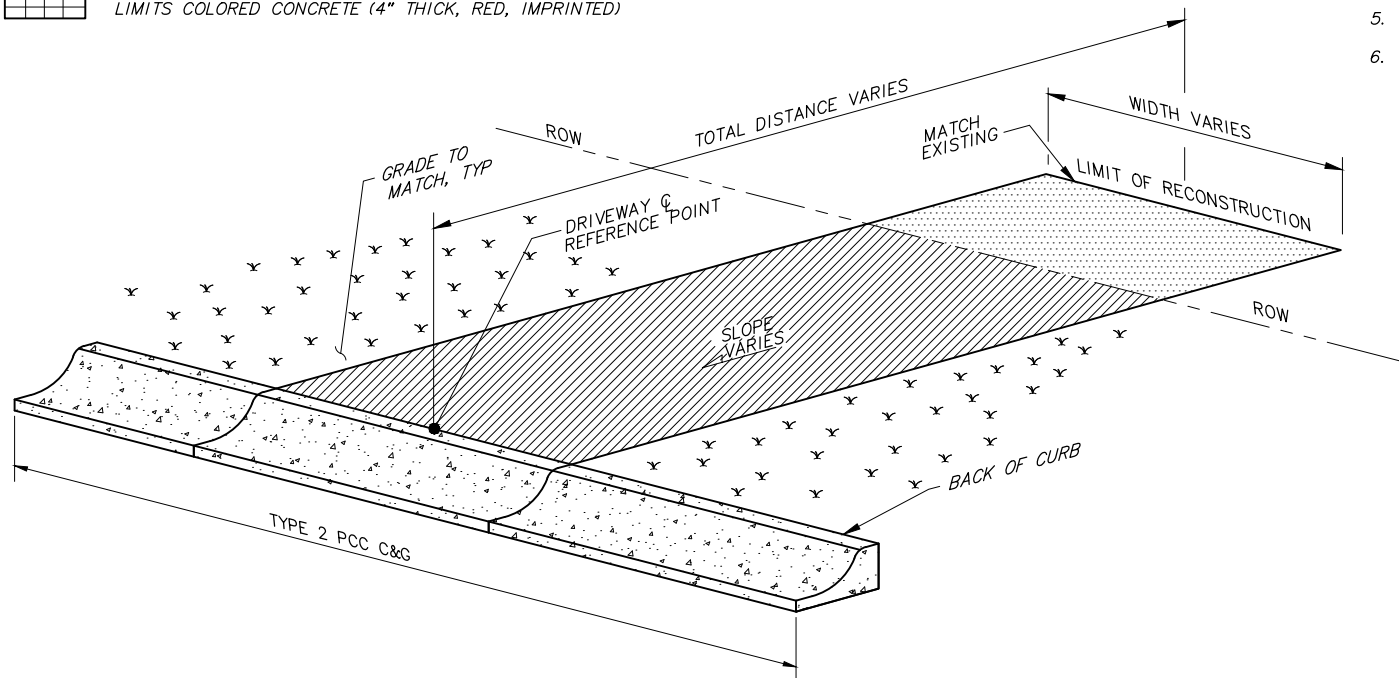
SCALE: NTS

SHEET LEGEND:

- LIMITS OF 2" AC PAVING FOR DRIVEWAY UON
- SURFACE TYPE VARIES, SEE NOTE 5
- LIMITS OF P.C.C. SIDEWALK (6" THICK, STANDARD FINISH), SEE NOTE 4
- LIMITS COLORED CONCRETE (4" THICK, RED, IMPRINTED)

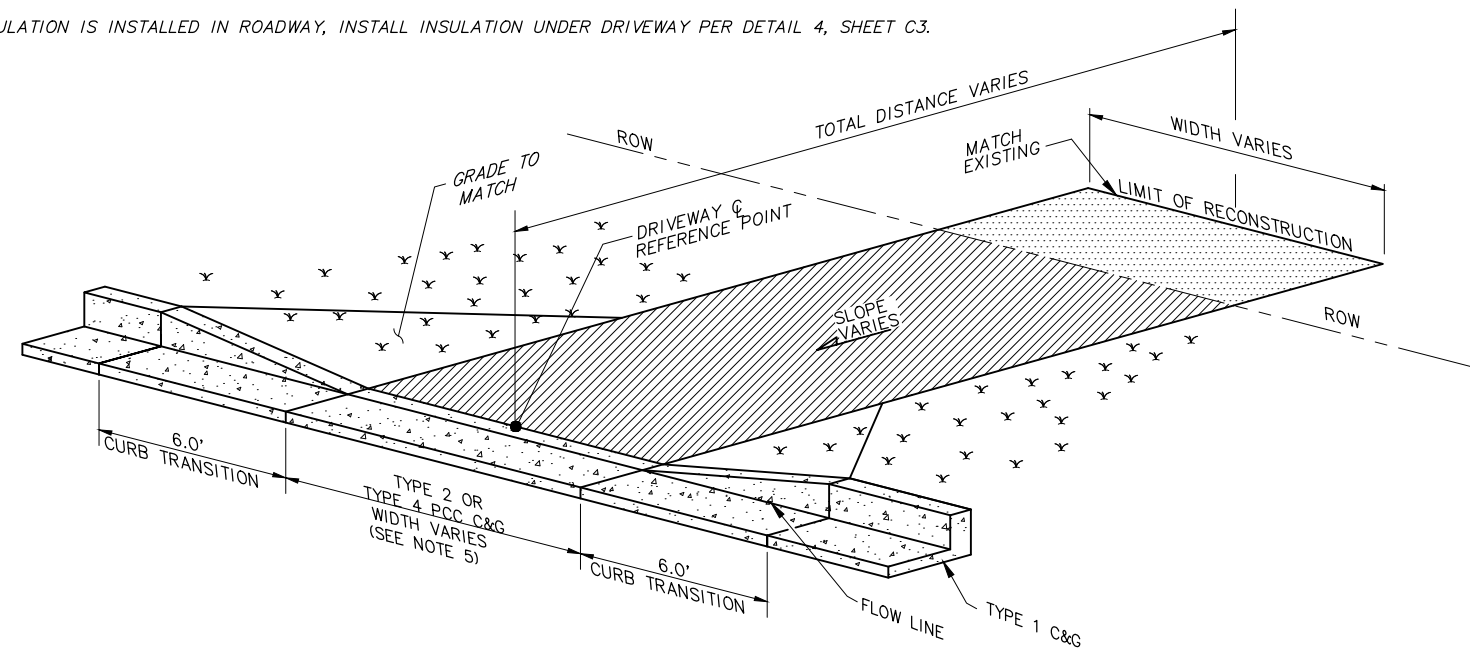
SHEET DRIVEWAY NOTES:

1. ALL SLOPES ARE IN REFERENCE TO THE HORIZONTAL.
2. PAYMENT FOR PCC CURB & GUTTER (ALL TYPES) AND TRANSITION C&G SHALL BE PAID UNDER THE BID ITEM "PCC CURB & GUTTER, (ALL TYPES)" AND NO SEPARATE PAYMENT SHALL BE MADE.
3. CENTER THE PROPOSED DRIVEWAY ENTRANCES ON DRIVEWAY CENTERLINE REFERENCE POINT AS SHOWN IN THE 20.28 RECONSTRUCT DRIVEWAY SUMMARY TABLES.
4. INCREASE SIDEWALK THICKNESS TO 6" ACROSS LANDINGS AND RAMP TRANSITIONS AND ADD WELDED STEEL WIRE REINFORCEMENT PER THE SPECIFICATIONS.
5. SEE 20.28 DRIVEWAY RECONSTRUCTION SUMMARY TABLE "T" SHEETS AND DRIVEWAY RECONSTRUCTION PLANS, FOR INDIVIDUAL DRIVEWAY SPECIFICS.
6. WHERE INSULATION IS INSTALLED IN ROADWAY, INSTALL INSULATION UNDER DRIVEWAY PER DETAIL 4, SHEET C3.



TYPICAL DRIVEWAY CURB CUT WITHOUT SIDEWALK (TYPE 2 CURB)

SCALE: NTS



TYPICAL DRIVEWAY CURB CUT WITHOUT SIDEWALK (TYPE 1 CURB)

SCALE: NTS

RECORD DRAWING
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 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
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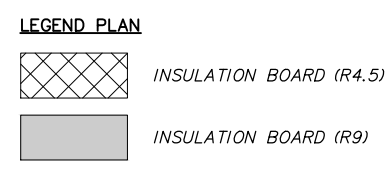
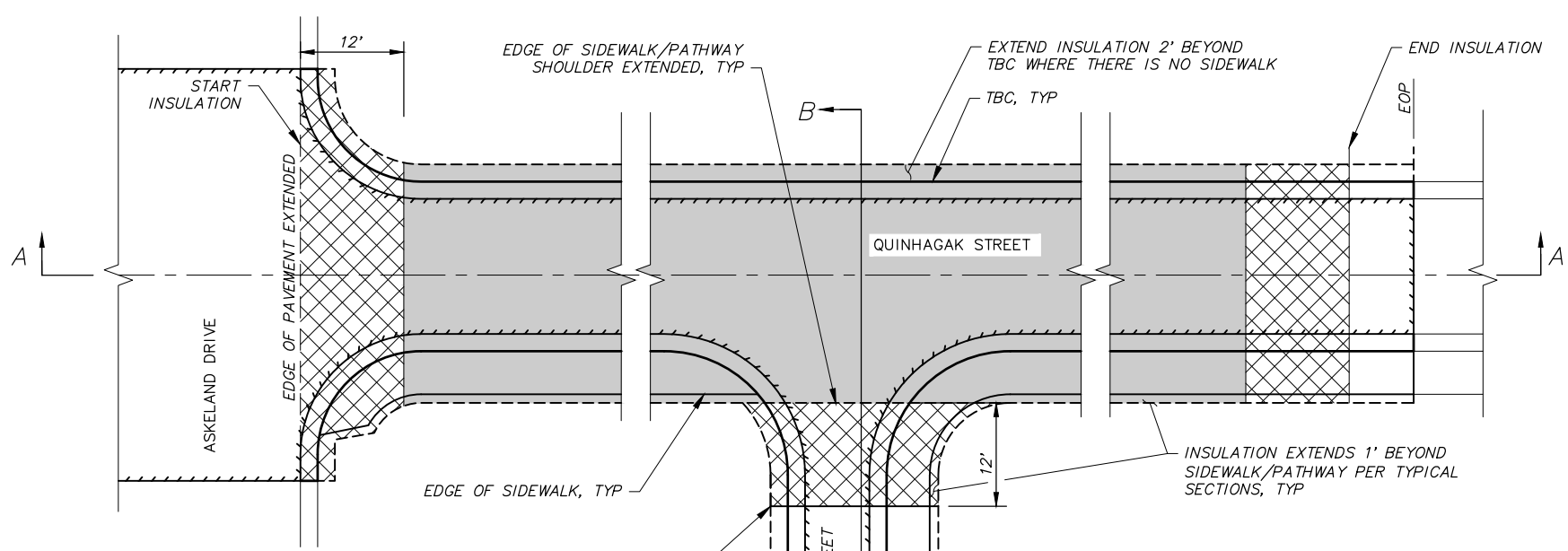
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UNIVERSITY OF ANCHORAGE

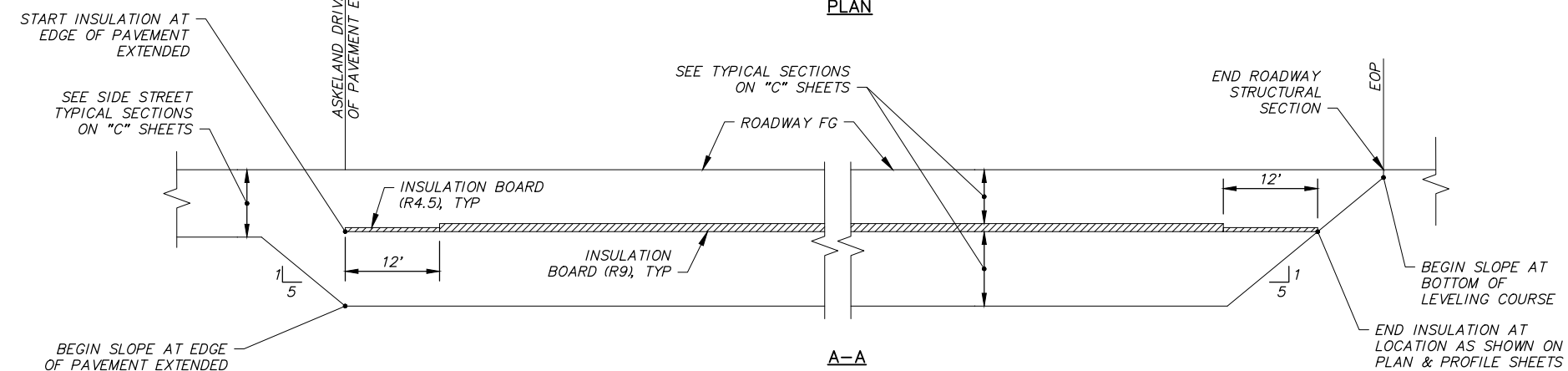
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY DETAILS
 DRIVEWAYS
 SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET D4 of D8

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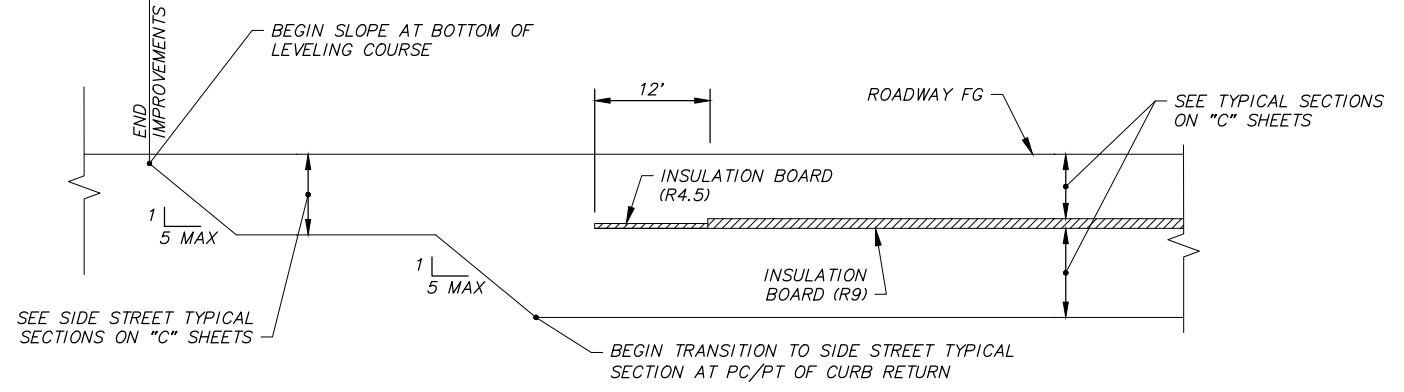
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PLAN



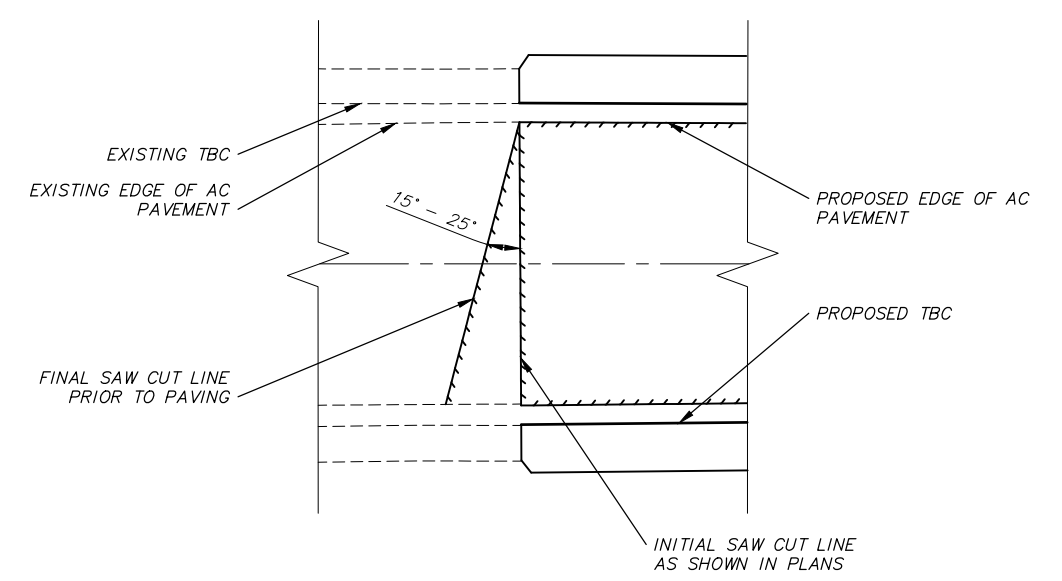
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B-B

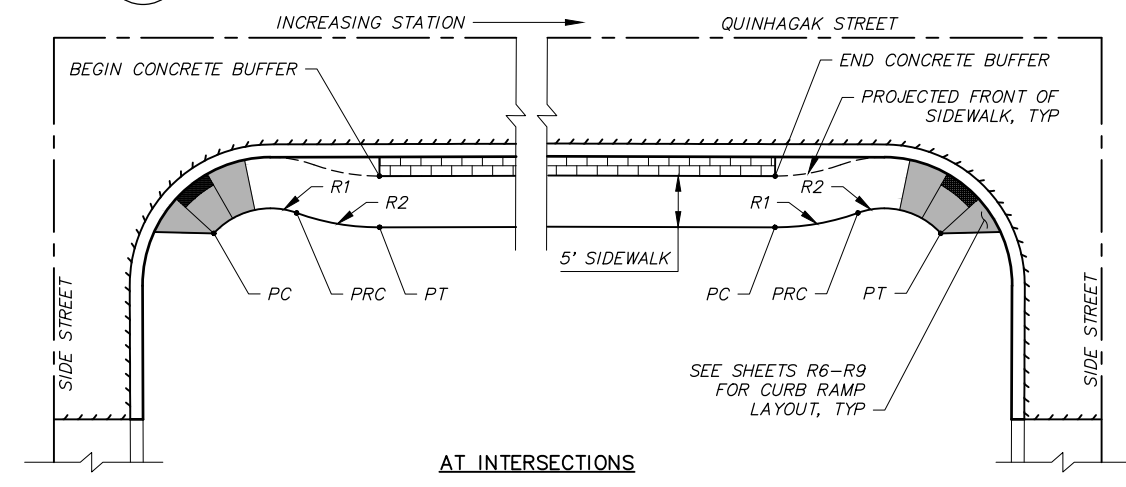
BOARD INSULATION AND EXCAVATION TRANSITION DETAIL

SCALE: NTS

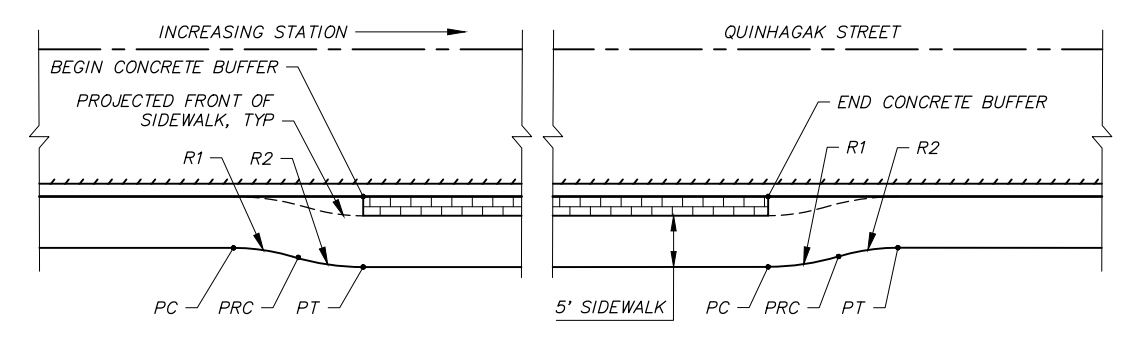


TRANSVERSE SAW CUT JOINT DETAIL

SCALE: NTS



AT INTERSECTIONS



OTHER AREAS

SIDEWALK TRANSITION DETAIL NOTES:

- SEE SIDEWALK TRANSITION SUMMARY TABLE ON SHEET T3 FOR LAYOUT POINTS.

SIDEWALK TRANSITION DETAIL

SCALE: NTS

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COMPANY: _____ DATE: _____

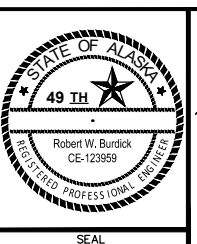
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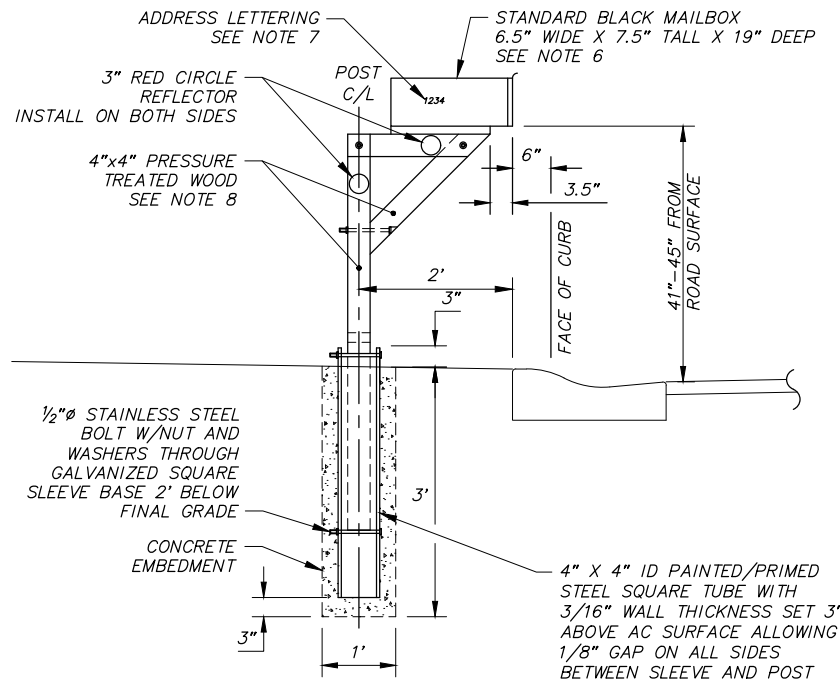
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BY: _____

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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW	STAKING							
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
PRELIMINARY/FINAL	RB	JK	INSPECTOR							
MUNICIPAL/STATE	RB	JK								
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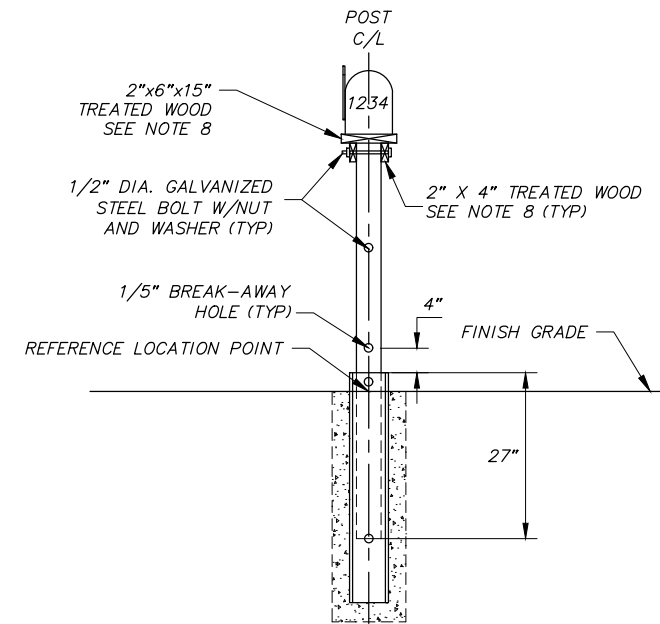


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE		SCHED A
ROADWAY DETAILS			
MISCELLANEOUS DETAILS			
SCALE	HOR. N/A VER. N/A	GRID 5W2033 DATE DEC 2024	STATUS 95%
			D5 of D8 SHEET



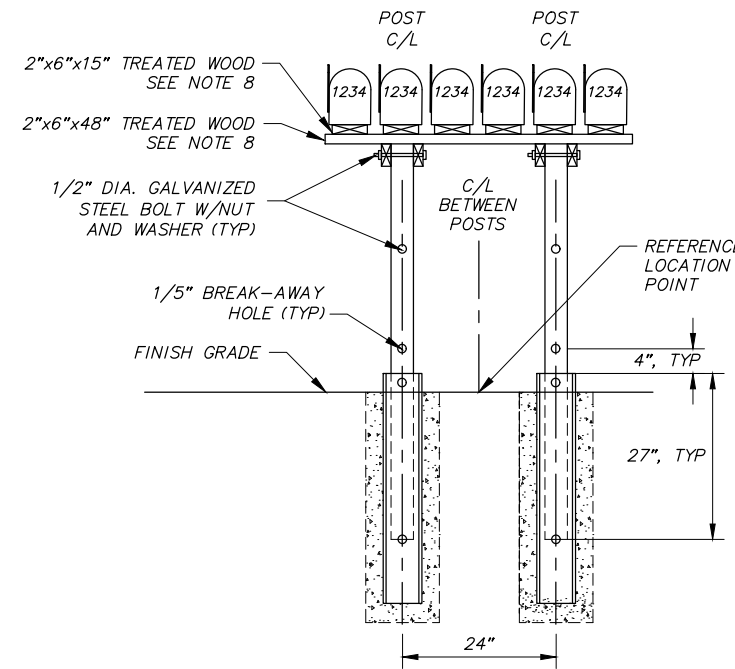
TYPICAL WOOD POST MAILBOX INSTALLATION (SIDE VIEW)

SCALE: NTS



TYPICAL SINGLE MAILBOX INSTALLATION (FRONT VIEW)

SCALE: NTS



TYPICAL COMBINED MAILBOX INSTALLATION (FRONT VIEW)

SCALE: NTS

TYPICAL WOOD POST MAILBOX INSTALLATION NOTES:

- SEE "RELOCATE MAILBOX" TABLE, DEMOLITION SHEETS & ROADWAY SHEETS FOR LOCATING MAILBOXES ALONG ROADWAY. LOCATIONS ARE APPROXIMATE, VERIFY LOCATION WITH ENGINEER PRIOR TO INSTALLATION.
- RELOCATE COMBINED MAILBOXES TO THE PROPOSED STATION AND 2' BEHIND THE TOP BACK OF CURB.
- CUT OFF EXCESS BOLTS AND FILE SMOOTH AFTER TIGHTENING.
- MAILBOXES AND SUPPORTS SHALL CONFORM WITH U.S. POSTAL SERVICE REGULATIONS.
- NEWSPAPER RECEPTACLES SHALL CONFORM TO THE SAME SETBACK AND SUPPORT REGULATIONS AS MAILBOXES. WHERE NEWSPAPER RECEPTACLES AND MAILBOXES ARE TO BE MOUNTED TOGETHER, THE NEWSPAPER RECEPTACLE SHALL BE MOUNTED BELOW THE BOTTOM SURFACE OF THE MAILBOX. RELOCATION OF EXISTING NEWSPAPER RECEPTACLES IS INCIDENTAL TO THE RELOCATE MAILBOX BID ITEM.
- CONTRACTOR SHALL COORDINATE WITH THE MOA AND ENGINEER IN THE FIELD REGARDING MAILBOX SUBSTITUTIONS OR MAILBOX SIZING, PRIOR TO ORDERING MATERIALS.
- CONTRACTOR SHALL INSTALL MAILBOX ADDRESS LABELS TO MATCH EXISTING LABELS. ADDRESS LABELS SHALL BE A MINIMUM OF 1" IN HEIGHT AND INSTALLED ON THE SIDE OF THE MAILBOX VISIBLE FROM ON COMING TRAFFIC. ADDRESS LABELS SHOULD BE CENTERED BOTH VERTICAL AND HORIZONTAL ON MAILBOX.
- ALL WOOD SHALL BE PRESSURE TREATED WOOD SEALED WITH A SEMI-TRANSPARENT OIL BASED STAIN BROWN IN COLOR. SUBMIT COLOR SAMPLE FOR APPROVAL.
- CONTRACTOR TO SEAL THE TUBE BASE WHEN SETTING CONCRETE TO AVOID CONCRETE FROM ENTERING THE TUBE.
- THE LOCATION OF EXISTING FEATURES AND UTILITIES SHOWN ON THE PLANS ARE APPROXIMATE. IT SHALL BE THE CONTRACTOR'S RESPONSIBILITY TO FIELD VERIFY HORIZONTAL AND VERTICAL LOCATION OF ALL ENCOUNTERED UTILITIES AND RECORD ANY CHANGES ON THE RECORD DRAWINGS.
- CONTRACTOR MAY ADJUST CONCRETE EMBEDMENT DEPTH IF UTILITY CONFLICTS ARE ENCOUNTERED.
- MAILBOX ITEMS CALLED OUT IN DETAIL 1 SHALL APPLY TO MAILBOX DETAILS 2 & 3.

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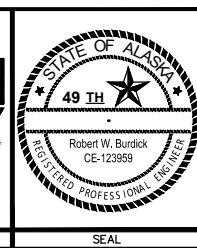
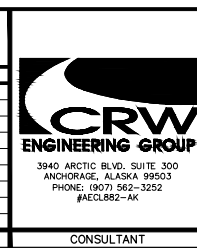
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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
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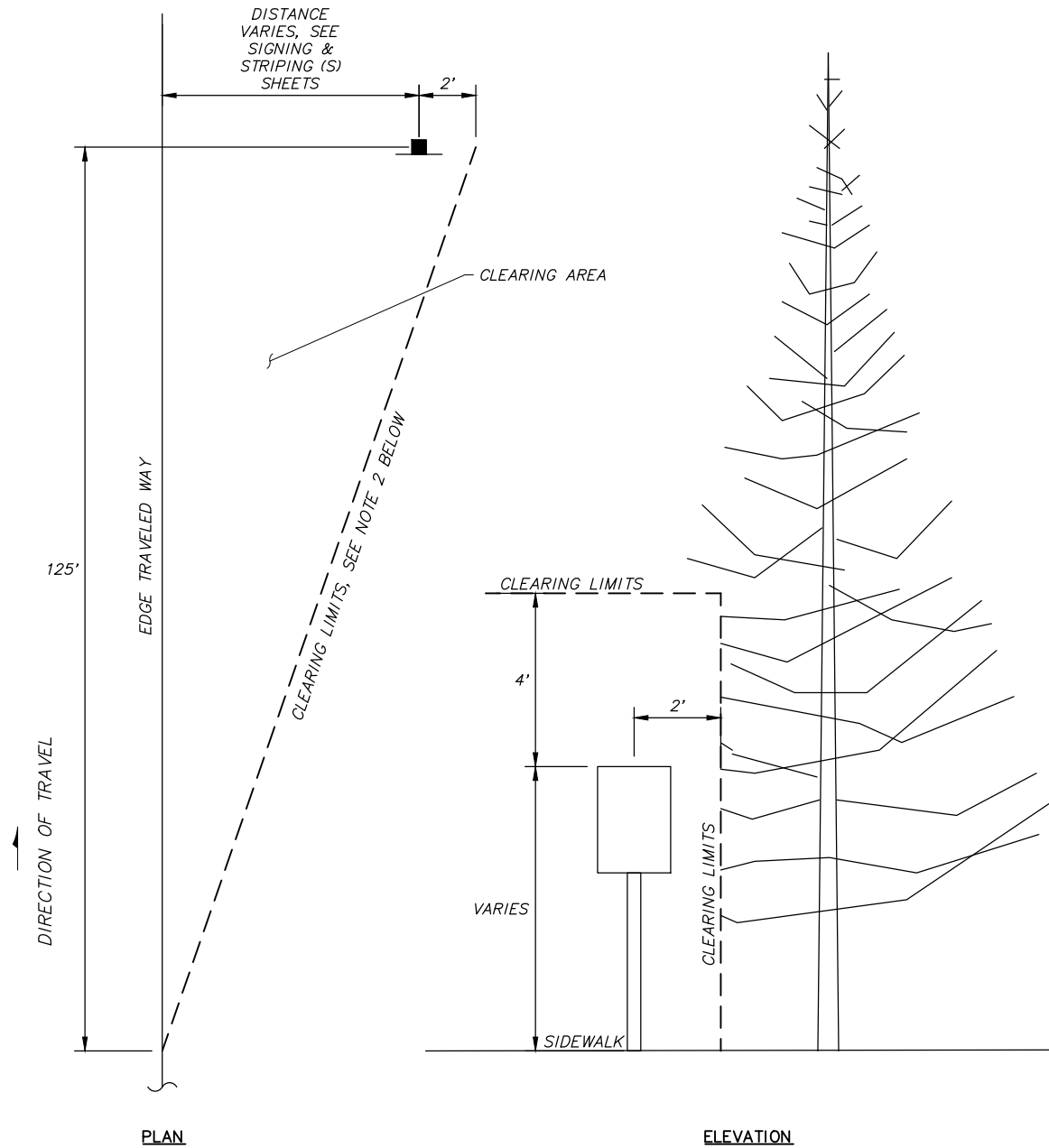
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION SCHED A
E. DOWLING ROAD TO ASKELAND DRIVE

ROADWAY DETAILS

MAILBOX

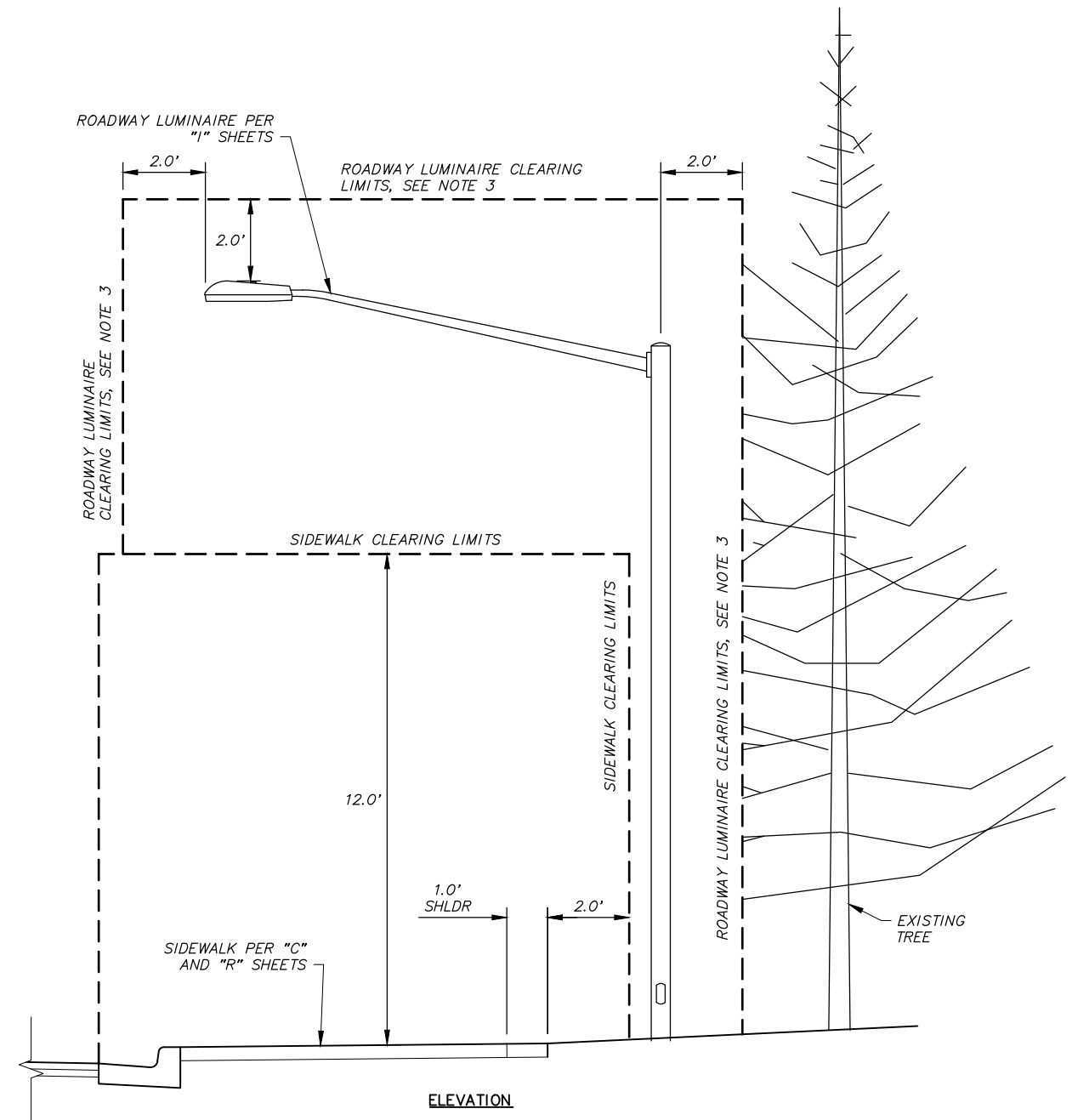
SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: D6 of D8



SIGN SIGHT DISTANCE CLEARING DETAIL NOTES:

- SIGN SIGHT DISTANCE CLEARING SHALL BE INCIDENTAL TO SECTION 20.04 CLEARING AND GRUBBING PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- MAINTAIN CLEARING LIMITS WITHIN AVAILABLE RIGHT-OF-WAY.
- ALL CLEARING ACTIVITIES SHALL BE PERFORMED BY AN ISA CERTIFIED ARBORIST AND FOLLOW ANSI A300, PART 1, STANDARD PRACTICES AND ANSI Z133.1, ARBORICULTURAL OPERATIONS SAFETY.

1 SIGN SIGHT DISTANCE CLEARING DETAIL
SCALE: NTS



SIDEWALK AND ROADWAY LUMINAIRE CLEARING DETAIL NOTES:

- SIDEWALK AND ROADWAY LUMINAIRE CLEARING SHALL BE INCIDENTAL TO SECTION 20.04 CLEARING AND GRUBBING PAY ITEM AND NO SEPARATE PAYMENT SHALL BE MADE.
- MAINTAIN CLEARING LIMITS WITHIN AVAILABLE RIGHT-OF-WAY OR TCP.
- ROADWAY LUMINAIRE CLEARING LIMITS SHALL INCLUDE 20 FEET UP STATION AND DOWN STATION ALONG THE ROADWAY.
- ALL CLEARING ACTIVITIES SHALL BE PERFORMED BY AN ISA CERTIFIED ARBORIST AND FOLLOW ANSI A300, PART 1, STANDARD PRACTICES AND ANSI Z133.1, ARBORICULTURAL OPERATIONS SAFETY.

2 SIDEWALK AND ROADWAY LUMINAIRE CLEARING DETAIL
SCALE: NTS

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Roadway Details.dwg

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STATE OF ALASKA
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 Robert W. Burdick
 CE-123959
 REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED A

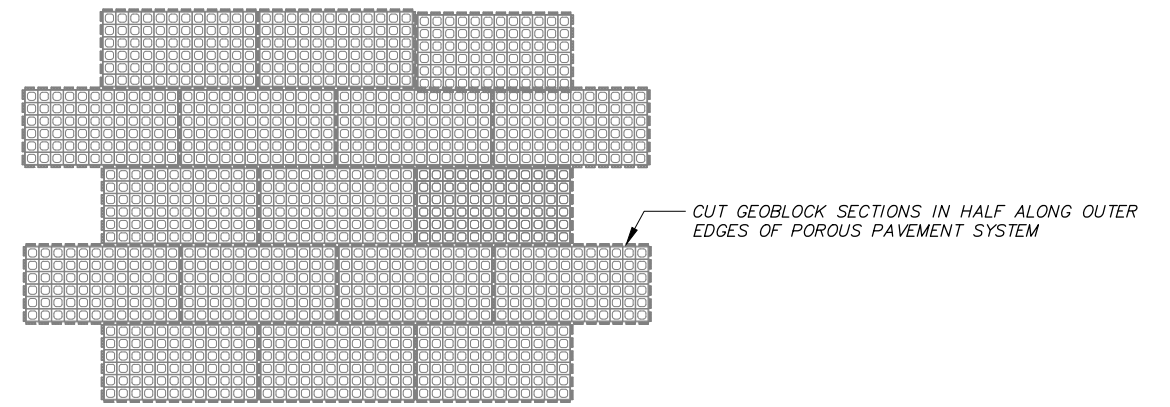
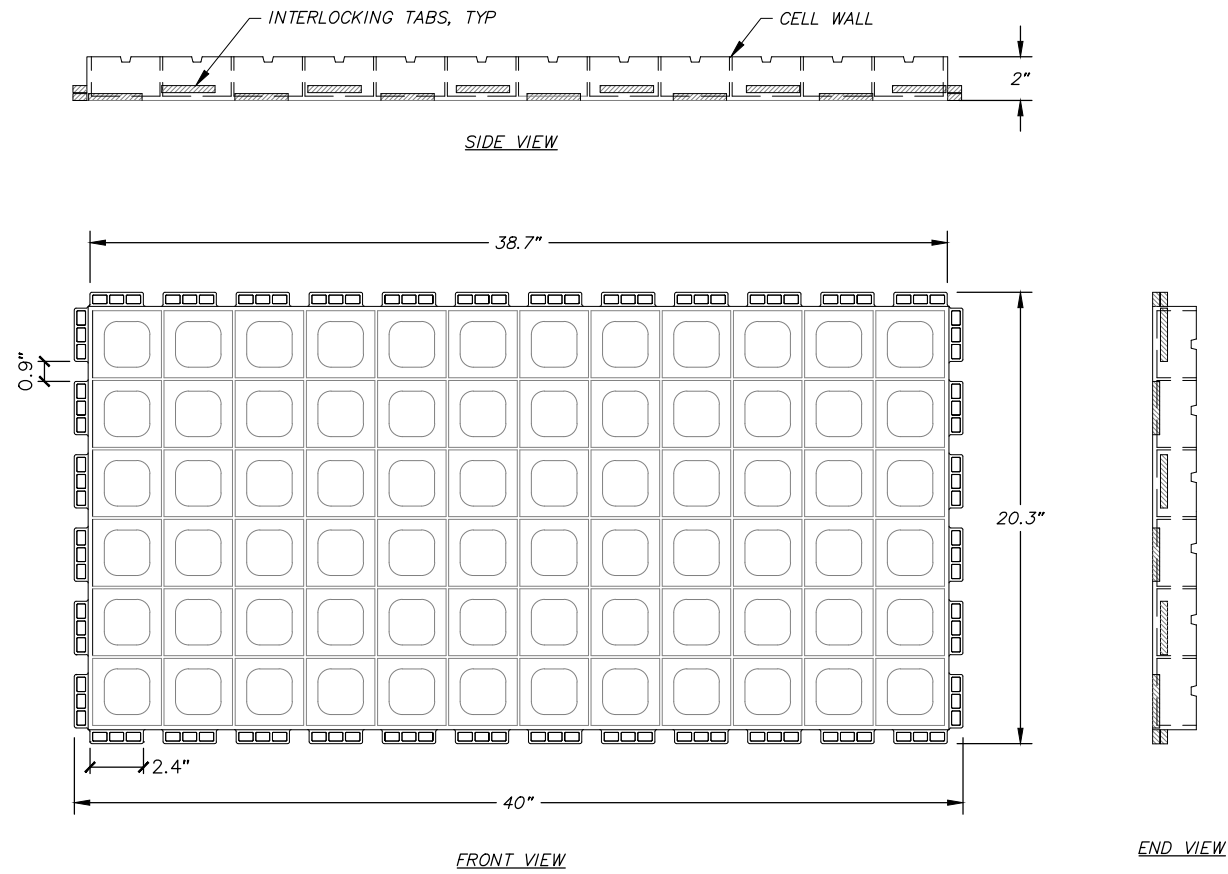
ROADWAY DETAILS

CLEARING DETAILS

SCALE HOR. N/A VER. N/A GRID SW2033 DATE DEC 2024 STATUS 95% SHEET D7 of D8

SHEET NOTE:

- POROUS PAVEMENT SYSTEM SHALL BE GEOBLOCK 5150 OR APPROVED EQUAL. GEOBLOCK 5150 DIMENSIONS AND TYPICAL LAYOUT SHOWN ON THIS SHEET FOR REFERENCE.



1 **GEOBLOCK 5150 DIMENSIONS**
SCALE: NTS

2 **GEOBLOCK 5150 TYPICAL LAYOUT-BRICKLAYER PATTERN**
SCALE: NTS

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Roadway Details.dwg

RECORD DRAWING
 1. DATA PROVIDED BY: _____ TITLE: _____
 THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
 CONTRACTOR: _____ TITLE: _____ DATE: _____
 BY: _____
 2. DATA TRANSFERRED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
 DATA TRANSFER CHECKED BY: _____ TITLE: _____
 COMPANY: _____ DATE: _____
 BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES/FINAL	RB	JK
PRELIMINARY/STATE	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

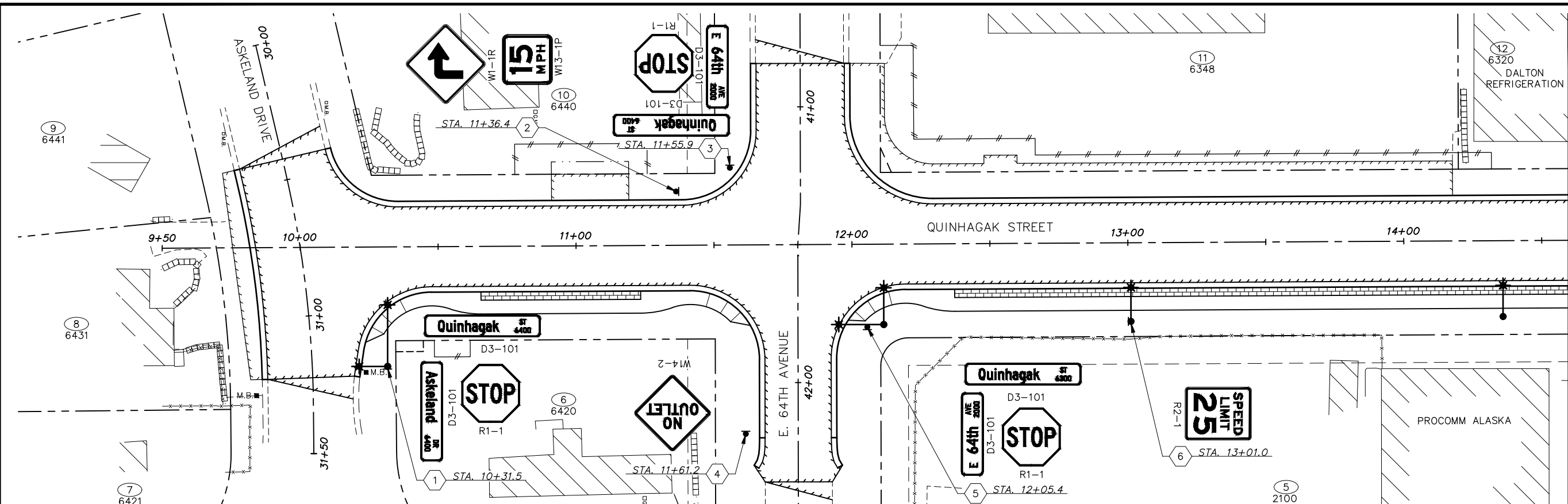
CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECL882-AK

STATE OF ALASKA
 49 TH
 Robert W. Burdick
 CE-123959
 REGISTERED PROFESSIONAL ENGINEER

MUNICIPALITY OF ANCHORAGE

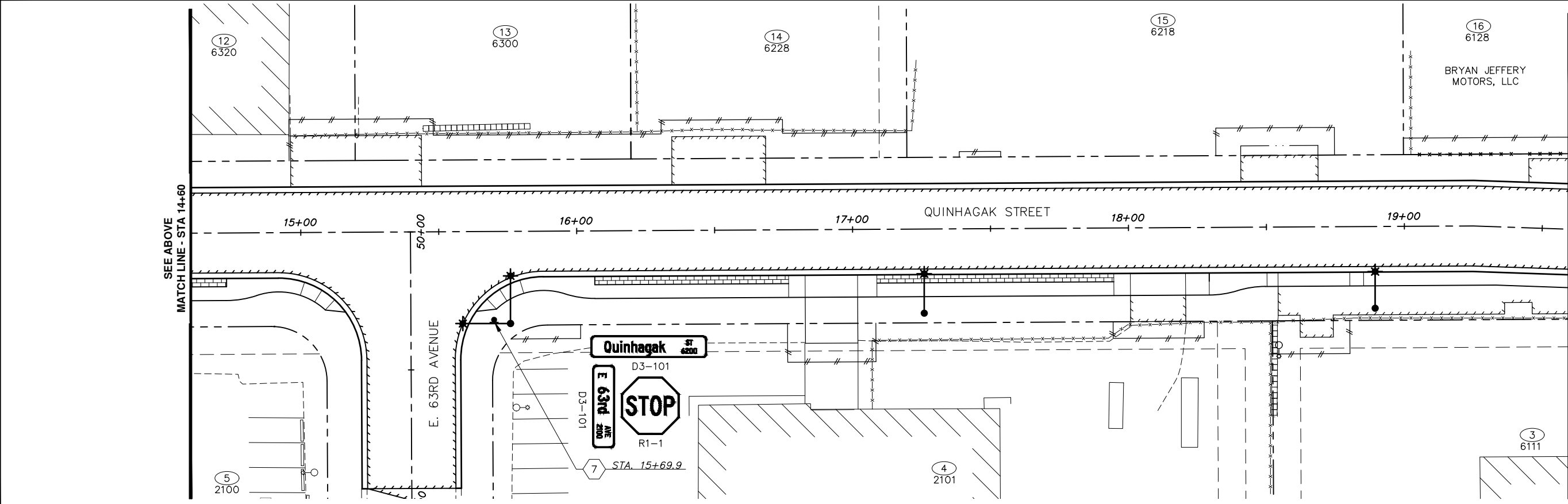
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
ROADWAY DETAILS
 GEOBLOCK DETAILS
 SCALE HOR. N/A VER. N/A
 GRID SW2033
 DATE DEC 2024 STATUS 95%
 SHEET D8 of D8

File: E:\webdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\01 Civil\10155.00 Signing & Striping.dwg



NOTE:
1. SEE SIGNING & STRIPING NOTES ON SHEET S2.

SEE BELOW
MATCH LINE - STA 14+60



SEE ABOVE
MATCH LINE - STA 14+60

MATCH LINE - STA 19+60

RECORD DRAWING
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CONTRACTOR: _____ DATE: _____
BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
DATA TRANSFER CHECKED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____
BY: _____

DATA	DRAWN BY	CHECKED BY
BASE	CB	BW
TOPOGRAPHY	CB	BW
PROFILE	RB	JK
STORM SEWER	JM	JH
WATER/SANITARY SEWER	CK	JK
GAS	CB	BW
TELEPHONE	CB	BW
ELECTRIC	JH	TK
DESIGN	RB	JK
QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

STAKING

ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST



CRW ENGINEERING GROUP
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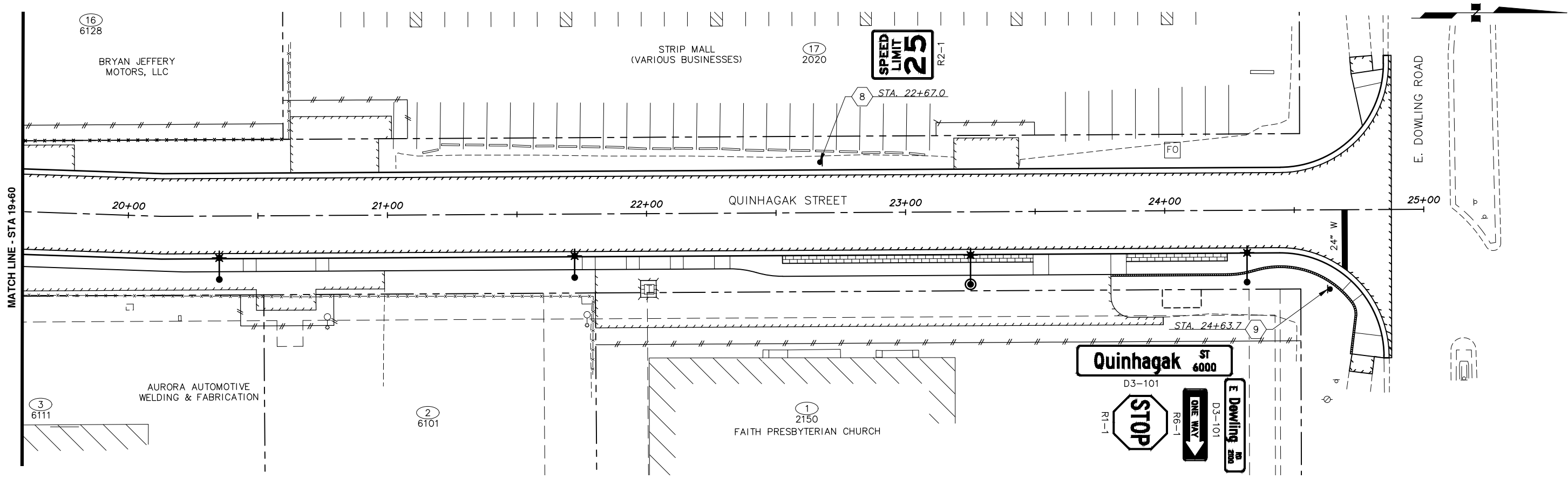


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
SIGNING & STRIPING
BOP TO STA 19+60

SCALE HOR. 1"=20'
VER. N/A

GRID SW2033
DATE DEC 2024
STATUS 95%

SHEET S1 of S2



85.04

STANDARD SIGN

SHEET NO.	POST NO.	STATION	OFFSET	TYPE	LEGEND	WIDTH	HEIGHT	AREA (SF)	SIGN FACES	SIGN POST	REMARKS
						(INCHES)	(INCHES)				
S1	1	10+31.5	43.3 RT	D3-101	ASKELAND DR 6400	36	8	2.00	N/S	MOUNT ON LIGHT POLE	ONE DOUBLE SIDED PANEL
				D3-101	QUINHAGAK ST 6400	42	8	2.33	E/W		ONE DOUBLE SIDED PANEL
				R1-1	STOP	30	30	6.25	W		
	2	11+36.4	19.5 LT	W1-1	RIGHT TURN	30	30	6.25	N	2.5" PST	
				W13-1P	15 MPH ADVISORY SPEED	18	18	2.25	N		
	3	11+55.9	27.7 LT	D3-101	E 64TH AVE 2000	30	8	1.67	N/S	2.5" PST	ONE DOUBLE SIDED PANEL
				D3-101	QUINHAGAK ST 6400	42	8	2.33	E/W		ONE DOUBLE SIDED PANEL
				R1-1	STOP	30	30	6.25	E		
	4	11+61.1	68.5 RT	W14-2	NO OUTLET	30	30	6.25	W	2.5" PST	
				D3-101	E 64TH AVE 2000	30	8	1.67	N/S		ONE DOUBLE SIDED PANEL
				D3-101	QUINHAGAK ST 6300	42	8	2.33	E/W		ONE DOUBLE SIDED PANEL
	5	12+05.4	30.2 RT	R1-1	STOP	30	30	6.25	E	2.5" PST	
				D3-101	E 64TH AVE 2000	30	8	1.67	N/S		ONE DOUBLE SIDED PANEL
				D3-101	QUINHAGAK ST 6300	42	8	2.33	E/W		ONE DOUBLE SIDED PANEL
6	13+01.0	28.5 RT	R2-1	SPEED LIMIT 25	24	30	5.00	S	MOUNT ON LIGHT POLE		
			D3-101	E 63RD AVE 2100	30	8	1.67	N/S		ONE DOUBLE SIDED PANEL	
			D3-101	QUINHAGAK ST 6200	42	8	2.33	E/W		ONE DOUBLE SIDED PANEL	
7	15+69.9	32.0 RT	R1-1	STOP	30	30	6.25	E	2.5" PST		
			D3-101	STOP	30	30	6.25	E			
			D3-101	STOP	30	30	6.25	E			
S2	8	22+67.0	19.5 LT	R2-1	SPEED LIMIT 25	24	30	5.00	N	2.5" PST	ONE DOUBLE SIDED PANEL
				D3-101	E DOWLING RD	36	8	2.00	N/S		ONE DOUBLE SIDED PANEL
				D3-101	QUINHAGAK ST	60	12	10.00	E/W		MOUNT TWO SIGNS BACK TO BACK
				R6-1R	ONE WAY RIGHT	36	12	3.00	S		
9	24+63.7	30.6 RT	R1-1	STOP	30	30	6.25	S	2.5" PST		
			D3-101	STOP	30	30	6.25	S			
			R1-1	STOP	30	30	6.25	S			

SIGNING NOTES:

- THE STATIONS INDICATED IN THE SIGN SUMMARY ARE APPROXIMATE. INSTALL SIGNS AND SIGN FOUNDATIONS PER MASS STANDARD DETAILS. BEFORE INSTALLING ANY SIGN, STAKE THE LOCATION OF ALL SIGNS FOR THE ENGINEER'S REVIEW AND APPROVAL.
- PROVIDE PERFORATED STEEL TUBE (PST) SIGN POSTS OF THE SIZE INDICATED IN THE SIGN SUMMARY.
- INSTALL THE POSTS FOR STOP SIGNS AT LOCATIONS THAT CONFORM TO MASS STANDARD DETAIL 85-14 & 85-15.
- ALL STOP SIGNS AND STREET NAME SIGNS SHALL REMAIN OPERATIONAL DURING CONSTRUCTION.
- THE LETTERING FOR STREET NAME SIGNS (D3 SERIES) SHALL BE FEDERAL HIGHWAY ADMINISTRATION "FHWA 2000 SERIES C" LETTERING, A COMBINATION OF LOWER-CASE LETTERS WITH INITIAL UPPER-CASE LETTERS.

STRIPING NOTES:

- ALL STRIPING SHALL CONFORM TO THESE CONTRACT DOCUMENTS AND THE STANDARD MASS DETAILS. ALL REVISIONS SHALL CONFORM TO THE LATEST EDITION OF THE ALASKA TRAFFIC MANUAL AND THE MANUAL OF UNIFORM TRAFFIC CONTROL DEVICES.
- UNLESS OTHERWISE NOTED, PROVIDE METHYL METHACRYLATE PAINT OF THE COLORS AND WIDTHS SPECIFIED FOR THE TRAFFIC MARKINGS INDICATED ON THE DRAWINGS. PROVIDE INLAIN APPLICATION MARKINGS IN THE FOLLOWING MANNER:
 - 125 MILS FOR 24" WHITE STOP BAR MARKINGS
- OBLITERATE AND REPLACE ALL STRIPING DAMAGED BY CONTRACTORS OPERATIONS.
- INSTALL 24" WIDE STOP BARS PER MASS STANDARD DETAILS 85-14 & 85-15.

File: E:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Signing & Striping.dwg

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DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

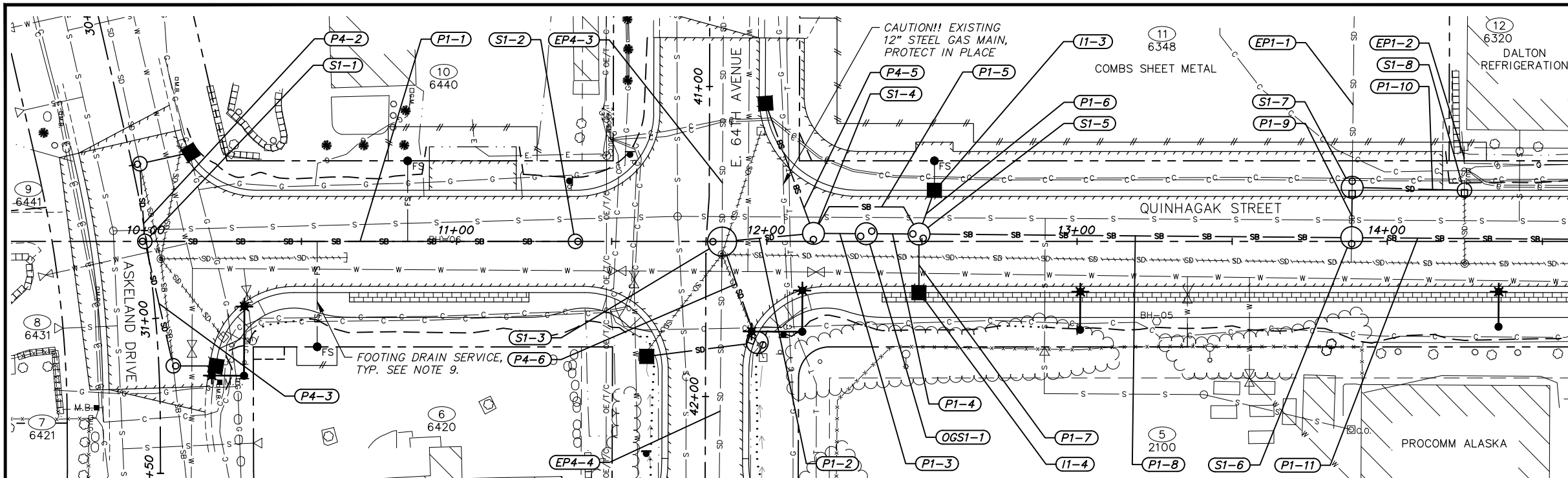


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UNIVERSITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED A
SIGNING & STRIPING
 STA 19+60 TO EOP
 SCALE HOR. 1"=20' VER. N/A
 GRID 5W2033
 DATE DEC 2024 STATUS 95% SHEET S2 of S2



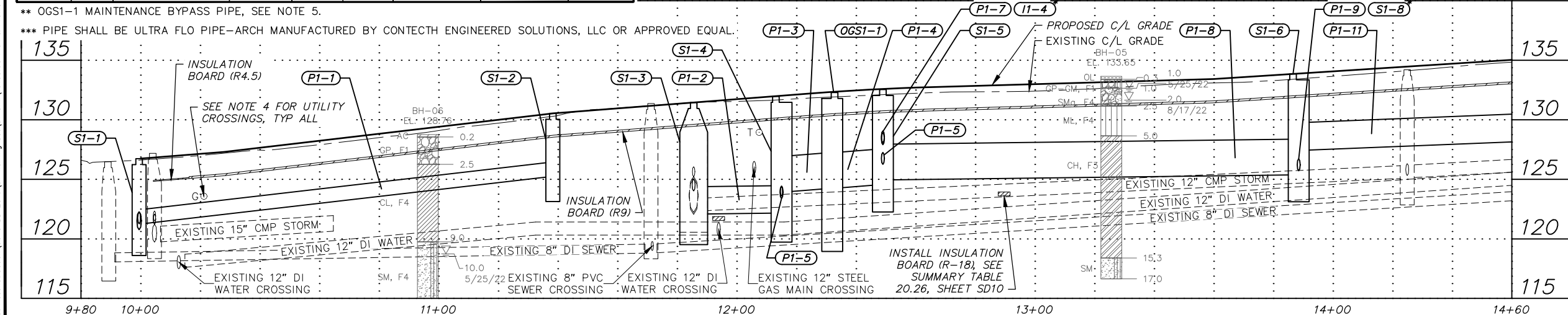
- NOTES:**
1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARIFY.
 2. REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
 3. REFER TO SHEETS SD6-SD10 FOR STORM DRAIN DETAILS AND SUMMARY TABLES.
 4. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. CONTRACTOR SHALL PROTECT AND SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED. EXISTING UTILITIES IN CONFLICT WITH PROPOSED IMPROVEMENTS WILL BE RELOCATED BY OTHERS. SEE SPECIFICATIONS FOR MORE INFORMATION.
 5. CPEP FITTINGS I.A.W. MASS SECTION 55.02 SHALL BE USED FOR BYPASS PIPING, UNLESS OTHERWISE DIRECTED BY THE ENGINEER. LOCATION OF FITTINGS SHOWN ARE APPROXIMATE. CONTRACTOR SHALL FIELD LOCATE FITTINGS WITH ENGINEER'S APPROVAL TO MINIMIZE CONFLICTS WITH OTHER UTILITIES AND OBSTRUCTIONS. CONCRETE THRUST BLOCKS I.A.W. MASS STANDARD DETAIL 60-06 SHALL BE INSTALLED AT ALL FITTINGS. PAYMENT FOR THRUST BLOCKS SHALL BE CONSIDERED INCIDENTAL TO PAY ITEM 55.03.
 6. REFER TO SHEET SD7 FOR OGS AND BYPASS STRUCTURE DETAILS.

55.02 & 55.03 - STORM DRAIN & SUBDRAIN PIPE

PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
P1-1	12	CPEP, SP (CLASS 2)	138.80	S1-2	S1-1	125.30	121.40	2.89%
P1-2	33x26	PIPE-ARCH	29.58	S1-4	S1-3	122.23	122.16	0.31%
P1-3	30	CPEP, SP (CLASS 1)	17.00	OGS1-1	S1-4	124.45	124.23	2.00%
P1-4	30	CPEP, SP (CLASS 1)	17.00	S1-5	OGS1-1	124.76	124.54	2.00%
P1-5**	12	CPEP, SP (CLASS 2)	44.02	S1-5	S1-4	126.26	123.40	7.52%
P1-6	12	CPEP, SP (CLASS 2)	14.96	I1-3	S1-5	128.27	128.07	2.01%
P1-7	12	CPEP, SP (CLASS 2)	18.89	I1-4	S1-5	128.19	127.91	2.02%
P1-8	30	CPEP, SP (CLASS 1)	139.50	S1-6	S1-5	125.62	125.21	0.31%
P1-9	12	CPEP, S	16.24	S1-7	S1-6	125.76	125.72	0.39%
P1-10	12	CPEP, S	36.21	S1-8	S1-7	125.98	125.88	0.32%
EP1-1	12	CMP	--	--	S1-7	--	125.9±	--
EP1-2	12	CPEP, S	--	--	S1-8	--	126.0±	--
P1-11	24	CPEP, SP (CLASS 2)	149.20	S2-2	S1-6	129.06	127.62	1.01%

55.04, 55.05, 55.09 & 55.22 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
S1-1	MH I	MH	9+99.50	CL	126.70	N/A	
S1-2	MH I	MH	11+38.30	CL	130.51	N/A	
S1-3	MH III/CONNECT	MH	11+85.55	0.07' RT	131.42	N/A	WATERTIGHT MH, SEE NOTE 8; CONNECT EX. PIPES (EP4-3, EP4-4)
S1-4	MH II	MH	12+15.01	2.59' LT	131.94	N/A	WATERTIGHT MH, SEE NOTE 8
OGS1-1	OGS	MH	12+32.01	2.49' LT	132.26	N/A	WATERTIGHT MH, SEE NOTE 8; SEE DETAIL 1, SHEET SD7
I1-3	CB	CI	12+54.00	16.50' LT	132.52	2	
S1-5	BYPASS/MH II	MH	12+49.01	2.39' LT	132.51	N/A	WATERTIGHT MH, SEE NOTE 8; SEE DETAIL 2, SHEET SD7
I1-4	CB	CI	12+49.00	16.50' RT	132.69	1	
S1-6	MH II	MH	13+88.50	1.22' LT	133.83	N/A	WATERTIGHT MH, SEE NOTE 8
S1-7	CB MH II/CONNECT	CI	13+88.64	17.45' LT	133.77	2	CONNECT EX. PIPE (EP1-2), SEE MASS DETAIL 55-28
S1-8	CB MH I/CONNECT	CI	14+24.84	16.50' LT	134.34	2	CONNECT EX. PIPE (EP1-3)



7. CONTRACTOR SHALL PERFORM TESTING I.A.W. MASS SECTION 50.02 (SANITARY SEWER PIPE) FOR WATERTIGHT STORM DRAIN PIPE.
8. PROVIDE WATERTIGHT CONNECTION AT MANHOLE. INSTALL Z-LOK STM BOOT STYLE CONNECTOR OR APPROVED EQUAL FOR PIPE TO MANHOLE CONNECTION. NO ADDITIONAL PAYMENT SHALL BE MADE FOR STRUCTURES CONSTRUCTED WITH BOOT STYLE CONNECTOR.
9. REFER TO SHEET SD10 FOR FOOTING DRAIN SERVICE SUMMARY TABLE.
10. FOOTING DRAIN SERVICES NOT SHOWN IN PROFILE FOR CLARITY.

RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
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DATA	DRAWN BY	CHECKED BY	DATE
BASE	CB	BW	
TOPOGRAPHY	CB	BW	
PROFILE	RB	JK	
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, GAAB 22 See MOA Benchmark Book, Page D-29 162.82
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830 GAAB 20 See MOA Benchmark Book, Page D-35 183.44
GAS	CB	BW	STAKING
TELEPHONE	CB	BW	
ELECTRIC	JH	TK	
DESIGN	RB	JK	ASBUILT
QUANTITIES	RB	JK	CONTRACTOR
PRELIMINARY/FINAL	RB	JK	INSPECTOR
MUNICIPAL/STATE	RB	JK	

PLAN CHECK CONSTRUCTION RECORD VERTICAL DATUM REVISIONS CONSULTANT SEAL

GRAPHIC SCALE: 40 20 0 20 40

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PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION SCHED B
 E. DOWLING ROAD TO ASKELAND DRIVE

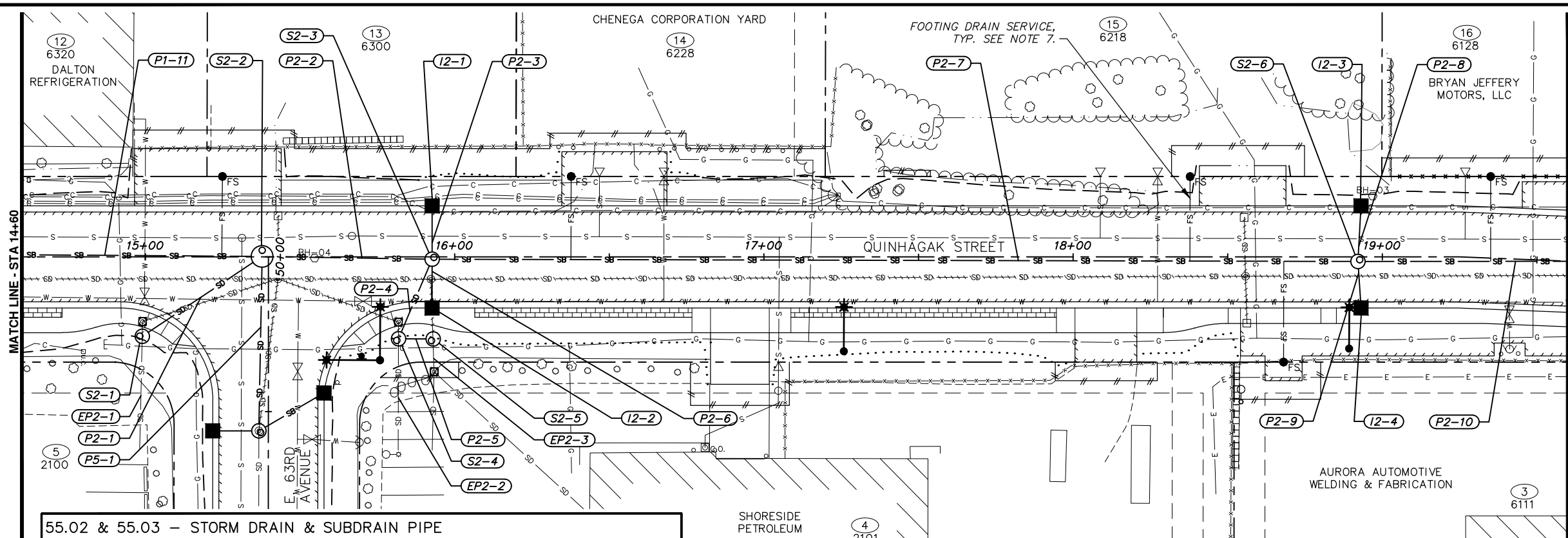
STORM DRAIN PLAN & PROFILE

QUINHAGAK STREET - BOP TO STA 14+60

SCALE: HOR. 1"=20'
 VER. 1"=5'

GRID 9W2033
 DATE DEC 2024 STATUS 95% SHEET SD1 of SD10

File: E:\webdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01 Civil\10155.00 Plan & Profile - Storm Drain.dwg



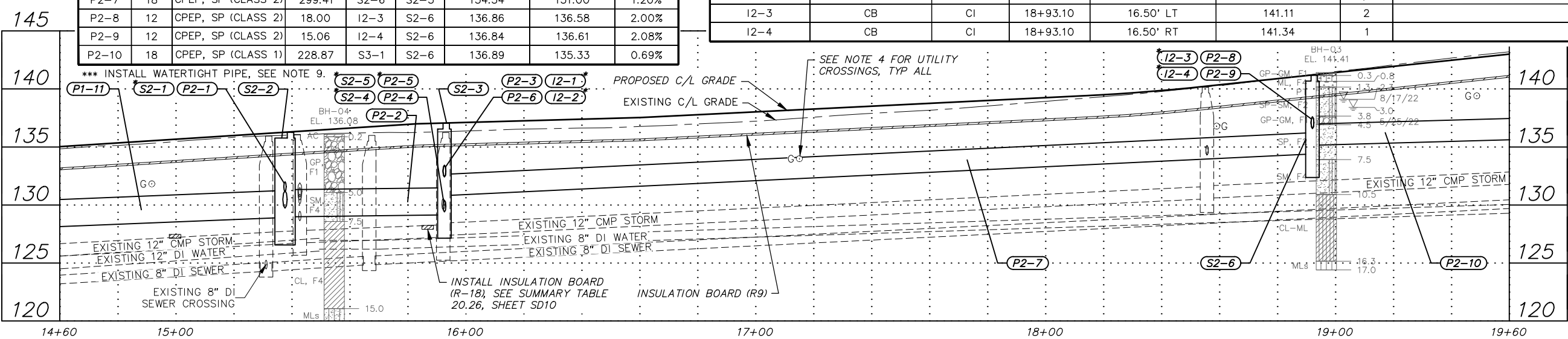
- NOTES:**
1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARIFY.
 2. REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
 3. REFER TO SHEETS SD6-SD10 FOR STORM DRAIN DETAILS AND SUMMARY TABLES.
 4. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. CONTRACTOR SHALL PROTECT AND SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED. EXISTING UTILITIES IN CONFLICT WITH PROPOSED IMPROVEMENTS WILL BE RELOCATED BY OTHERS. SEE SPECIFICATIONS FOR MORE INFORMATION.
 5. CONTRACTOR SHALL PERFORM TESTING I.A.W. MASS SECTION 50.02 (SANITARY SEWER PIPE) FOR WATERTIGHT STORM DRAIN PIPE.
 6. PROVIDE WATERTIGHT CONNECTION AT MANHOLE. INSTALL Z-LOK STM BOOT STYLE CONNECTOR OR APPROVED EQUAL FOR PIPE TO MANHOLE CONNECTION. NO ADDITIONAL PAYMENT SHALL BE MADE FOR STRUCTURES CONSTRUCTED WITH BOOT STYLE CONNECTOR.
 7. REFER TO SHEET SD10 FOR FOOTING DRAIN SERVICE SUMMARY TABLE.
 8. FOOTING DRAIN SERVICES NOT SHOWN IN PROFILE FOR CLARITY.

55.02 & 55.03 - STORM DRAIN & SUBDRAIN PIPE

PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
EP2-1	4	CPEP, S	--	--	S2-1	--	133.0±	--
P2-1***	10	CPEP, S	46.56	S2-1	S2-2	131.30	131.17	0.31%
P2-2	24	CPEP, SP (CLASS 1)	55.00	S2-3	S2-2	129.31	129.16	0.30%
P2-3	12	CPEP, SP (CLASS 2)	17.25	I2-1	S2-3	132.69	132.42	2.04%
P2-4***	12	CPEP, S	27.93	S2-4	S2-3	129.49	129.41	0.33%
EP2-2	6	CMP	--	--	S2-4	--	132.1±	--
P2-5	12	CPEP, SP (CLASS 2)	11.20	S2-5	S2-4	129.62	129.59	0.42%
EP2-3	12	CPEP, S	--	--	S2-5	--	129.7±	--
P2-6	12	CPEP, SP (CLASS 2)	15.75	I2-2	S2-3	132.67	132.43	2.04%
P2-7	18	CPEP, SP (CLASS 2)	299.41	S2-6	S2-3	134.54	131.00	1.20%
P2-8	12	CPEP, SP (CLASS 2)	18.00	I2-3	S2-6	136.86	136.58	2.00%
P2-9	12	CPEP, SP (CLASS 2)	15.06	I2-4	S2-6	136.84	136.61	2.08%
P2-10	18	CPEP, SP (CLASS 1)	228.87	S3-1	S2-6	136.89	135.33	0.69%

55.04, 55.05 & 55.09 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
S2-1	MH I/CONNECT	MH	14+98.95	25.66' RT	136.38	N/A	CONNECT TO EX. PIPE (EP2-1)
S2-2	MH II	MH	15+37.70	0.15' LT	136.24	N/A	
S2-4	MH I/CONNECT	MH	15+81.80	26.47' RT	136.37	N/A	CONNECT TO EX. PIPE (EP2-2)
I2-1	CB	CI	15+92.69	16.50' LT	136.94	2	
I2-2	CB	CI	15+92.69	16.50' RT	137.17	1	
S2-3	MH I	MH	15+92.69	0.75' RT	137.02	N/A	
S2-5	MH I/CONNECT	MH	15+93.00	26.53' RT	136.30	N/A	CONNECT TO EX. PIPE (EP2-3)
S2-6	MH I	MH	18+92.10	1.47' RT	141.14	N/A	
I2-3	CB	CI	18+93.10	16.50' LT	141.11	2	
I2-4	CB	CI	18+93.10	16.50' RT	141.34	1	



9. CONTRACTOR SHALL PERFORM TESTING I.A.W. MASS SECTION 50.02 (SANITARY SEWER PIPE) FOR WATERTIGHT STORM DRAIN PIPE.

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RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____

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2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

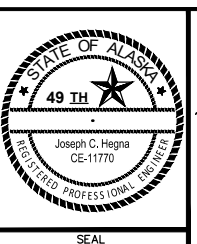
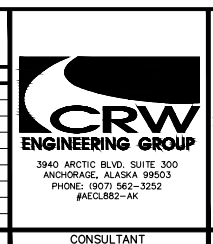
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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DATA	DRAWN BY	CHECKED BY	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW							
TOPOGRAPHY	CB	BW							
PROFILE	RB	JK							
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82			
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44			
GAS	CB	BW							
TELEPHONE	CB	BW							
ELECTRIC	JH	TK							
DESIGN	RB	JK							
QUANTITIES	RB	JK							
PRELIMINARY/FINAL	RB	JK							
MUNICIPAL/STATE	RB	JK							



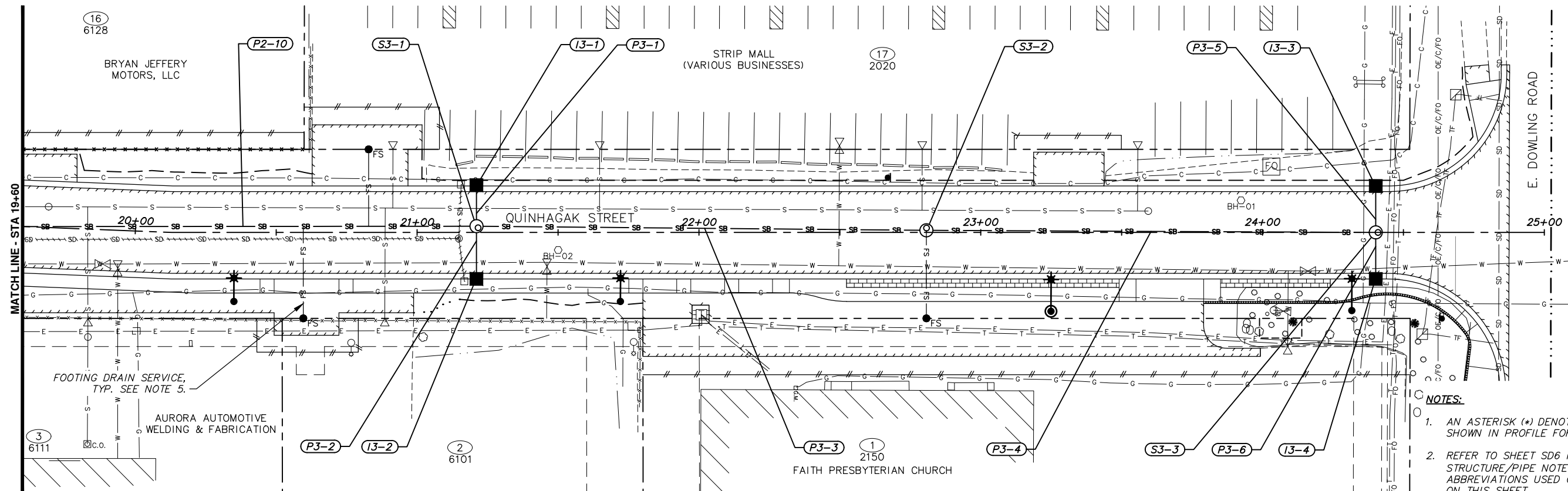
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED B

STORM DRAIN PLAN & PROFILE

QUINHAGAK STREET - STA 14+60 TO STA 19+60

SCALE HOR. 1"=20' VER. 1"=5' GRID 9W2033 DATE DEC 2024 STATUS 95% SHEET SD2 of SD10



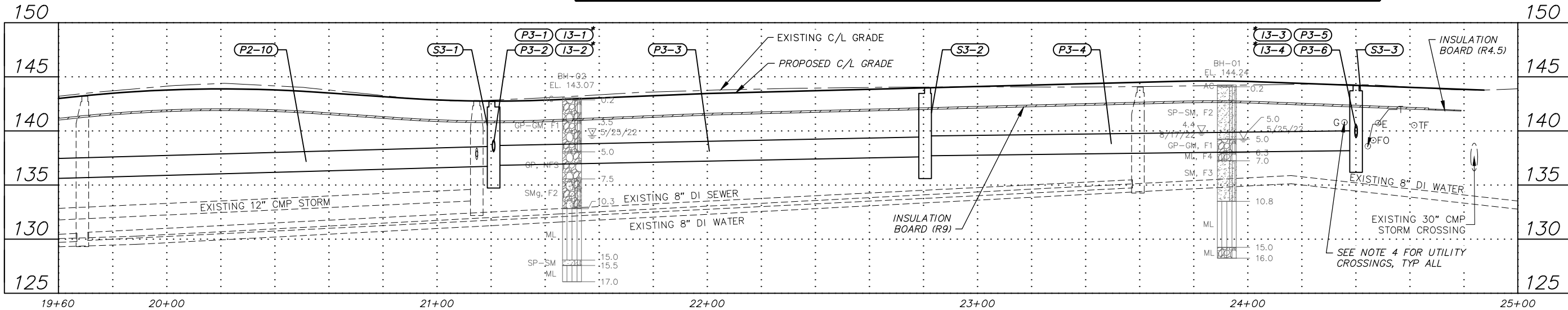
- NOTES:**
1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARITY.
 2. REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
 3. REFER TO SHEETS SD6-SD10 FOR STORM DRAIN DETAILS AND SUMMARY TABLES.
 4. CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. CONTRACTOR SHALL PROTECT AND SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED. EXISTING UTILITIES IN CONFLICT WITH PROPOSED IMPROVEMENTS WILL BE RELOCATED BY OTHERS. SEE SPECIFICATIONS FOR MORE INFORMATION.
 5. REFER TO SHEET SD10 FOR FOOTING DRAIN SERVICE SUMMARY TABLE.
 6. FOOTING DRAIN SERVICES NOT SHOWN IN PROFILE FOR CLARITY.

55.03 - SUBDRAIN PIPE

PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
P3-1	12	CPEP, SP (CLASS 2)	14.57	I3-1	S3-1	138.39	138.17	2.08%
P3-2	12	CPEP, SP (CLASS 2)	18.71	I3-2	S3-1	138.36	138.06	2.04%
P3-3	18	CPEP, SP (CLASS 2)	159.69	S3-2	S3-1	137.77	136.99	0.50%
P3-4	18	CPEP, SP (CLASS 2)	159.46	S3-3	S3-2	138.34	137.87	0.30%
P3-5	12	CPEP, SP (CLASS 2)	16.50	I3-3	S3-3	139.82	139.57	2.00%
P3-6	12	CPEP, SP (CLASS 2)	16.50	I3-4	S3-3	139.64	139.38	2.08%

55.05 & 55.09 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
I3-1	CB	CI	21+21.04	16.78' LT	142.89	1	
I3-2	CB	CI	21+21.04	16.50' RT	142.86	1	
S3-1	MH I	MH	21+21.04	2.21' LT	142.68	N/A	
S3-2	MH I	MH	22+80.72	0.75' LT	143.93	N/A	
I3-3	CB	CI	24+40.18	16.50' LT	144.32	1	
I3-4	CB	CI	24+40.18	16.50' RT	144.14	1	
S3-3	MH I	MH	24+40.18	CL	144.17	N/A	



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 BY: _____

DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE TOPOGRAPHY	CB	BW		GAAB 22	See MOA Benchmark Book, Page D-29	162.82				
PROFILE	RB	JK		GAAB 20	See MOA Benchmark Book, Page D-35	163.44				
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830							
WATER/SANITARY SEWER	CK	JK								
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

CRW ENGINEERING GROUP

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 #AECUB82-AK

STATE OF ALASKA
 49 TH
 Joseph C. Hegna
 CE-11770
 REGISTERED PROFESSIONAL ENGINEER

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

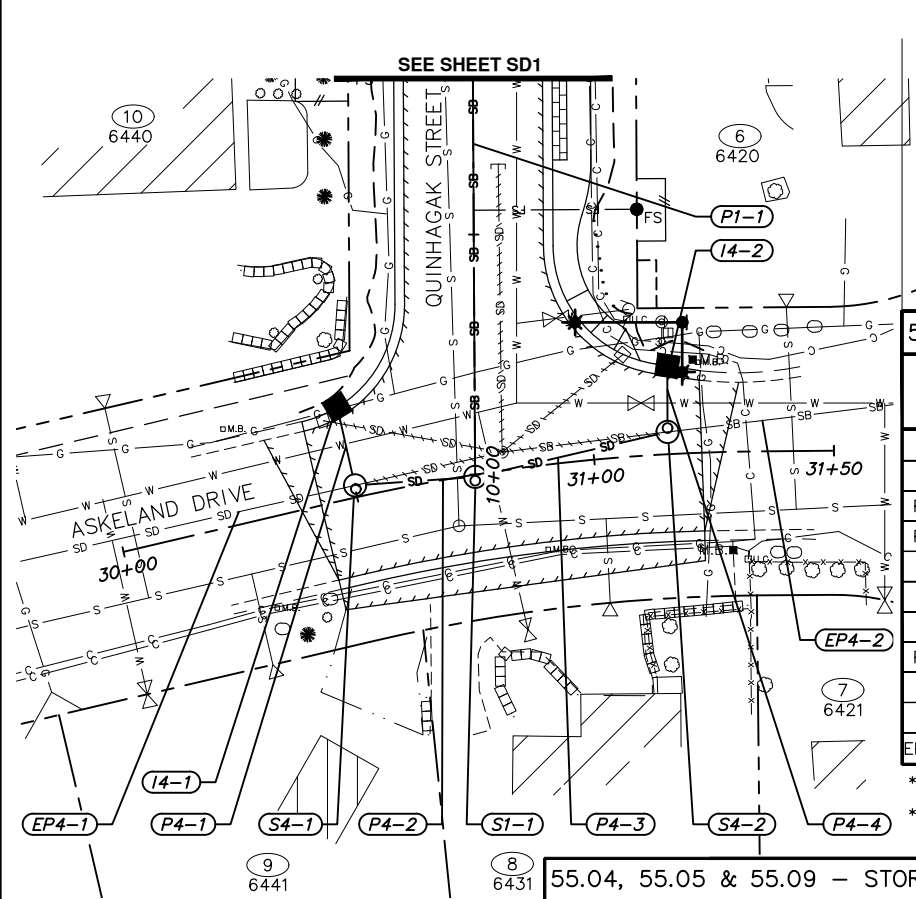
21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKLAND DRIVE SCHED B

STORM DRAIN PLAN & PROFILE

QUINHAGAK STREET - STA 19+60 TO EOP

SCALE HOR. 1"=20' VER. 1"=5' GRID 9W2033 DATE DEC 2024 STATUS 95% SHEET SD3 of SD10

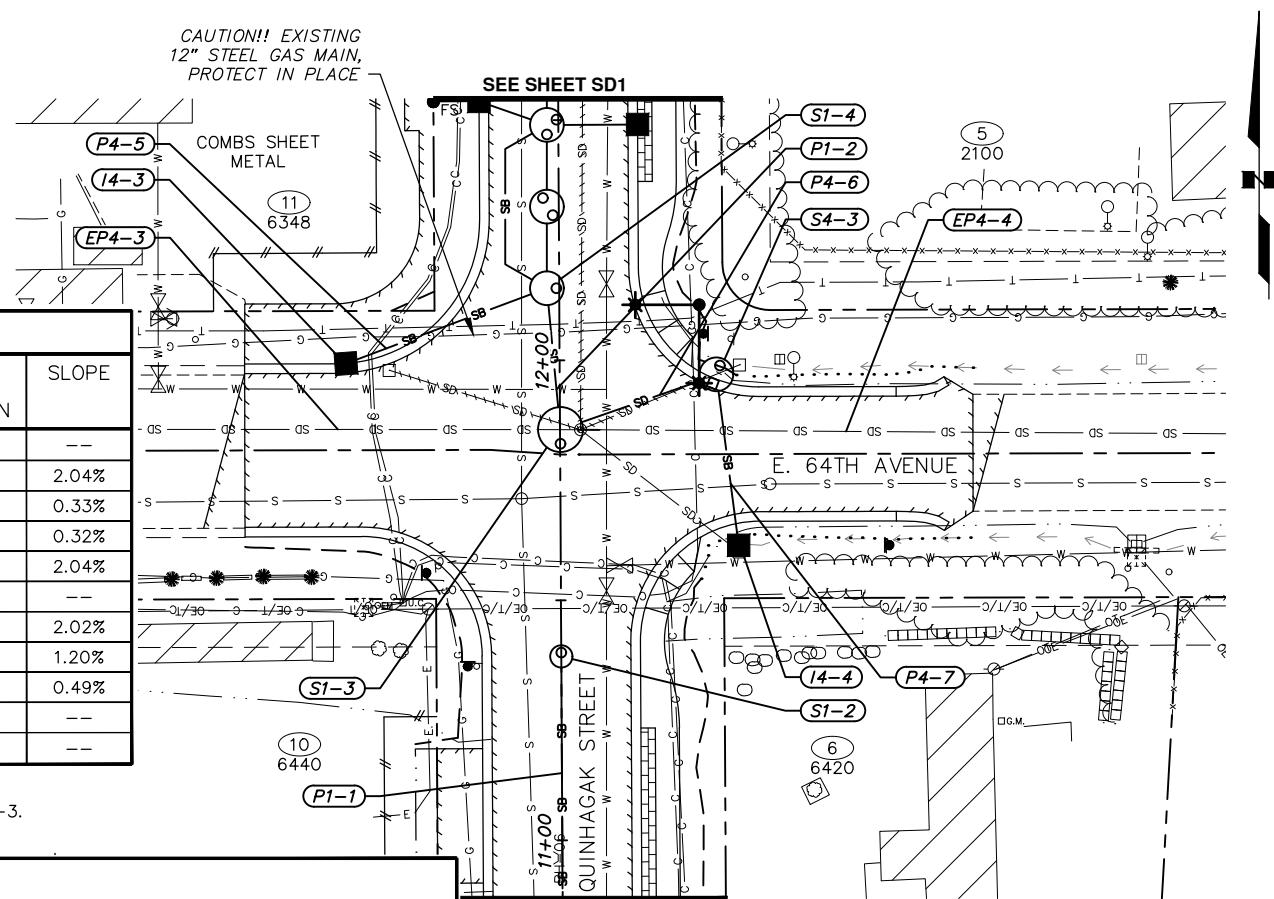
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55.02 & 55.03 - STORM DRAIN & SUBDRAIN PIPE

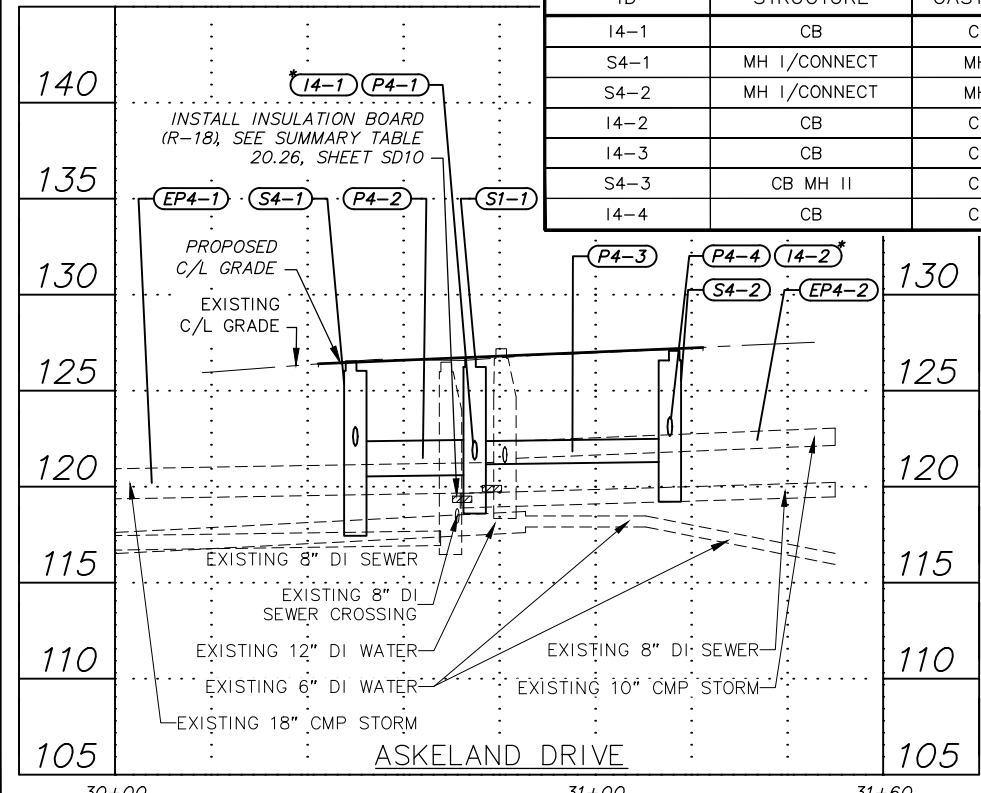
PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
EP4-1	18	CMP	--	S4-1	--	--	119.6±	--
P4-1	12	CPEP, SP (CLASS 2)	16.77	I4-1	S4-1	122.38	122.12	2.04%
P4-2**	18	CPEP, S	25.10	S1-1	S4-1	120.77	120.70	0.33%
P4-3**	12	CPEP, S	41.15	S4-2	S1-1	121.39	121.27	0.32%
P4-4	12	CPEP, SP (CLASS 2)	13.80	I4-2	S4-2	122.83	122.63	2.04%
EP4-2	10	CMP	--	--	S4-2	--	121.9±	--
P4-5	10	CPEP, SP (CLASS 2)	44.68	I4-3	S1-4	124.40	123.60	2.02%
P4-6**	12	CPEP, S	34.47	S4-3	S1-3	126.68	126.35	1.20%
P4-7	12	CPEP, SP (CLASS 2)	35.88	I4-4	S4-3	126.94	126.78	0.49%
EP4-3	57x38	PIPE-ARCH	--	S1-3	--	--	122.1±	--
EP4-4***	36	CMP	--	--	S1-3	--	122.2±	--

** INSTALL WATERTIGHT PIPE, SEE NOTE 5.
 *** CONNECT TO EXISTING 36" CMP PIPE AND EXTEND 8 FEET OF NEW 36" CMP PIPE TO S1-3.

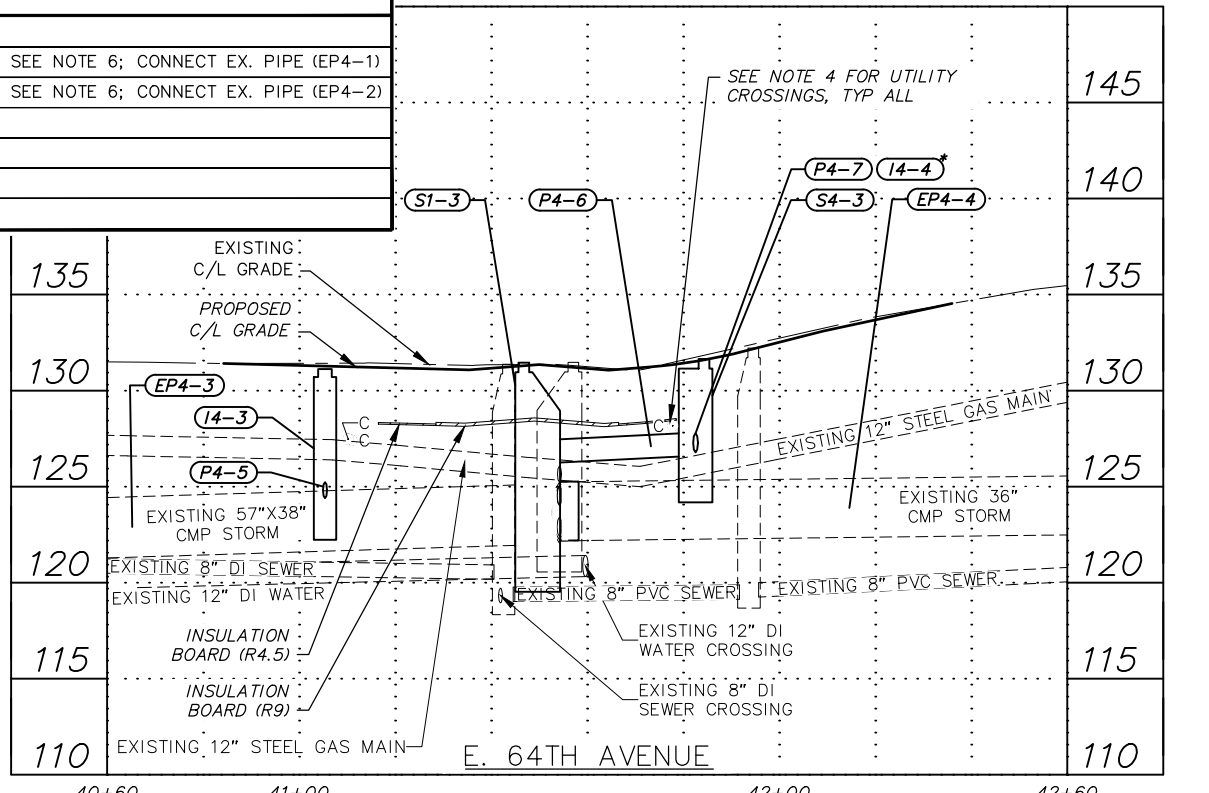


55.04, 55.05 & 55.09 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
I4-1	CB	CI	30+50.01	19.24' LT	126.63	2	
S4-1	MH I/CONNECT	MH	30+50.06	2.46' LT	126.50	N/A	WATERTIGHT MH, SEE NOTE 6; CONNECT EX. PIPE (EP4-1)
S4-2	MH I/CONNECT	MH	31+15.57	4.61' LT	127.04	N/A	WATERTIGHT MH, SEE NOTE 6; CONNECT EX. PIPE (EP4-2)
I4-2	CB	CI	31+16.25	18.39' LT	127.08	2	
I4-3	CB	CI	11+99.57	44.51' LT	131.12	2	
S4-3	CB MH II	CI	11+96.88	32.63' RT	131.65	2	
I4-4	CB	CI	11+61.27	37.08' RT	131.03	2	



- NOTES:**
- AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARIFY.
 - REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
 - REFER TO SHEETS SD6-SD10 FOR STORM DRAIN DETAILS AND SUMMARY TABLES.
 - CAUTION!!! THE LOCATION OF EXISTING UTILITY CROSSINGS SHOWN IN PROFILE ARE APPROXIMATE. CONTRACTOR SHALL PROTECT AND SHORE EXISTING UTILITIES IN PLACE WHERE NECESSARY OR AS NOTED. EXISTING UTILITIES IN CONFLICT WITH PROPOSED IMPROVEMENTS WILL BE RELOCATED BY OTHERS. SEE SPECIFICATIONS FOR MORE INFORMATION.
 - CONTRACTOR SHALL PERFORM TESTING I.A.W. MASS SECTION 50.02 (SANITARY SEWER PIPE) FOR WATERTIGHT STORM DRAIN PIPE.
 - PROVIDE WATERTIGHT CONNECTION AT MANHOLE. INSTALL Z-LOK STM BOOT STYLE CONNECTOR OR APPROVED EQUAL FOR PIPE TO MANHOLE CONNECTION. NO ADDITIONAL PAYMENT SHALL BE MADE FOR STRUCTURES CONSTRUCTED WITH BOOT STYLE CONNECTOR.



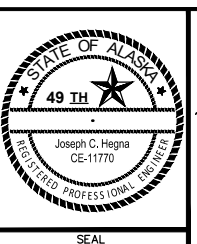
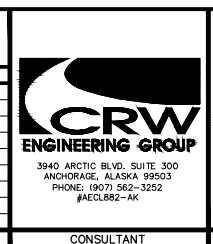
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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								



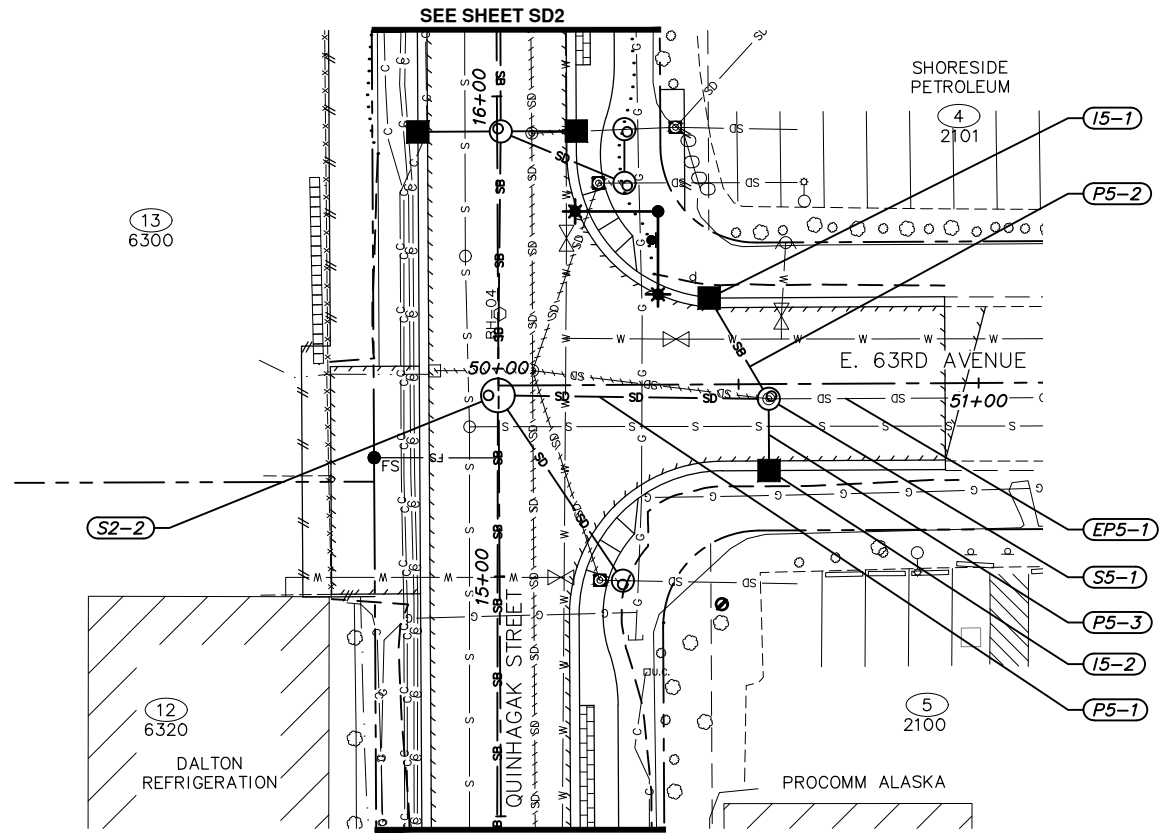
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED B

STORM DRAIN PLAN & PROFILE

ASKELAND DRIVE & E. 64TH AVENUE

SCALE: HOR. 1"=20' VER. 1"=5' GRID: 9W2033 DATE: DEC 2024 STATUS: 95% SHEET: SD4 of SD10



- NOTES:**
1. AN ASTERISK (*) DENOTES PIPE OR STRUCTURE NOT SHOWN IN PROFILE FOR CLARIFY.
 2. REFER TO SHEET SD6 FOR GENERAL STORM DRAIN STRUCTURE/PIPE NOTES AND STRUCTURE ABBREVIATIONS USED ON SUMMARY TABLES SHOWN ON THIS SHEET.
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 5. CONTRACTOR SHALL PERFORM TESTING I.A.W. MASS SECTION 50.02 (SANITARY SEWER PIPE) FOR WATERTIGHT STORM DRAIN PIPE.

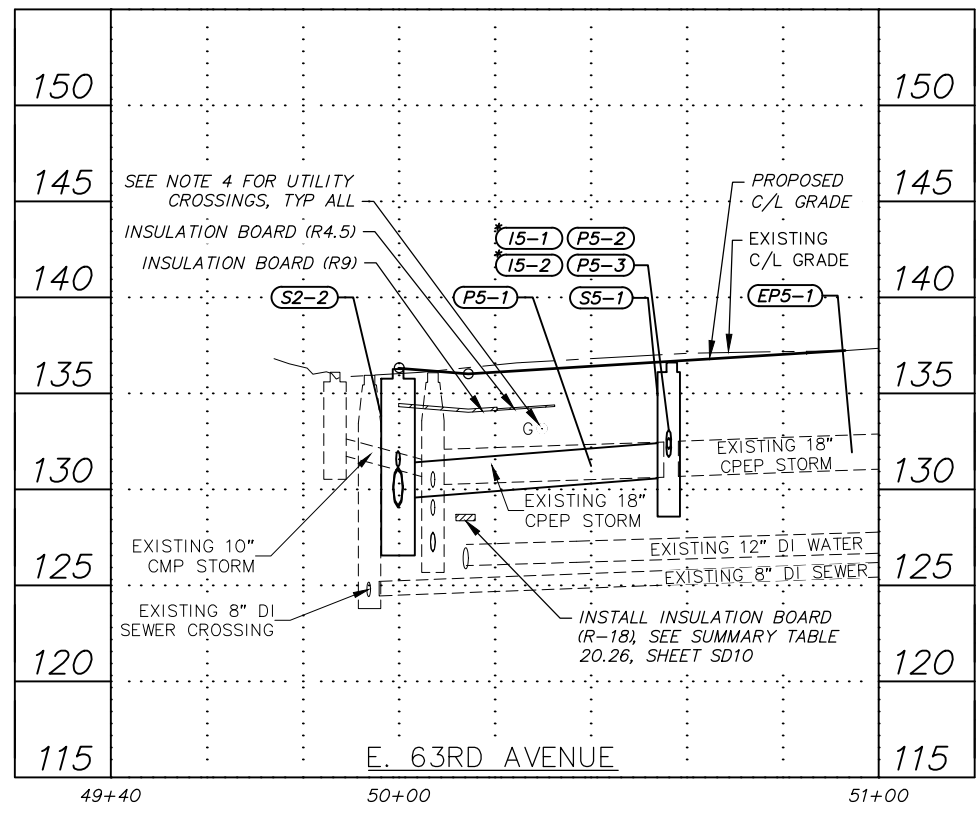
55.02 & 55.03 - STORM DRAIN & SUBDRAIN PIPE

PIPE NAME	SIZE (IN.)	PIPE TYPE	LENGTH (FT.)	FROM	TO	INLET ELEVATION	OUTLET ELEVATION	SLOPE
P5-1**	18	CPEP, S	56.41	S5-1	S2-2	130.75	129.73	1.98%
P5-2	12	CPEP, SP (CLASS 2)	24.39	I5-1	S5-1	132.09	131.68	2.01%
P5-3	12	CPEP, SP (CLASS 2)	14.92	I5-2	S5-1	132.32	132.10	2.01%
EP5-1	18	CPEP, S	--	--	S5-1	--	130.9±	--

** INSTALL WATERTIGHT PIPE, SEE NOTE 5.

55.04, 55.05 & 55.09 - STORM DRAIN STRUCTURES

STRUCTURE ID	TYPE OF STRUCTURE	TYPE OF CASTING	STATION	OFFSET TO STRUCTURE C/L	TOP OF CASTING ELEVATION	CURB TYPE	COMMENTS
I5-1	CB	CI	50+43.99	18.00' LT	136.34	2	
S5-1	MH 1/CONNECT	MH	50+56.25	3.08' RT	136.57	N/A	CONNECT EX. PIPE (EP5-1)
I5-2	CB	CI	50+56.25	18.00' RT	136.54	2	



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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								

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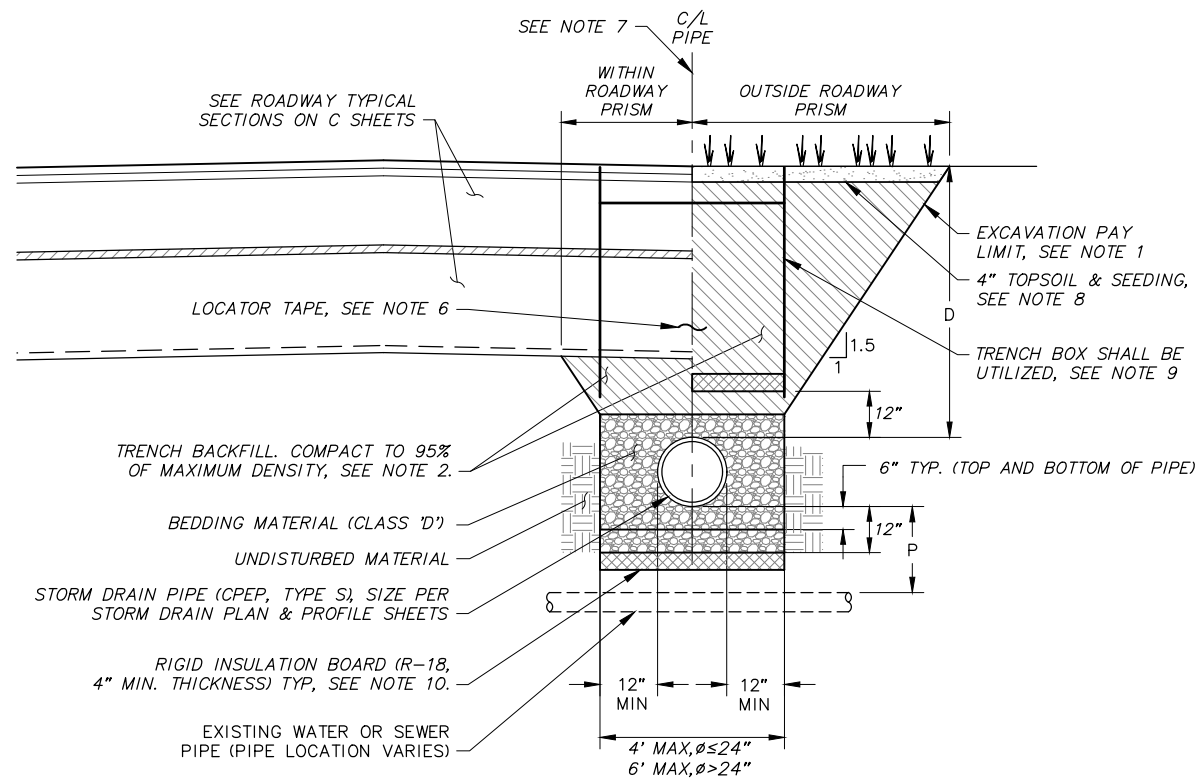
PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED B

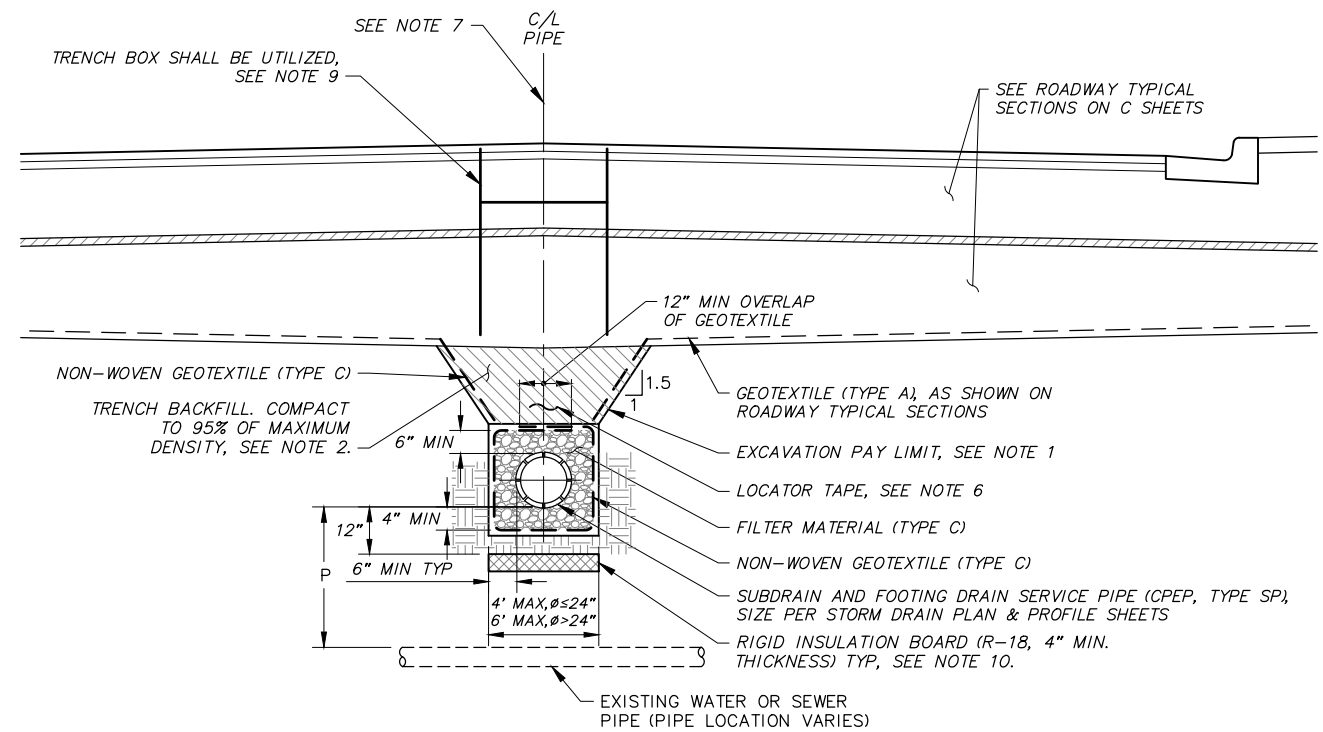
STORM DRAIN PLAN & PROFILE

E. 63RD AVENUE

SCALE: HOR. 1"=20' VER. 1"=5' GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: SD5 of SD10



1 TYPICAL STORM DRAIN TRENCH SECTION
SCALE: NTS



2 TYPICAL SUBDRAIN TRENCH SECTION
SCALE: NTS

STORM DRAIN & SUBDRAIN TRENCH SECTION NOTES:

- TRENCH EXCAVATION AND SHORING SHALL COMPLY WITH ALL LOCAL, STATE, AND OSHA REGULATIONS AND REQUIREMENTS. INDICATED TRENCH WALL SLOPES AND DIMENSIONS ARE FOR PAY QUANTITY DETERMINATIONS ONLY.
- TRENCH BACKFILL SHALL BE NATIVE MATERIAL MEETING TYPE III CLASSIFICATION (MINIMUM) AS APPROVED BY THE ENGINEER. NATIVE MATERIAL NOT MEETING TYPE III CLASSIFICATION SHALL BE REMOVED AND REPLACED WITH FURNISH TRENCH BACKFILL (TYPE II).
- REMOVE AND DISPOSE OF ALL ORGANIC MATERIALS IN ACCORDANCE WITH MASS SECTION 20.13.
- IN PREPARATION FOR AND IMMEDIATELY PRIOR TO PAVING, CONTRACTOR SHALL SAW CUT AND REMOVE AN ADDITIONAL 12 INCHES FROM EXISTING PAVEMENT EDGE. THE ENGINEER MAY REQUIRE MORE THAN 12 INCHES ADDITIONAL CUT IF THE EXISTING PAVEMENT HAS BEEN LIFTED IN THE REMOVAL PROCESS, IF THE JOINT DOES NOT OCCUR ON UNDISTURBED MATERIAL, OR IF THE JOINT IS LOCATED WITHIN THE TRAVEL LANE.
- WHERE WATER AND STORM DRAIN/SUBDRAIN MAINS CROSS, STORM DRAIN/SUBDRAIN MAIN JOINTS SHALL BE AT LEAST 10 FEET FROM WATER MAIN JOINTS.
- INSTALL DETECTABLE LOCATOR TAPE AT LEAST 24 INCHES BUT NO MORE THAN 36 INCHES ABOVE THE CROWN OF THE PIPE.
- LOCATION OF STORM DRAIN/SUBDRAIN VARIES WITHIN ROADWAY. INSTALL STORM DRAIN/SUBDRAIN AS SHOWN ON STORM DRAIN PLAN & PROFILE SHEETS.
- PLACE 4" OF COMPACTED TOPSOIL AND SEEDING (SCHEDULE A) ON ALL DISTURBED AREAS, UNLESS OTHERWISE NOTED.
- TRENCH BOX SHALL BE UTILIZED TO MINIMIZE TRENCH WIDTH AND REDUCE IMPACTS TO ADJACENT PROPERTIES AND RE-VEGETATION. CONTRACTOR SHALL AVOID IMPACTS TO TREE PROTECTION ZONES.
- INSTALL INSULATION BOARD (R-18) WHEN:
 - 'D' IS LESS THAN 4' IN AREAS OUTSIDE OF THE INSULATED ROADWAY SECTIONS. INSULATION PLACEMENT SHALL CONFORM TO MASS DETAIL 20-9.
 - 'P' IS LESS THAN 3', AS MEASURED FROM OUTSIDE OF PIPES & WITHIN BEDDING LIMITS, OR AS DIRECTED BY ENGINEER IN THE FIELD.
- WATER LINES CROSSING STORM DRAIN LINES REQUIRE A MINIMUM INSULATED VERTICAL SEPARATION OF EIGHTEEN (18) INCHES. IF EIGHTEEN (18) INCHES CAN NOT BE OBTAINED, THE WATER LINE WILL HAVE TO BE RELOCATED.

GENERAL STORM DRAIN STRUCTURE & PIPE NOTES:

- HORIZONTAL AND VERTICAL CONTROL POINTS FOR STORM DRAIN STRUCTURES (REFERENCE POINTS CALLED OUT IN PLAN & PROFILE SHEETS) ARE:

STRUCTURE	HORZ CONTROL	REFERENCE ELEV.
TYPE I MH	CENTER OF MH	FG/TOP OF LID.
TYPE II MH	CENTER OF MH	FG/TOP OF LID.
TYPE II CB MH	CENTER OF MH	TBC @ MID. PT. OF CURB INLET HOOD
CATCH BASIN	CENTER OF CB	TBC @ MID. PT. OF CURB INLET HOOD
- PIPE LENGTHS ARE BASED ON THE HORIZONTAL DISTANCE BETWEEN THE CENTER OF CONNECTING STRUCTURES OR FITTINGS. PIPE SLOPES ARE CALCULATED USING THE ACTUAL LENGTH OF PIPE FROM THE INSIDE FACE OF STRUCTURES.
- UNLESS OTHERWISE NOTED, ALL STORM DRAIN MAIN PIPE SHALL BE CPEP, TYPE S AND ALL SUBDRAIN PIPE AND FOOTING DRAIN SERVICES SHALL BE CPEP, TYPE SP.
- THE FOLLOWING ABBREVIATIONS USED ON THE STORM DRAIN STRUCTURE TABLES ON THE PLAN & PROFILES SHEETS ARE DESCRIBED BELOW:
 - CB - CATCH BASIN
 - CB MH I - CATCH BASIN MANHOLE, TYPE I
 - CB MH II - CATCH BASIN MANHOLE, TYPE II
 - MH I - STORM DRAIN MANHOLE, TYPE I
 - MH II - STORM DRAIN MANHOLE, TYPE II
 - OGS - OIL AND GRIT SEPARATOR
 - CONNECT - CONNECT TO EXISTING STORM DRAIN MANHOLE AND/OR PIPE
 - BYPASS - BYPASS PIPE USED TO REROUTE FLOW AROUND OGS DURING MAINTENANCE
 - CI - CURB INLET
 - MH - MANHOLE FRAME AND LID

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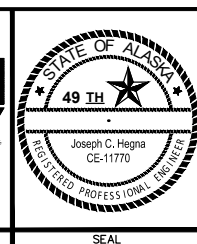
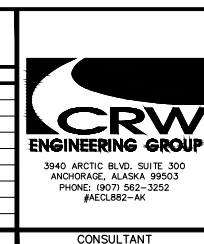
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DATA	DRAWN BY	CHECKED BY	FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
BASE	CB	BW								
TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
STORM SEWER	JM	JH	DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
WATER/SANITARY SEWER	CK	JK	3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW	STAKING							
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK	ASBUILT							
QUANTITIES	RB	JK	CONTRACTOR							
PRELIMINARY/FINAL	RB	JK	INSPECTOR							
MUNICIPAL/STATE	RB	JK								
PLAN CHECK			CONSTRUCTION RECORD							
			VERTICAL DATUM							
			REVISIONS							
			CONSULTANT							
			SEAL							

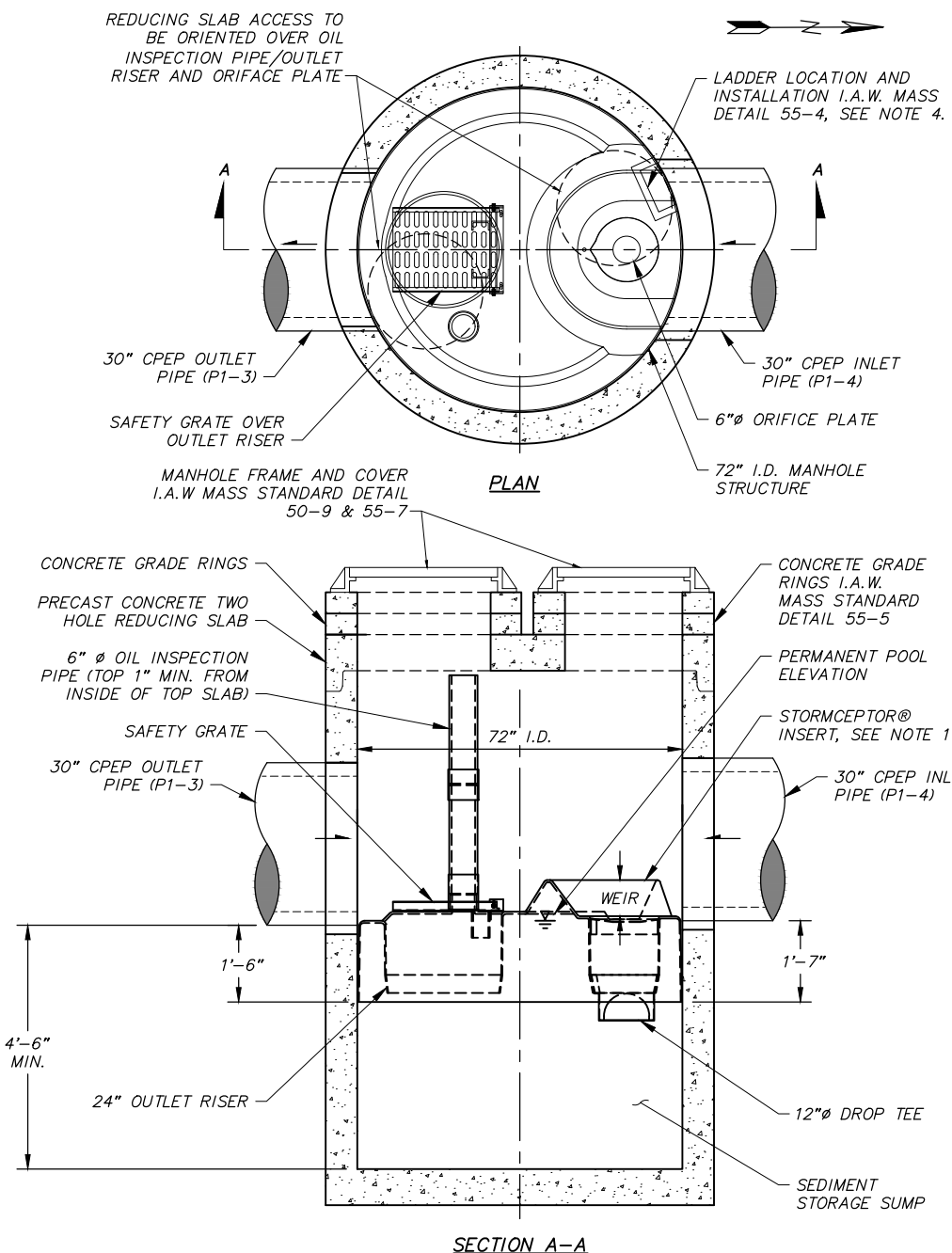


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED B

STORM DRAIN DETAILS

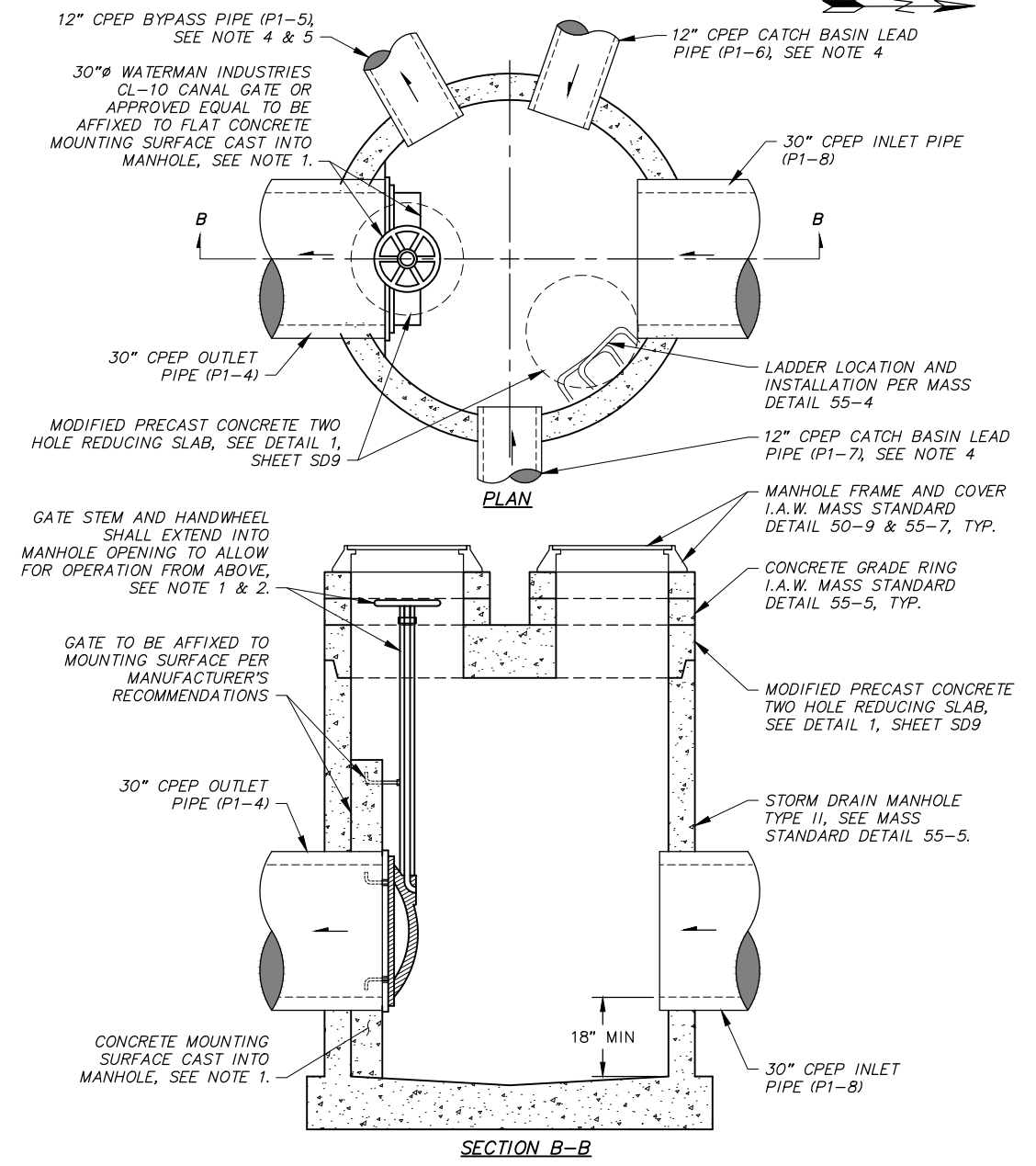
SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: SD6 of SD10



OIL & GRIT SEPARATOR NOTES

- OIL AND GRIT SEPARATOR (STRUCTURE OGS1-1) SHALL BE STORMCEPTOR MODEL STC900 MANUFACTURED BY CONTECH ENGINEERED SOLUTIONS LLC OR APPROVED EQUAL.
- ACCESS OPENING THROUGH REDUCING SLAB SHOULD BE POSITIONED OVER THE DROP TEE AND OIL PORT.
- SEE STORM DRAIN PLAN & PROFILE SHEETS FOR INLET AND OUTLET PIPE INVERTS & ORIENTATION AND STRUCTURE INFORMATION.
- LADDER RUNGS NOT SHOWN IN SECTION VIEW FOR CLARITY.

1 OIL AND GRIT SEPARATOR (OGS1-1) DETAIL
SCALE: NTS



BYPASS MANHOLE NOTES

- CAST CONCRETE MOUNTING SURFACE INTO MANHOLE SUCH THAT BYPASS GATE HANDWHEEL IS CENTERED IN ACCESS OPENING.
- BYPASS GATE STEM SHALL BE NON-RISING TO POSITION HANDWHEEL AT CONVENIENT STATIC OPERATING ELEVATION FROM MANHOLE OPENING ABOVE.
- BYPASS MANHOLE (S1-5) SHALL BE PAID FOR UNDER PAY ITEM 55.05 CONSTRUCT (TYPE II) BYPASS MANHOLE.
- BYPASS PIPE (P1-5) AND CATCH BASIN LEADS (P1-6 & P1-7) NOT SHOWN IN SECTION B-B FOR CLARITY.
- ADJUST LOCATION OF PIPE PENETRATION INTO MANHOLE FOR BYPASS PIPE (P1-6) AS REQUIRED TO AVOID CONFLICT WITH CONCRETE MOUNTING SURFACE.

2 BYPASS MANHOLE (S1-5) DETAIL
SCALE: NTS

File: s:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\01\10155.00 Details - Storm Drain.dwg

RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
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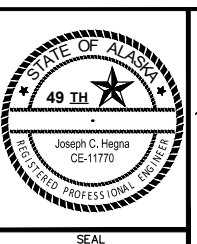
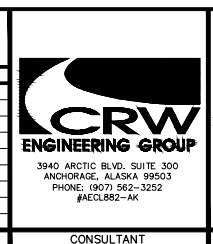
2. DATA TRANSFERRED BY: _____ TITLE: _____
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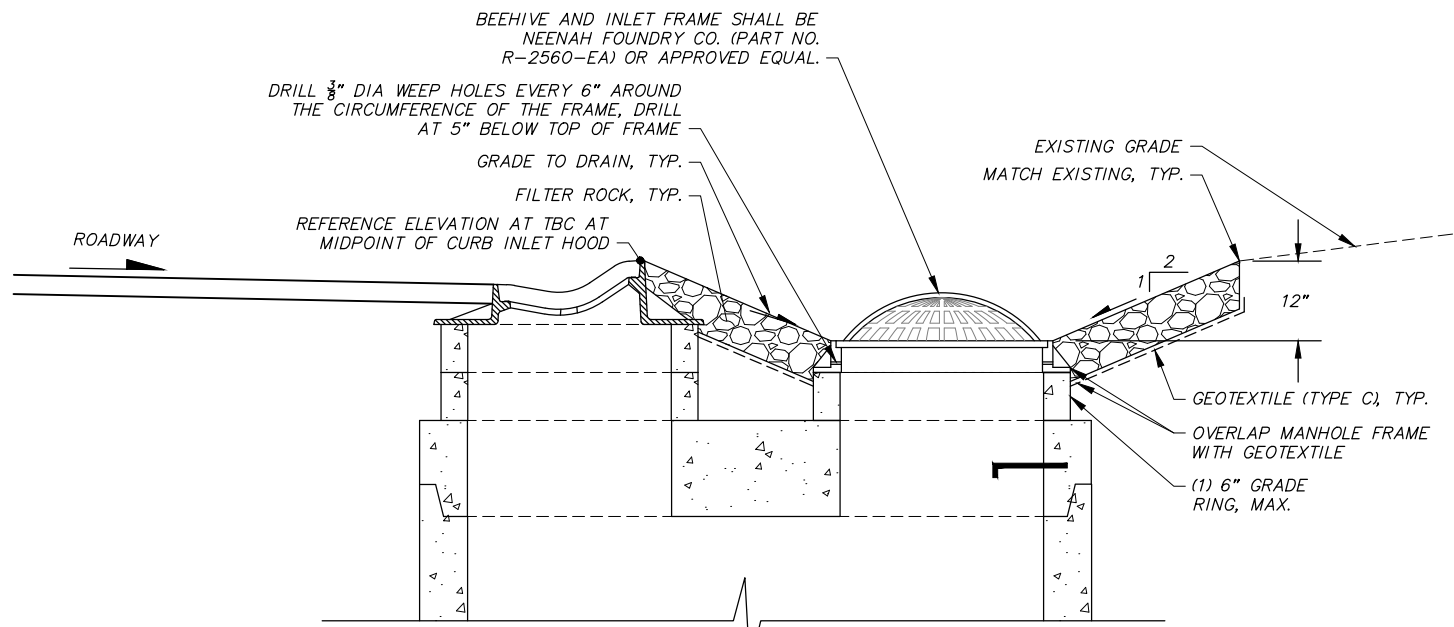
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TOPOGRAPHY	CB	BW		
PROFILE	RB	JK		
STORM SEWER	JM	JH		
WATER/SANITARY SEWER	CK	JK		
GAS	CB	BW		
TELEPHONE	CB	BW		
ELECTRIC	JH	TK		
DESIGN	RB	JK		
QUANTITIES	RB	JK		
PRELIMINARY/FINAL	RB	JK		
MUNICIPAL/STATE	RB	JK		

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE	SCHED B	
STORM DRAIN DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW2033 DATE DEC 2024	STATUS 95% SHEET SD7 of SD10



REFER TO MASS STANDARD DETAIL 55-28 FOR DUAL ENTRY MANHOLE (TYPE II CATCH BASIN MANHOLE)

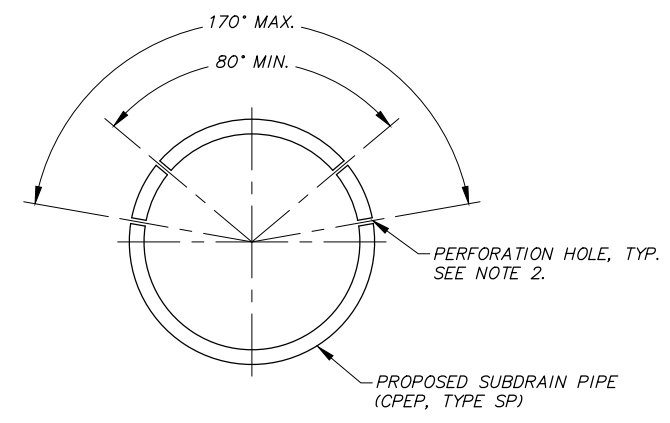
TYPE II CATCHBASIN MANHOLE WITH BEEHIVE INLET NOTES

1. FILTER ROCK AND GEOTEXTILE SHALL BE INCIDENTAL TO PAY ITEM 55.05 (CONSTRUCT (TYPE II) CATCH BASIN MANHOLE)

TYPE II CATCH BASIN MANHOLE WITH BEEHIVE INLET

SCALE: NTS

1



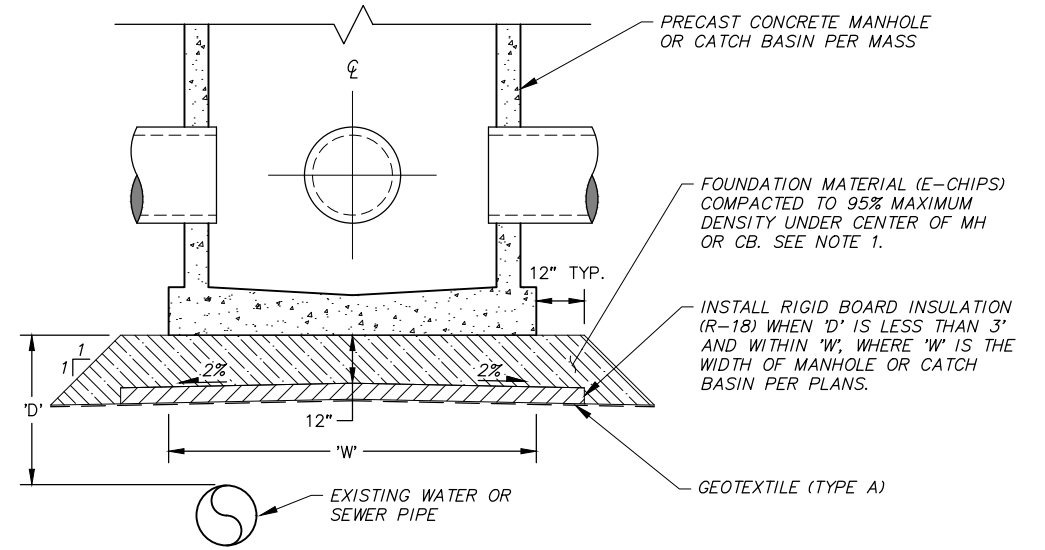
PERFORATION NOTES:

1. PERFORATION PATTERN SHALL BE CLASS 1 PER AASHTO M294 FOR SELECT SUBDRAIN PIPES AS SPECIFIED ON STORM DRAIN PLAN AND PROFILE SHEETS.
2. THE ORIENTATION OF CLASS 1 PERFORATIONS SHALL BE LOCATED ABOVE SPRINGLINE OF THE PIPE AS SHOWN ON THIS DETAIL.

CLASS 1 PERFORATION PATTERN & SUBDRAIN PIPE ORIENTATION

SCALE: NTS

3



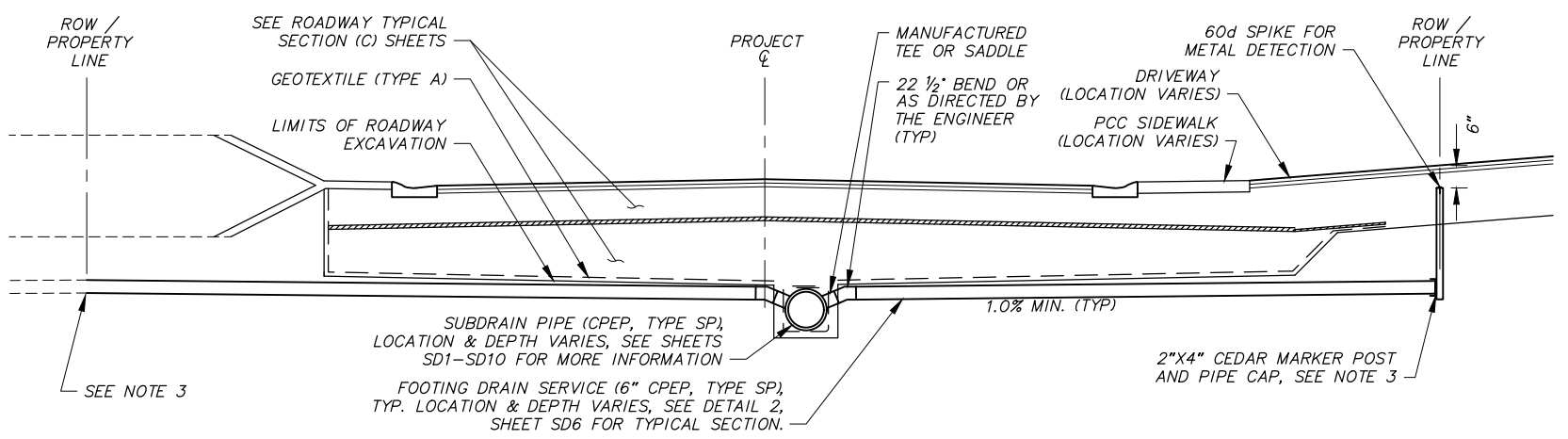
FOUNDATION BACKFILL & STORM DRAIN STRUCTURE INSULATION NOTES

1. INSTALL FOUNDATION MATERIAL (E-CHIPS) AS DIRECTED BY ENGINEER OR WHERE INSULATION IS REQUIRED. PAYMENT FOR GEOTEXTILE SHALL BE INCIDENTAL TO PAY ITEM 20.19 FOUNDATION BACKFILL (E-CHIPS)

FOUNDATION BACKFILL & STORM DRAIN STRUCTURE INSULATION DETAIL

SCALE: NTS

2



FOOTING DRAIN SERVICE NOTES

1. FINAL LOCATION OF THE FOOTING DRAIN SERVICE MAY BE ADJUSTED BY THE ENGINEER.
2. WHEN FOOTING DRAIN CONNECTS DIRECTLY TO A MANHOLE, OMIT THE 22 1/2\"/>

FOOTING DRAIN SERVICE DETAIL

SCALE: NTS

4

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RECORD DRAWING

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CONTRACTOR: _____ TITLE: _____ DATE: _____

BY: _____

2. DATA TRANSFERRED BY: _____ TITLE: _____

COMPANY: _____ DATE: _____

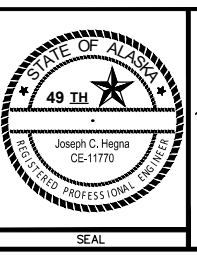
3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.

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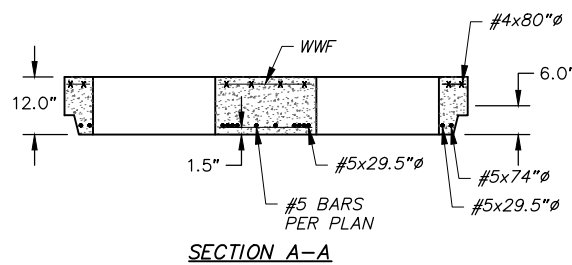
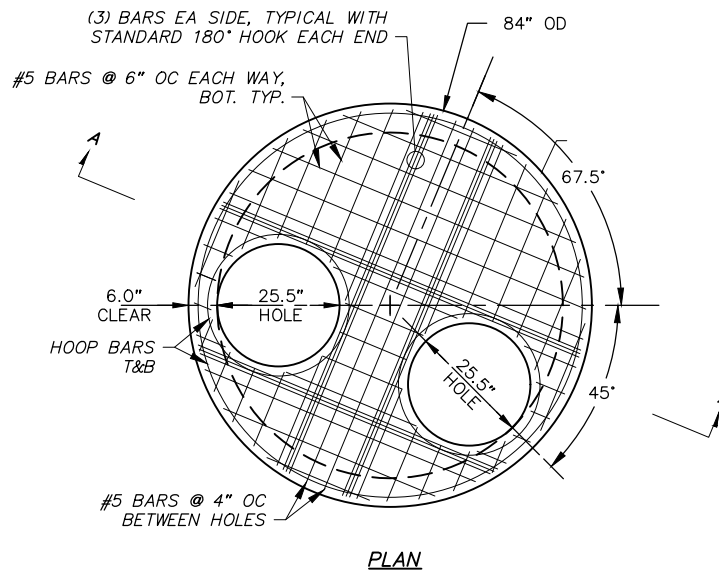
COMPANY: _____ DATE: _____

BY: _____

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WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
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TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								
PLAN CHECK										
CONSTRUCTION RECORD										
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REVISIONS										
CONSULTANT										
SEAL										



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT			
21-13	QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE	SCHED B	
STORM DRAIN DETAILS			
SCALE	HOR. N/A VER. N/A	GRID SW2033 DATE DEC 2024	STATUS 95% SHEET 8 of 10



REDUCING SLAB NOTES

1. CONCRETE MINIMUM DESIGN STRENGTH OF 4,000 PSI.

1 **MODIFIED PRECAST CONCRETE TWO HOLE REDUCING SLAB DETAIL**
SCALE: NTS

File: I:\labdata\101555.00 Quinhagak Street Reconstruction\00_CADD_2018\01_Working Set\01_Civil\101555.00_Details - Storm_Drain.dwg

RECORD DRAWING
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TOPOGRAPHY	CB	BW
PROFILE	RB	JK
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GAS	CB	BW
TELEPHONE	CB	BW
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DESIGN	RB	JK
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MUNICIPAL/STATE	RB	JK

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795, 3797, 3798 & 3830	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL

CRW ENGINEERING GROUP
 3940 ARCTIC BLVD. SUITE 300
 ANCHORAGE, ALASKA 99503
 PHONE: (907) 562-3252
 #AECLE882-AK

STATE OF ALASKA
 49 TH
 Joseph C. Hegna
 CE-11770
 REGISTERED PROFESSIONAL ENGINEER



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED B
STORM DRAIN DETAILS
 SCALE HOR. N/A VER. N/A
 GRID SW2033
 DATE DEC 2024 STATUS 95%
 SHEET SD9 of SD10

20.26

INSULATION BOARD (R-18) – PIPE CROSSINGS & STORM DRAIN INSULATION							
SHEET	BEGIN STATION	END STATION	OFFSET	WIDTH (FT)	LENGTH (FT)	AREA (SF)	COMMENTS
SD1	11+94	–	0.68' LT	4	8	32	WATER CROSSING (64TH AVENUE AT QUINHAGAK STREET)
	12+18	–	7.83' LT	4	8	32	SEWER MAIN
	12+90	–	2.05' LT	4	8	32	SEWER SERVICE (PARCEL 5)
	13+89	–	6.89' LT	4	8	32	SEWER MAIN
SD2	15+00	–	0.42' LT	4	8	32	WATER SERVICE (PARCEL 12)
	15+87	–	14.35' RT	4	8	32	WATER CROSSING (QUINHAGAK STREET)
SD4	30+71	–	0.16' RT	4	8	32	SEWER MAIN
	30+79	–	0.21' RT	4	8	32	WATER SERVICE (PARCEL 8)
SD5	50+14	–	2.30' RT	4	8	32	WATER CROSSING (63RD AVENUE AT QUINHAGAK STREET)

INSULATION BOARD NOTES:

- INSULATION BOARD SHALL BE INSTALLED I.A.W. TYPICAL STORM DRAIN AND SUBDRAIN TYPICAL SECTIONS (SEE SHEET SD6) AND MASS STANDARD DETAIL 20-9.

55.18 – CONSTRUCT FOOTING DRAIN SERVICE								
SHEET	PARCEL	AT PROPERTY LINE		AT MAIN		APPROX. LENGTH (FT)	CONNECT TO / COMMENTS	ELEVATION AT ROW (2)
		STATION	OFFSET (FT)	STATION	OFFSET (FT)			
SD1	6	10+55	34.0 RT	10+55	CL	34.0	SUBDRAIN PIPE (P1-1)	
	10	10+84	26.0 LT	10+84	CL	26.0	SUBDRAIN PIPE (P1-1)	
	11	12+54	26.0 LT	12+54	16.5 LT	9.5	CATCH BASIN (I1-3)	
SD2	3	18+68	34.0 RT	18+68	1.4 RT	32.6	SUBDRAIN PIPE (P2-7)	
	13	15+25	26.0 LT	15+25	0.2 LT	25.8	SUBDRAIN PIPE (P1-11)	
	14	16+68	26.0 LT	16+38	0.9 RT	40.3	SUBDRAIN PIPE (P2-7)	
	15	18+38	26.0 LT	18+38	1.3 RT	27.3	SUBDRAIN PIPE (P2-7)	
	16	19+34	26.4 LT	19+35	1.0 RT	27.4	SUBDRAIN PIPE (P2-10)	
SD3	1	22+81	30.5 RT	22+81	1.3 RT	29.3	MANHOLE (S3-2)	
	2	20+60	30.5 RT	20+60	2.2 LT	32.7	SUBDRAIN PIPE (P2-10)	
	17	20+83	29.5 LT	20+83	2.2 LT	27.3	SUBDRAIN PIPE (P2-10)	

FOOTING DRAIN SERVICE NOTES:

- FOOTING DRAIN SERVICES SHALL BE INSTALLED PER DETAIL 4, SHEET SD8 AND THE SPECIAL PROVISIONS.
- TO BE COMPLETED BY CONTRACTOR AS PART OF AS-BUILT DRAWINGS.
- FOOTING DRAIN SERVICES SHALL BE INSTALLED A MINIMUM OF 11 FEET FROM ANY WATER SERVICE KEY BOX.
- FOOTING DRAIN SERVICES SHALL BE CONSTRUCTED I.A.W. TYPICAL SUBDRAIN TRENCH SECTION (DETAIL 2, SHEET SD6) UNLESS OTHERWISE NOTED.

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RECORD DRAWING

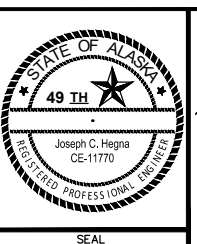
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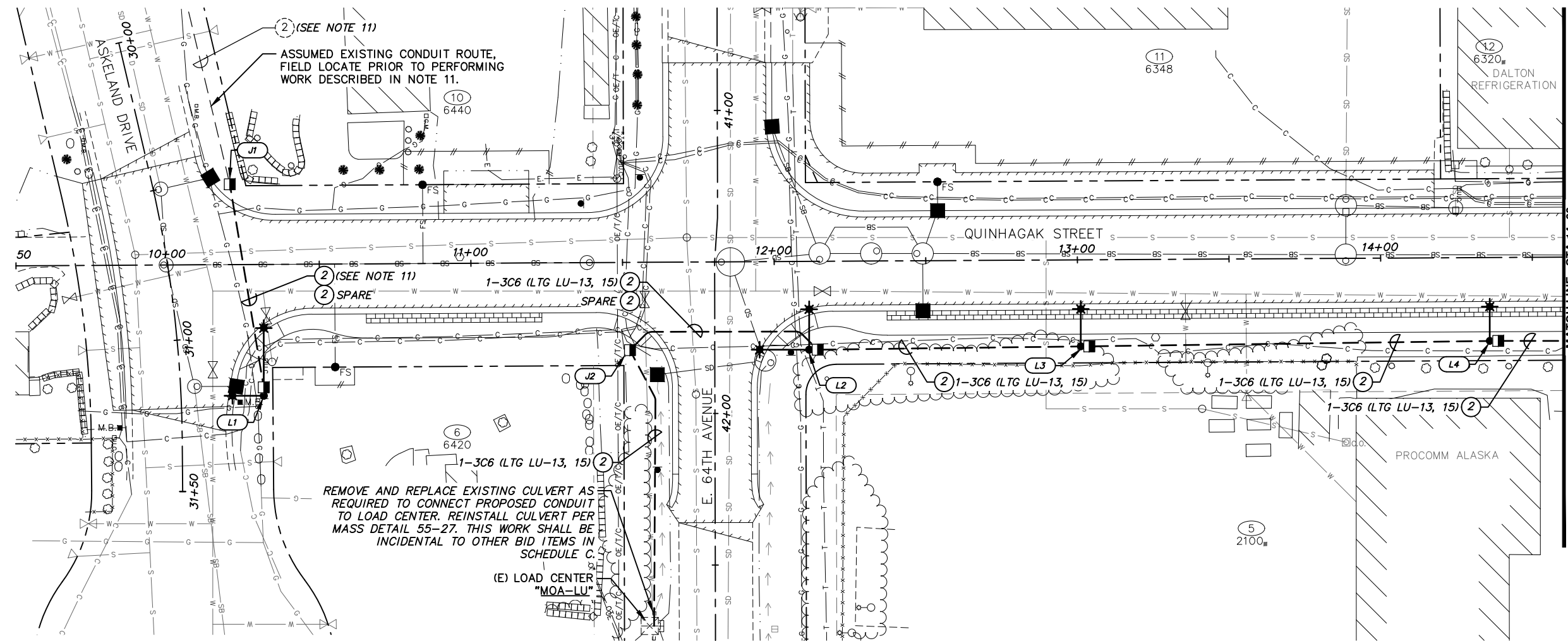


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED B

STORM DRAIN SUMMARY TABLES

SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: SD10 of SD10



2 (SEE NOTE 11)
 ASSUMED EXISTING CONDUIT ROUTE,
 FIELD LOCATE PRIOR TO PERFORMING
 WORK DESCRIBED IN NOTE 11.

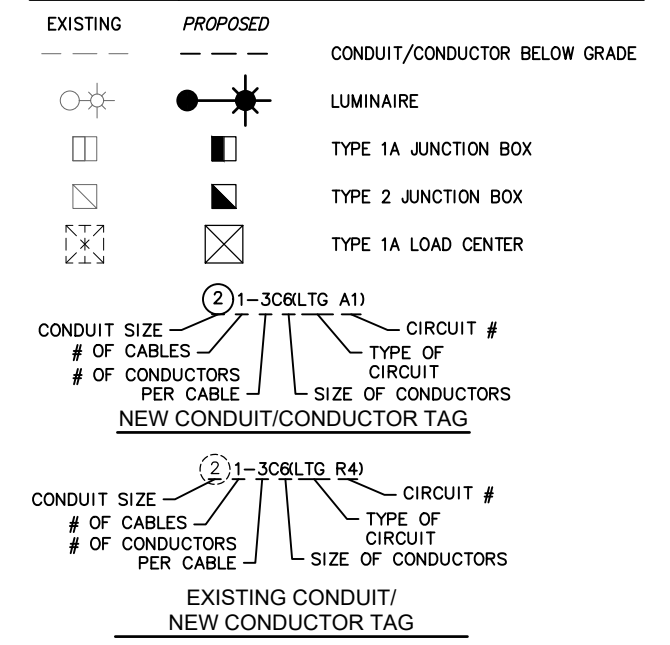
REMOVE AND REPLACE EXISTING CULVERT AS
 REQUIRED TO CONNECT PROPOSED CONDUIT
 TO LOAD CENTER. REINSTALL CULVERT PER
 MASS DETAIL 55-27. THIS WORK SHALL BE
 INCIDENTAL TO OTHER BID ITEMS IN
 SCHEDULE C.

(E) LOAD CENTER
 "MOA-LU"

ILLUMINATION NOTES:

1. PROVIDE HOT DIP GALVANIZED STEEL POLES WITH MAST ARMS PER MOA STANDARD DETAILS 80-19 AND 80-20, RESPECTIVELY.
2. ALL LUMINAIRE POLE FOUNDATIONS SHALL BE DRIVEN PILE UNLESS OTHERWISE NOTED ON THE DRAWINGS. PILE EMBEDMENT DEPTH SHALL BE 25' MINIMUM. LUMINAIRE POLE FOUNDATION SHALL BE LOCATED A MINIMUM OF 3 FEET FROM BACK OF SIDEWALK/PATHWAY OR A MINIMUM OF 7 FEET FROM BACK OF CURB OR SHOULDER. WHEN POLE LOCATION IS WITHIN 10' OF A UTILITY, EXCAVATE A HOLE TO 12" BELOW ANTICIPATED UTILITIES DEPTH WITH A VACTOR TRUCK BEFORE DRIVING PILE. THIS WORK SHALL BE INCIDENTAL TO THE SECTION 80.04 PAY ITEM. SEE 2024 MASS DETAIL 80-9. CONTRACTOR SHALL STAKE LUMINAIRE POLE LOCATIONS IN THE FIELD FOR ENGINEERS REVIEW AND APPROVAL PRIOR TO INSTALLATION OF PILES.
3. INSTALL THE POLES WITH FIXED BASES PER 2024 MASS DETAIL 80-9.
4. LUMINAIRES APPROVED FOR SUBSTITUTION SHALL PROVIDE THE LIGHT LEVELS AND UNIFORMITIES INDICATED IN THE LIGHT LEVELS TABLE.
5. PROVIDE THE POLE SHAFT LENGTHS AND MAST ARM LENGTHS SHOWN IN THE ROADWAY LUMINAIRE SCHEDULE.
6. PROVIDE RIGID METAL CONDUIT (RMC) WITH A BARE, STRANDED COPPER GROUND FOR ALL RACEWAYS. GROUND TO BE SIZED TO EQUAL THE LARGEST CONDUCTOR SIZE IN THE CONDUIT, MINIMUM #6 AWG.
7. PROVIDE ONE SPARE 2" RMC WITH PULL ROPE BETWEEN THE JUNCTION BOXES ADJACENT TO EVERY ROAD CROSSING.
8. PROVIDE A 3 CONDUCTOR CABLE FOR EACH BRANCH CIRCUIT. SIZE AS SHOWN ON THE DRAWINGS.
9. INSTALL THE JUNCTION BOX WITHIN 3' OF THE POLE OR LOAD CENTER. DO NOT INSTALL JUNCTION BOXES IN SIDEWALKS, PATHWAYS, TRAILS, DRIVEWAYS, OR DRAINAGE DITCHES OR ON PRIVATE PROPERTY. JUNCTION BOXES INSTALLED BEHIND SIDEWALKS, PATHWAYS OR TRAILS SHALL HAVE A MINIMUM SETBACK OF 2' AND BE PLACED BEHIND OR ON THE DOWN TRAFFIC SIDE OF FOUNDATIONS.
10. IN THE DRAWINGS, EACH JUNCTION BOX HAS THE SAME IDENTIFYING NUMBER AS THE LIGHT POLE OR LOAD CENTER NEXT TO IT. FOR JUNCTION BOXES LOCATED BETWEEN POLES, THE IDENTIFYING NUMBER INCLUDES THE SMALLER OF THE TWO POLE NUMBERS BETWEEN WHICH THE JUNCTION BOX IS LOCATED.
11. REMOVE CONDUCTORS SERVING DEMOLISHED LUMINAIRE ON ASKELAND DRIVE/QUINHAGAK STREET INTERSECTION TO PREVIOUS LIGHT. FIELD LOCATE AND CUT CONDUIT NEAREST TO LOCATION OF JUNCTION BOX SHOWN AND PROVIDE NEW JUNCTION BOX OVER CUT LOCATION. PROVIDE CONDUITS AS SHOWN AND 1-3C6 CABLE TO CONNECT L1 TO LUMINAIRE FORMERLY SERVING DEMOLISHED LUMINAIRE.

LEGEND



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TELEPHONE	CB	BW
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QUANTITIES	RB	JK
PRELIMINARY/FINAL	RB	JK
MUNICIPAL/STATE	RB	JK

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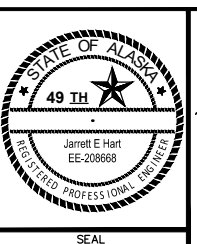
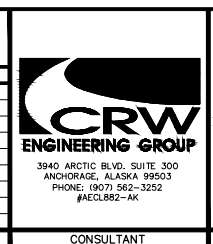
STAKING

ASBUILT

CONTRACTOR

INSPECTOR

BASIS OF THIS DATUM GAAB 1972 ADJUST



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED C

ILLUMINATION PLAN

BOP TO STA 14+60

SCALE HOR. 1"=20'
 VER. N/A

GRID 5W2033

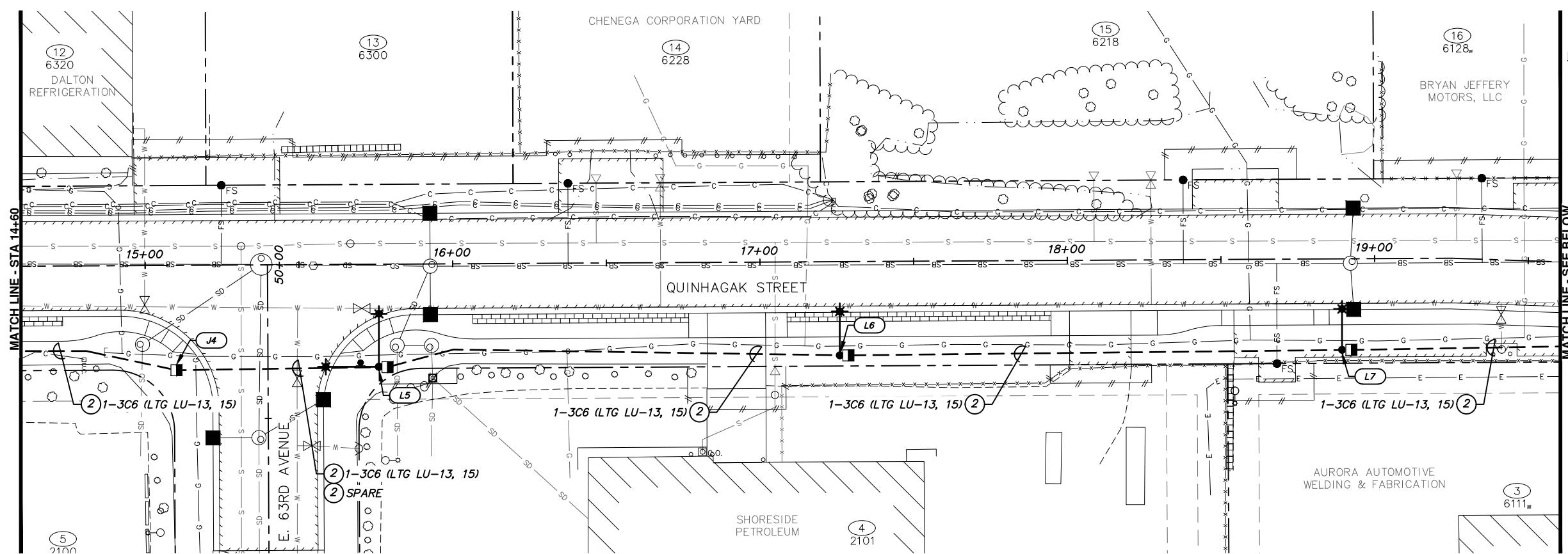
DATE DEC 2024

STATUS 95%

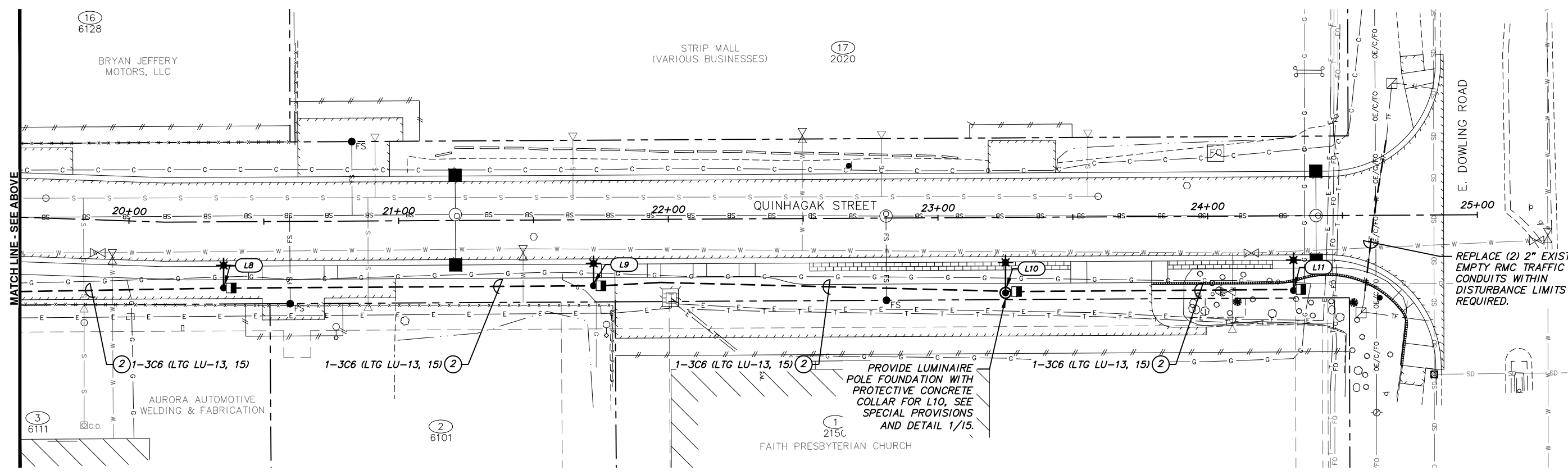
SHEET 11 of 16

File: E:\JobsData\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\03 Electrical\10155.00 Illumination Plans.dwg

File: I:\labdata\10155.00_Quinhagak Street Reconstruction\00_CADD_2019\01_Working Set\03_Electrical\10155.00_Illumination_Plans.dwg



ILLUMINATION NOTES:
1. SEE SHEET 11 FOR ILLUMINATION NOTES.



REPLACE (2) 2" EXISTING EMPTY RMC TRAFFIC CONDUITS WITHIN DISTURBANCE LIMITS AS REQUIRED.

RECORD DRAWING
1. DATA PROVIDED BY: _____ TITLE: _____
THIS WILL SERVE TO CERTIFY THAT THESE RECORD DRAWINGS ARE A TRUE AND ACCURATE REPRESENTATION OF THE PROJECT AS CONSTRUCTED.
CONTRACTOR: _____ TITLE: _____ DATE: _____
BY: _____

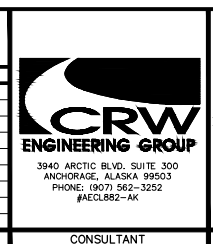
2. DATA TRANSFERRED BY: _____ TITLE: _____
COMPANY: _____ DATE: _____

3. BASED ON PERIODIC FIELD OBSERVATIONS BY THE ENGINEER (OR AN INDIVIDUAL UNDER HIS/HER DIRECT SUPERVISION), THE CONTRACTOR-PROVIDED DATA APPEARS TO REPRESENT THE PROJECT AS CONSTRUCTED.
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DATA	DRAWN BY	CHECKED BY	DATE
BASE	CB	BW	
TOPOGRAPHY	CB	BW	
PROFILE	RB	JK	
STORM SEWER	JM	JH	
WATER/SANITARY SEWER	CK	JK	
GAS	CB	BW	
TELEPHONE	CB	BW	
ELECTRIC	JH	TK	
DESIGN	RB	JK	
QUANTITIES	RB	JK	
PRELIMINARY/FINAL	RB	JK	
MUNICIPAL/STATE	RB	JK	

FIELD BOOKS	BM NO.	LOCATION	ELEV.	REV.	DATE	DESCRIPTION	BY
DESIGN CRW BOOK No. 3795,	GAAB 22	See MOA Benchmark Book, Page D-29	162.82'				
3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK	CONSTRUCTION RECORD	VERTICAL DATUM	REVISIONS	CONSULTANT	SEAL



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED C

ILLUMINATION PLAN

STA 14+60 TO EOP

SCALE HOR. 1"=20' VER. N/A

GRID SW2033 DATE DEC 2024 STATUS 95% SHEET 12 of 16

LIGHT LEVELS TABLE

LOCATION	MOA REQUIRED MIN. AVERAGE ILLUMINANCE (FC)	AVERAGE DESIGN ILLUMINANCE (FC)	MOA REQUIRED MAXIMUM UNIFORMITY RATIO	DESIGN UNIFORMITY RATIO	MOA REQUIRED MAX. VEILING LUMINANCE RATIO	DESIGN VEILING LUMINANCE RATIO
QUINHAGAK ST	0.7	0.7	4.0:1	2.4:1	0.4:1	0.4:1
QUINHAGAK ST/ASKELAND DR INTX	1.6	1.6	4.0:1	3.9:1	-	-
QUINHAGAK ST/E. 64TH AVE INTX	1.6	1.6	4.0:1	3.9:1	-	-
QUINHAGAK ST/E. 63RD AVE INTX	1.6	1.6	4.0:1	2.7:1	-	-
SIDEWALKS/PATHWAYS	0.5	0.5	4.0:1	2.5:1	-	-

NOTES:

1. MOA REQUIREMENTS ARE FROM 2007 DCM CHAPTER 5 FOR A LOCAL ROADWAY WITH MEDIUM PEDESTRIAN CONFLICT.
2. ALL INTERSECTIONS TO BE UPGRADED WITH NEW LIGHT ARE CLASSIFIED AS COLLECTOR/LOCAL.
3. LIGHT LOSS FACTOR (LLF) = 0.85.
4. MOUNTING HEIGHTS ARE 30'.
5. SEE LUMINAIRE DEFINITION AND SCHEDULE FOR LUMINAIRES USED AS BASIS OF DESIGN.

LUMINAIRE DEFINITION

TYPE	SYMBOL	MAKE	MODEL	LAMP	CCT*	DISTRIBUTION	VOLTAGE	COLOR	OPTIONS	MOUNT
ROADWAY		GE	ERL	SEE LUMINAIRE SCHEDULE	3000K	SEE LUMINAIRE SCHEDULE	240	GREY	7-PIN RECEPTACLE WITH SHORTING CAP, BACKLIGHT SHIELD	MAST ARM

*CCT = CORRELATED COLOR TEMPERATURE

ROADWAY LUMINAIRE SCHEDULE

POLE	STATION	OFFSET (FT)	SHAFT LENGTH	MAST ARM LENGTH	LUMENS	DISTRIBUTION
L1	10+31.5	43.31 RT	28'	21'	10,000	TYPE 2, MEDIUM
				9*	10,000	TYPE 2, MEDIUM
L2	12+11.4	29.13 RT	27'	12'	10,000	TYPE 2, MEDIUM
				15*	10,000	TYPE 2, MEDIUM
L3	13+01.0	28.53 RT	28'	11'	6,000	TYPE 2, MEDIUM
L4	14+35.8	27.50 RT	28'	10'	6,000	TYPE 2, MEDIUM
L5	15+75.8	33.42 RT	26'	16'	10,000	TYPE 2, MEDIUM
				16'	10,000	TYPE 2, MEDIUM
L6	17+25.8	30.52 RT	28'	13'	6,000	TYPE 2, MEDIUM
L7	18+89.2	29.64 RT	29'	12'	6,000	TYPE 2, MEDIUM
L8	20+34.9	24.50 RT	29'	7'	6,000	TYPE 2, MEDIUM
L9	21+72.1	24.50 RT	29'	7'	6,000	TYPE 2, MEDIUM
L10	23+24.9	27.50 RT	28'	9'	6,000	TYPE 2, MEDIUM
L11	24+31.7	27.74 RT	28'	10'	6,000	TYPE 2, MEDIUM

* = OVER INTERSECTING ROADWAY

JUNCTION BOX SCHEDULE

J-BOX	TYPE	CIRCUIT	STATION	OFFSET
J1	1A	LU-13, 15	10+20.7	26.51 LT
J2	1A	LU-13, 15	11+52.3	28.92 RT
J4	1A	LU-13, 15	15+09.9	34.15 RT

NOTE: ONLY JUNCTION BOXES NOT ASSOCIATED WITH AN LUMINAIRE OR LOAD CENTER ARE SHOWN IN THIS TABLE.

File: I:\jobdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2018\01 Working Set\03 Electrical\10155.00 Illumination Schedules.dwg

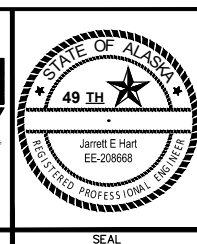
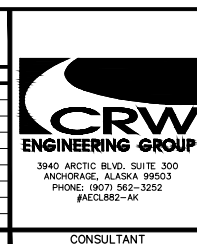
RECORD DRAWING

1. DATA PROVIDED BY: _____ TITLE: _____
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 BY: _____

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TOPOGRAPHY	CB	BW								
PROFILE	RB	JK								
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WATER/SANITARY SEWER	CK	JK		GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				
GAS	CB	BW								
TELEPHONE	CB	BW								
ELECTRIC	JH	TK								
DESIGN	RB	JK								
QUANTITIES/FINAL	RB	JK								
PRELIMINARY/FINAL	RB	JK								
MUNICIPAL/STATE	RB	JK								
PLAN CHECK										
CONSTRUCTION RECORD										
VERTICAL DATUM										
REVISIONS										
CONSULTANT										
SEAL										

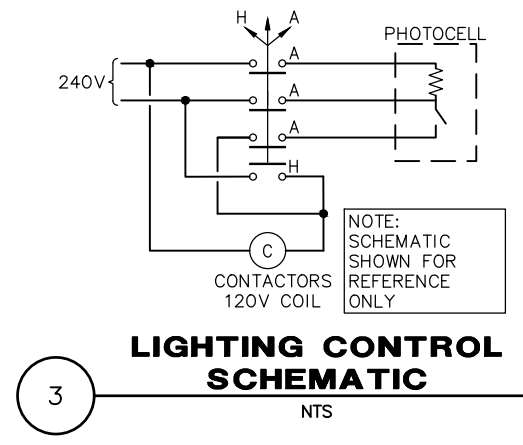
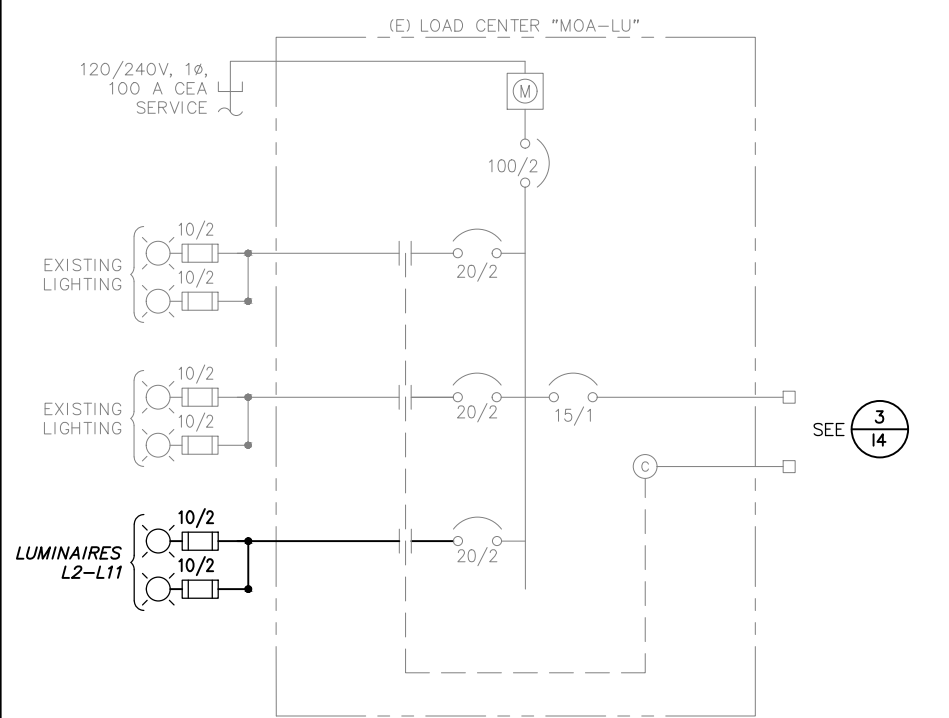


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED C

ILLUMINATION SCHEDULES

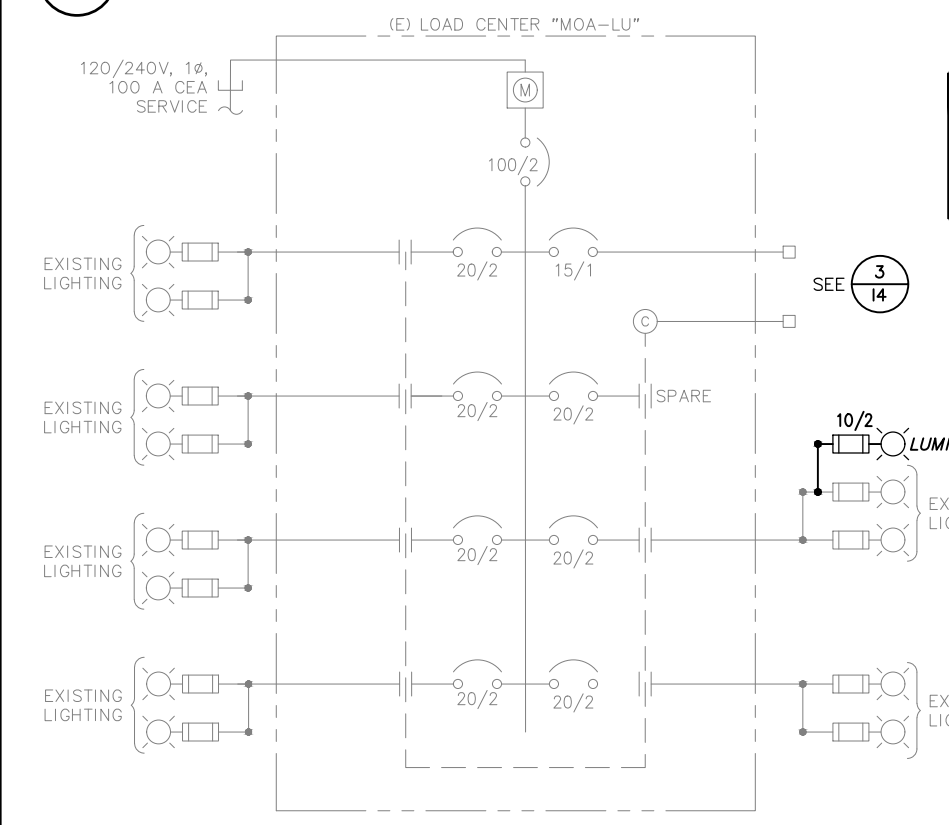
SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: 13 of 16



LIGHTING CONTROL SCHEMATIC
NTS

(E) LOAD CENTER POWER ONE-LINE (E. 64TH AVE)
NTS

1



VOLTAGE DROP SUMMARY					
CIRCUIT	SIZE	LENGTH	VOLTAGE	CURRENT	V.D.
LTG-4 (68TH)	VARIES	2200	240	3.71	2.03%
LU-13,15	#6 AWG	1370	240V	3.11	2.57%

(E) LOAD CENTER POWER ONE-LINE (E. 68th AVE)
NTS

2

LOAD CENTER NO. MOA-LU (EXISTING) TYPE: 1A
 LOCATION: E. 64TH AVENUE
2 POLE, 100 AMP CONTACTOR
 MAIN BREAKER: 2 POLE, 100 AMPS, 240 VOLTS

PANEL A 100 MAIN BREAKER, 120/240 VOLTS SINGLE PHASE 3 WIRE
65,000 AMPS INTERRUPT CAPACITY

CKT.	CIRCUIT DESCRIPTION	KVA	AMP	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	17	18	AMP	KVA	CIRCUIT DESCRIPTION	CKT.	
LU1	MAIN BREAKER		100/2	1	2																				LU2	
LU3																										
LU5	EXISTING LIGHTING		20/2	5	6																	15/1	0.5	PHOTOELECTRIC CONTROL	LU6	
LU7																										
LU9	EXISTING LIGHTING		20/2	9	10																					LU10
LU11																										
LU13	LUMINAIRES L2-L11	0.9	20/2	11	12																					LU14
LU15																										
LU17				13	14																					LU18

NOTE: ALL BREAKERS ARE EXISTING-TO-REMAIN

TOTAL CONNECTED LOAD = 1.4 KVA
 TOTAL AMPS = 6.0 A

LOAD CENTER NO. MOA-LU (EXISTING) TYPE: 1A
 LOCATION: ASKELAND DR/ E. 68TH AVE
2 POLE, 100 AMP CONTACTOR
 MAIN BREAKER: 2 POLE, 100 AMPS, 240 VOLTS

PANEL A 100 MAIN BREAKER, 120/240 VOLTS SINGLE PHASE 3 WIRE
65,000 AMPS INTERRUPT CAPACITY

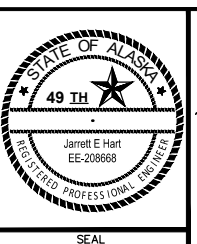
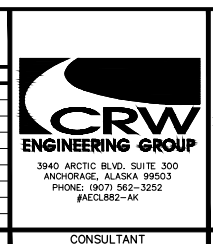
CKT.	CIRCUIT DESCRIPTION	KVA	AMP	1	2	3	4	5	6	7	8	9	10	11	12	13	14	15	16	AMP	KVA	CIRCUIT DESCRIPTION	CKT.			
MAIN	MAIN BREAKER		100/2	1	2															15/1		PHOTOELECTRIC CONTROL	CTRL			
LTG 1	EXISTING LIGHTING		20/2	3	4																				LTG 2	
LTG 3																										
LTG 5	EXISTING LIGHTING		20/2	5	6																					LTG 6

NOTES: 1. ALL BREAKERS ARE EXISTING-TO-REMAIN
 2. L1 IS ESTIMATED TO INCREASE DEMAND BY 0.34A (WITH 125% DF)

TOTAL CONNECTED LOAD = 1.1 KVA
 TOTAL AMPS = 4.6 A

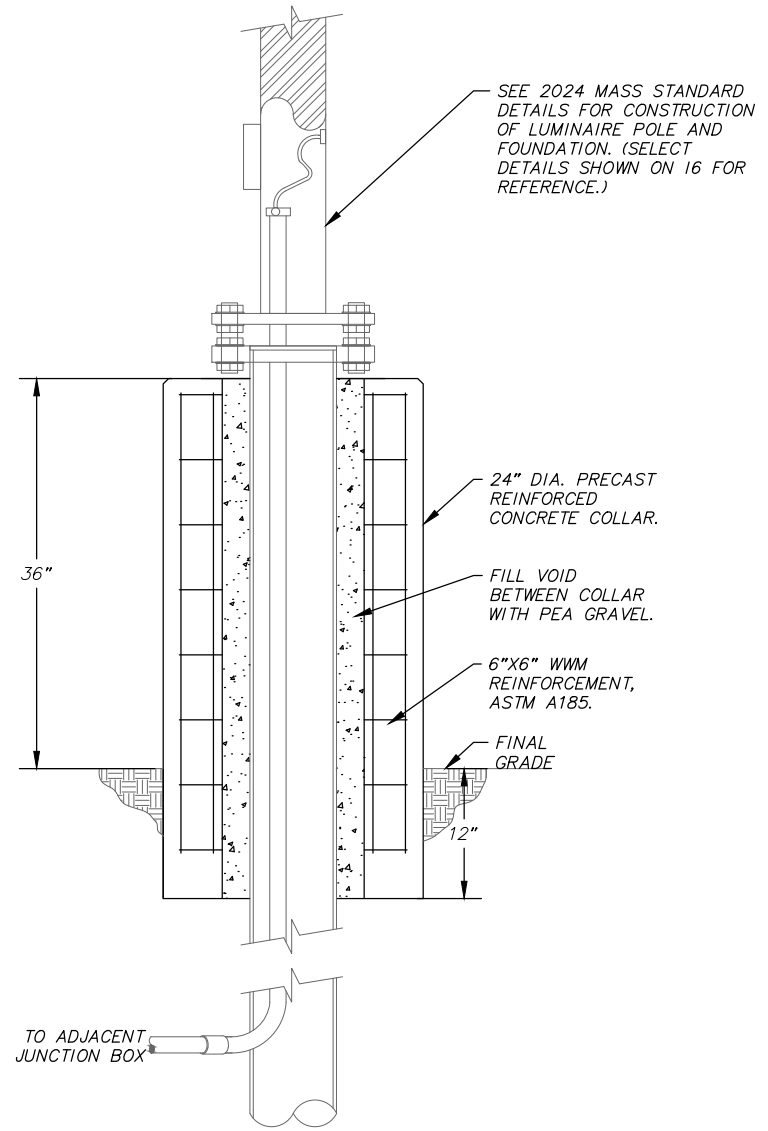
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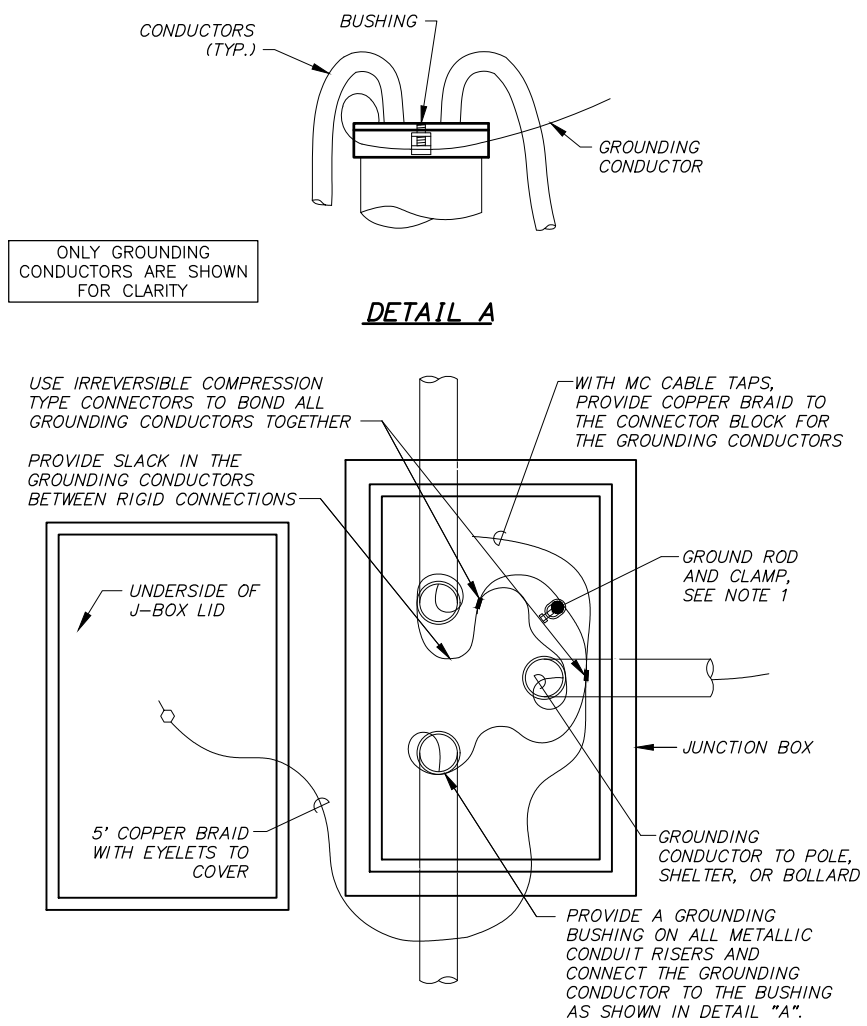


PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT
 21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED C
LOAD CENTER SCHEDULE AND SCHEMATICS
 SCALE: HOR. N/A VER. N/A GRID: SW2033 DATE: DEC 2024 STATUS: 95% SHEET: 14 of 16

File: I:\labdata\10155.00 Quinhagak Street Reconstruction\00_CADD_2019\01 Working Set\03 Electrical\10155.00 Illumination Details.dwg



1 CONCRETE COLLAR DETAIL
NTS



2 JUNCTION BOX GROUNDING DETAIL
NTS

RECORD DRAWING

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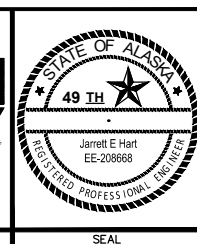
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MUNICIPAL/STATE	RB	JK								

CRW ENGINEERING GROUP

3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
FAC: (907) 562-3252



PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION E. DOWLING ROAD TO ASKELAND DRIVE SCHED C

ILLUMINATION DETAILS

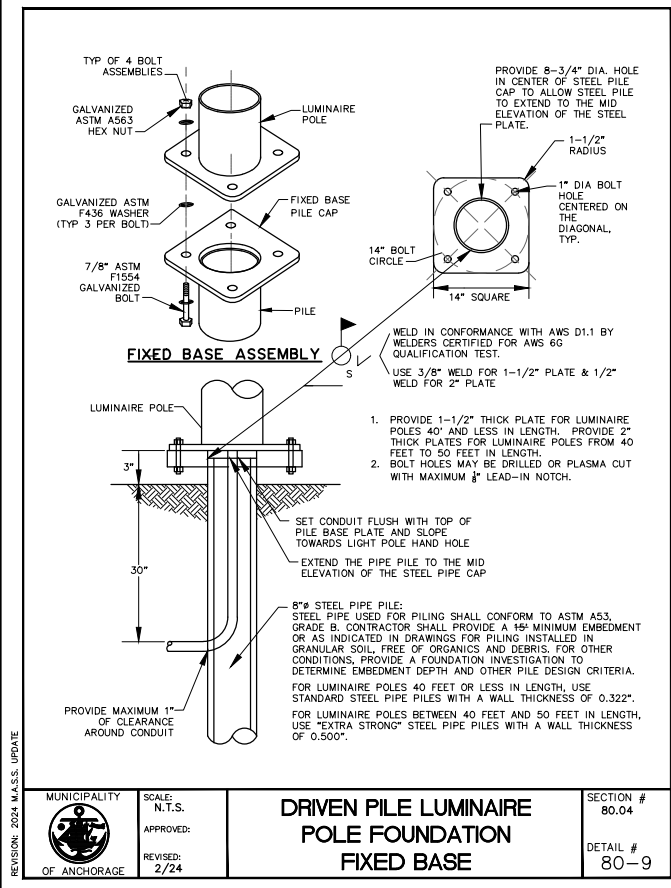
SCALE: HOR. N/A VER. N/A

GRID: SW2033

DATE: DEC 2024

STATUS: 95%

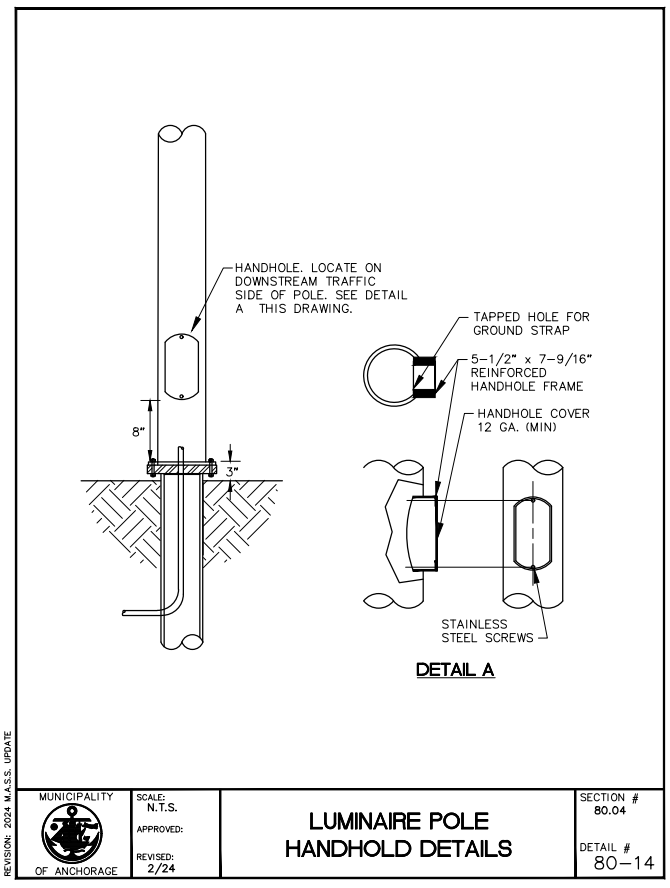
SHEET: 15 of 16



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

DRIVEN PILE LUMINAIRE POLE FOUNDATION FIXED BASE

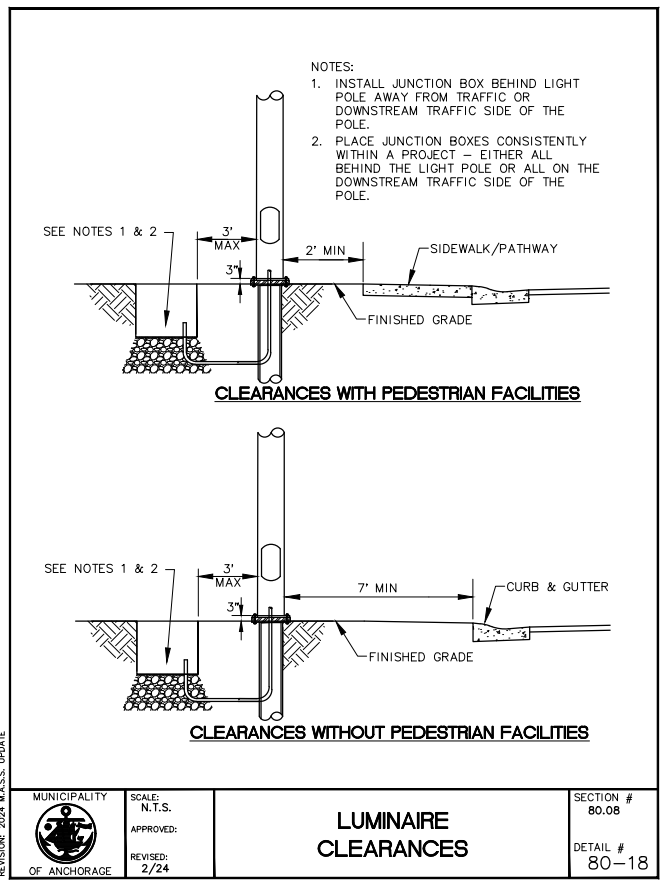
SECTION # 80.04
DETAIL # 80-9



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

LUMINAIRE POLE HANDHOLD DETAILS

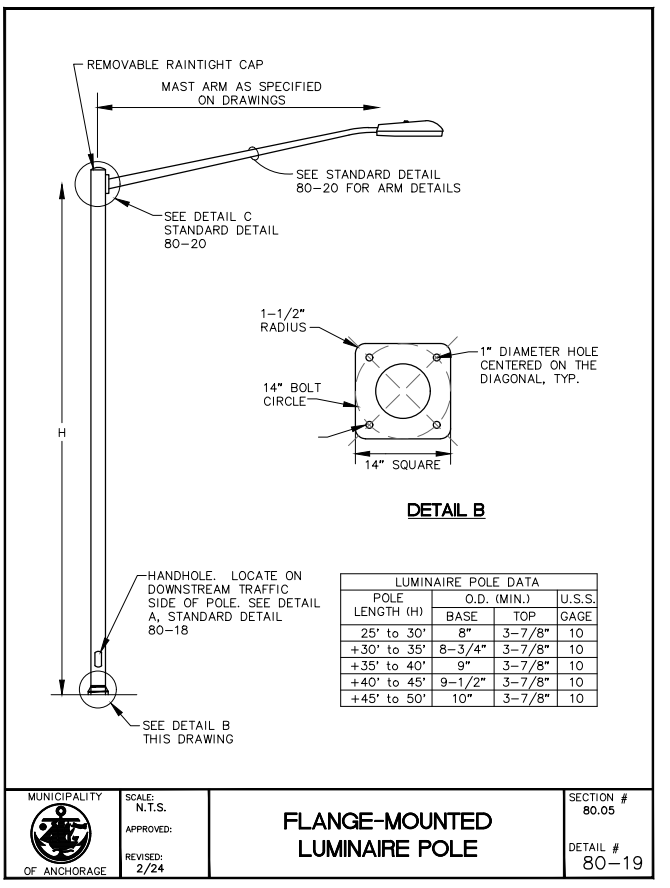
SECTION # 80.04
DETAIL # 80-14



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

LUMINAIRE CLEARANCES

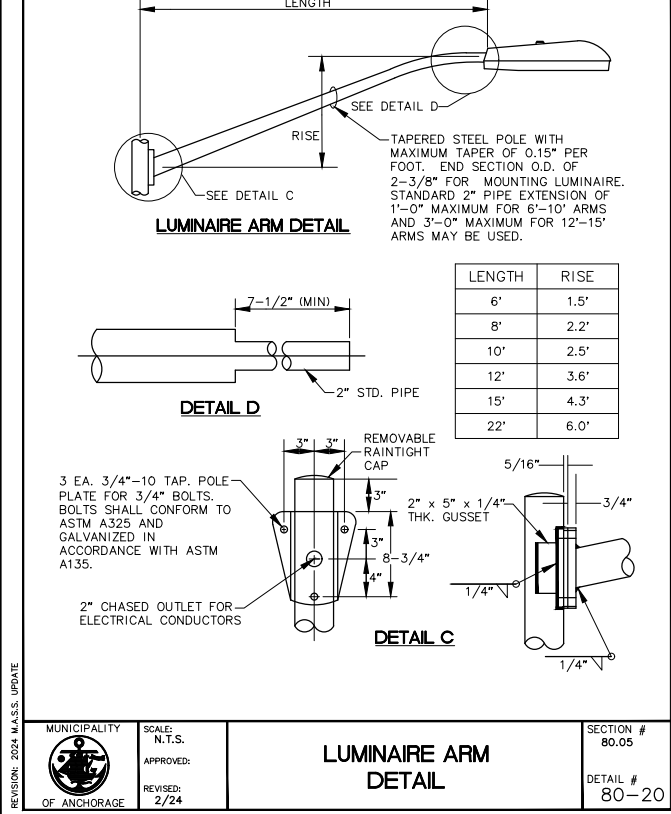
SECTION # 80.08
DETAIL # 80-18



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

FLANGE-MOUNTED LUMINAIRE POLE

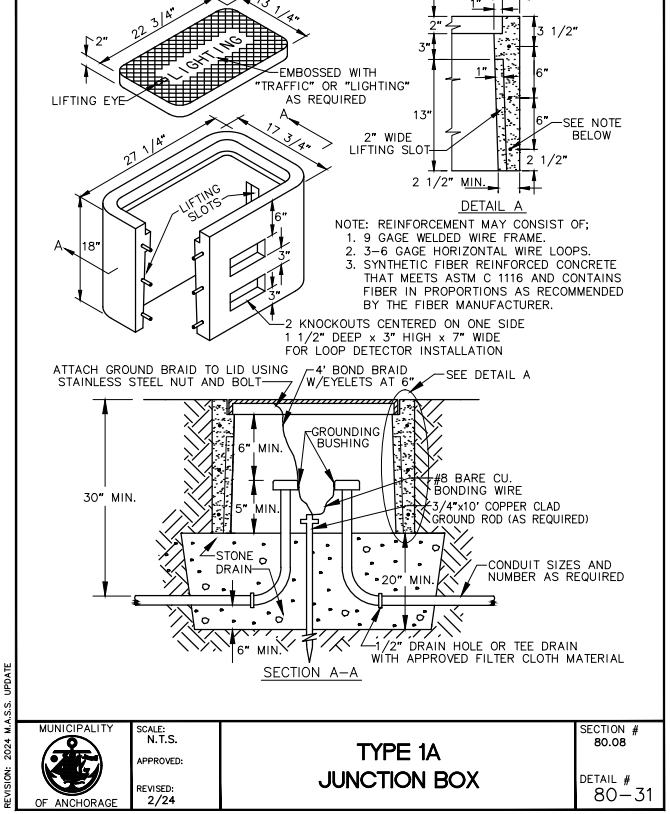
SECTION # 80.05
DETAIL # 80-19



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

LUMINAIRE ARM DETAIL

SECTION # 80.05
DETAIL # 80-20



MUNICIPALITY OF ANCHORAGE
SCALE: N.T.S.
APPROVED: [Signature]
REVISED: 2/24

TYPE 1A JUNCTION BOX

SECTION # 80.08
DETAIL # 80-31

RECORD DRAWING

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TELEPHONE	CB	BW
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DESIGN	RB	JK
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MUNICIPAL/STATE	RB	JK

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3797, 3798 & 3830	GAAB 20	See MOA Benchmark Book, Page D-35	183.44'				

PLAN CHECK CONSTRUCTION RECORD VERTICAL DATUM REVISIONS CONSULTANT SEAL

SELECT MUNICIPALITY OF ANCHORAGE STANDARD SPECIFICATIONS (MASS) DIVISION 80 ILLUMINATION DETAILS SHOWN FOR REFERENCE.

CRW ENGINEERING GROUP
3940 ARCTIC BLVD. SUITE 300
ANCHORAGE, ALASKA 99503
PHONE: (907) 562-3252
#AEC1882-AK

MUNICIPALITY OF ANCHORAGE

PROJECT MANAGEMENT AND ENGINEERING DEPARTMENT

21-13 QUINHAGAK STREET RECONSTRUCTION
E. DOWLING ROAD TO ASKELAND DRIVE SCHED C

SELECT 2024 MASS STANDARD DETAILS (REFERENCE ONLY)

SCALE HOR. N/A VER. N/A GRID 9W2033 DATE DEC 2024 STATUS 95% SHEET 16 of 16